



City Council
Mayor John W. Minto
Vice Mayor Laura Koval
Council Member Ronn Hall
Council Member Rob McNelis
Council Member Dustin Trotter

CITY OF SANTEE REGULAR MEETING AGENDA Santee City Council

City Manager | Marlene D. Best
City Attorney | Shawn D. Hagerty
City Clerk | Annette Fagan Ortiz

MEETING INFORMATION

Wednesday, October 25, 2023

6:30 p.m.

Council Chambers | Building 2

10601 Magnolia Ave • Santee, CA 92071

TO WATCH LIVE:

AT&T U-verse channel 99 (SD Market) | Cox channel 117 (SD County)

www.cityofsanteeca.gov

IN-PERSON ATTENDANCE

Members of the public who wish to view the Council Meeting live, can watch the live taping of the Council meeting in the Council Chambers on the meeting date and time listed above.

LIVE PUBLIC COMMENT

Members of the public who wish to comment on matters on the City Council agenda or during Non-Agenda Public Comment may appear in person and submit a speaker slip, before the item is called. Your name will be called when it is time to speak.

PLEASE NOTE: Public Comment will be limited to 3 minutes and speaker slips will only be accepted until the item is called. The timer will begin when the participant begins speaking.



The City Council also sits as the Community Development Commission Successor Agency and the Santee Public Financing Authority. Any actions taken by these agencies are separate from the actions taken by City Council. For questions regarding this agenda, please contact the City Clerk's Office at (619) 258-4100 x114

ROLL CALL: Mayor John W. Minto
Vice Mayor Laura Koval – District 3
Council Member Rob McNelis – District 1
Council Member Ronn Hall – District 2
Council Member Dustin Trotter – District 4

LEGISLATIVE INVOCATION: Guardian Angels Catholic Church – Reverend André Ramos

PLEDGE OF ALLEGIANCE

PRESENTATION: San Diego County Library – Santee Branch

CONSENT CALENDAR:

PLEASE NOTE: Consent Calendar items are considered routine and will be approved by one motion, with no separate discussion prior to voting. The public, staff or Council Members may request specific items be removed from the Consent Calendar for separate discussion or action. Speaker slips for this category must be presented to the City Clerk at the start of the meeting. Speakers are limited to 3 minutes.

- (1) **Approval of Reading by Title Only and Waiver of Reading in Full of Ordinances and Resolutions on the Agenda. (City Clerk – Ortiz)**
- (2) **Approval of Payment of Demands as Presented. (Finance – Jennings)**
- (3) **Approval of the Expenditure of \$72,440.21 for September 2023 Legal Services. (Finance – Jennings)**

NON-AGENDA PUBLIC COMMENT (15 minutes):

Persons wishing to address the City Council regarding items not on the posted agenda may do so at this time. In accordance with State law, Council may not take action on an item not scheduled on the Agenda. If appropriate, the item will be referred to the City Manager or placed on a future agenda. This first Non-Agenda Public Comment period is limited to a total of 15 minutes. Additional Non-Agenda Public Comment is received prior to Council Reports.

PUBLIC HEARING:

- (4) Public Hearing for Conditional Use Permit P2022-10 and Mitigated Negative Declaration AEIS2022-11 for an Auto Center with Two Dealership Buildings, a Detail Building, a Body Shop, an Automatic Carwash and Related Site Improvements on a 13.1-Acre Site Located at 10335 Mission Gorge Road (APN 384-091-01, -13, -14) in the General Commercial (GC) Zone (Applicant: Kevin Perry, HED Design). (Planning and Building – Sawa)**

Recommendation:

1. Conduct and close the Public Hearing; and
2. Adopt Mitigated Negative Declaration AEIS2022-11 and the Mitigation Monitoring and Reporting Program as complete and in compliance with the provisions of CEQA per the Resolution and authorize filing a Notice of Determination; and
3. Adopt the Resolution approving Conditional Use Permit P2022-10.

NON-AGENDA PUBLIC COMMENT (Continued):

All public comment not presented within the first Non-Agenda Public Comment period above will be heard at this time.

CITY COUNCIL REPORTS:

CITY MANAGER REPORTS:

CITY ATTORNEY REPORTS:

CLOSED SESSION:

ADJOURNMENT:





BOARDS, COMMISSIONS & COMMITTEES
OCTOBER & NOVEMBER MEETINGS

Table with 4 columns: Date, Meeting Name, and Location. Rows include SPARC, Community Oriented Policing Committee, and Council Meeting for both October and November.

The Santee City Council welcomes you and encourages your continued interest and involvement in the City’s decision-making process.

For your convenience, a complete Agenda Packet is available for public review at City Hall and on the City’s website at www.CityofSanteeCA.gov.

The City of Santee complies with the Americans with Disabilities Act. Upon request, this agenda will be made available in appropriate alternative formats to persons with disabilities, as required by Section 12132 of the American with Disabilities Act of 1990 (42 USC § 12132).



MEETING DATE October 25, 2023

ITEM TITLE PRESENTATION: SAN DIEGO COUNTY LIBRARY, SANTEE BRANCH

DIRECTOR/DEPARTMENT John W. Minto, Mayor

SUMMARY

Liz A. Vagani, Librarian Branch Manager from the San Diego County Library, Santee Branch, will present a PowerPoint presentation to the City Council, with an update regarding services offered, events, and resources to the community.

FINANCIAL STATEMENT

N/A

CITY ATTORNEY REVIEW N/A • Completed

RECOMMENDATION

Receive the presentation.

ATTACHMENT

None.

MEETING DATE October 25, 2023

ITEM TITLE **APPROVAL OF READING BY TITLE ONLY AND WAIVER OF READING
IN FULL OF ORDINANCES AND RESOLUTIONS ON THE AGENDA**

DIRECTOR/DEPARTMENT Annette Ortiz, CMC, City Clerk

SUMMARY

This Item asks the City Council to waive the reading in full of all Ordinances on the Agenda (if any) and approve their reading by title only. The purpose of this Item is to help streamline the City Council meeting process, to avoid unnecessary delay and to allow more time for substantive discussion of Items on the agenda.

State law requires that all Ordinances be read in full either at the time of introduction or at the time of passage, unless a motion waiving further reading is adopted by a majority of the City Council. (Gov. Code, § 36934). This means that each word in each Ordinance would have to be read aloud unless such reading is waived. Such reading could substantially delay the meeting and limit the time available for discussion of substantive Items. Adoption of this waiver streamlines the procedure for adopting the Ordinances on tonight's Agenda (if any), because it allows the City Council to approve Ordinances by reading aloud only the title of the Ordinance instead of reading aloud every word of the Ordinance.

The procedures for adopting Resolutions are not as strict as the procedures for adopting Ordinances. For example, Resolutions do not require two readings for passage, need not be read in full or even by title, are effective immediately unless otherwise specified, do not need to be in any particular format unless expressly required, and, with the exception of fixing tax rates or revenue amounts, do not require publication. However, like Ordinances, all Resolutions require a recorded majority vote of the total membership of the City Council. (Gov. Code § 36936).

FINANCIAL STATEMENT

N/A

CITY ATTORNEY REVIEW N/A Completed

RECOMMENDATION

It is recommended that the Council waive the reading of all Ordinances and Resolutions in their entirety and read by title only.

ATTACHMENT

None



MEETING DATE October 25, 2023

ITEM TITLE APPROVAL OF PAYMENT OF DEMANDS

DIRECTOR/DEPARTMENT Heather Jennings, Finance *HJ*

SUMMARY

A listing of checks that have been disbursed since the last Council meeting is submitted herewith for approval by the City Council.

FINANCIAL STATEMENT

Adequate budgeted funds are available for the Payment of Demands per the attached listing.

CITY ATTORNEY REVIEW N/A Completed

RECOMMENDATION *MDB*

Approve the Payment of Demands as presented.

ATTACHMENT

- 1) Summary of Payments Issued
- 2) Voucher Lists

Payment of Demands
Summary of Payments Issued

<u>Date</u>	<u>Description</u>	<u>Amount</u>
09/29/23	Accounts Payable	\$ 600,000.00
10/02/23	Retiree Health	5,385.00
10/04/23	Accounts Payable	128,017.91
10/05/23	Payroll	439,048.90
10/05/23	Accounts Payable	372,183.74
10/05/23	Accounts Payable	238,600.05
10/06/23	Accounts Payable	38,337.92
10/11/23	Accounts Payable	145,114.62
10/11/23	Accounts Payable	<u>619,706.20</u>
	TOTAL	<u>\$2,586,394.34</u>

I hereby certify to the best of my knowledge and belief that the foregoing demands listing is correct, just, conforms to the approved budget, and funds are available to pay said demands.



Heather Jennings, Director of Finance

vchlist
10/10/2023 9:46:07AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
1002462815	9/29/2023	10429 CALPERS	FY23/24 UAL 1393		FY23/24 ADDITIONAL UAL SAFETY	350,000.00
					Total :	350,000.00
1002462816	9/29/2023	10429 CALPERS	FY23/24 UAL 1392		FY23/24 ADDITONAL UAL MISC PL/	250,000.00
					Total :	250,000.00
2 Vouchers for bank code : ubgen					Bank total :	600,000.00
2 Vouchers in this report					Total vouchers :	600,000.00

Prepared by: [Signature]
Date: 10.10.23
Approved by: [Signature]
Date: 10/10/23

PyBatch
10/02/2023 8:12:21AM

Payroll Processing Report
CITY OF SANTEE

Org: santee Home Dept: 1601

10/1/2023 to 10/31/2023-1 Cycle m

Status: Active Employees

EARNINGS SECTION				DEDUCTIONS SECTION				LEAVE SECTION				
Type	Hours/units	Rate	Amount Src	Plan	Base Wages	Deduction	Benefit/Cont	LvPlan	Accrued	Taken	Banked	Lost
Totals	0.00		5,642.00			257.00	0.00					
									Gross:	5,642.00		
									Net:	5,385.00		

<< No Errors / No Warnings >>

E Bull
10/2/23
PPE 10/6/23
Paydate 10/6/23

vchlist
10/09/2023 8:45:24AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
42200	10/4/2023	10956 FRANCHISE TAX BOARD	October Retiree PPE 9/27/23		CA STATE TAX WITHHELD CA STATE TAX WITHHELD	46.00 31,714.27
					Total :	31,760.27
42211	10/4/2023	10955 DEPARTMENT OF THE TREASURY	October Retiree PPE 9/27/23		FEDERAL WITHHOLDING TAX FED WITHHOLD & MEDICARE	211.00 96,046.64
					Total :	96,257.64
2 Vouchers for bank code : ubgen						Bank total : 128,017.91
2 Vouchers in this report						Total vouchers : 128,017.91

Prepared by: Tina M
 Date: 10.9.23
 Approved by: E. B. K.
 Date: 10.10.23

PyBatch
10/03/2023 8:30:06AM

Payroll Processing Report
CITY OF SANTEE
9/14/2023 to 9/27/2023-1 Cycle b

EARNINGS SECTION					DEDUCTIONS SECTION			LEAVE SECTION					
Type	Hours/units	Rate	Amount	Src	Plan	Base Wages	Deduction	Benefit/Cont	LvPlan	Accrued	Taken	Banked	Lost
					sb-1		80.91						
					sb-3		72.39						
					sffa		3,582.75						
					sffapc		483.48						
					st1cs3	85,791.62	2,573.76	-2,573.76					
					st2cs3	14,362.83	430.89	-430.89					
					texlif		55.19						
					vaccpr		536.20						
					vaccpt		232.89						
					vcanpr		328.21						
					vcanpt		116.75						
					vgcprt		79.44						
					vision	11,076.05	545.46						
					voladd		25.56						
					voldis		214.57						
					vollad			238.03					
					vollif		238.06						
Grand Totals	15,718.75		680,021.29				240,972.39	295,395.59					

Gross:	680,021.29
Net:	439,048.90

<< No Errors / 13 Warnings >>

E Bull
10/3/23

FPE 9/27/23
Paydate 10/5/23

vchlist
10/05/2023 9:12:54AM

Voucher List
CITY OF SANTEE

Bank code : ubqen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134660	10/5/2023	10412 AT&T	301053963; OCT23		MAST PARK	149.80
					Total :	149.80
134661	10/5/2023	12951 BERRY, BONNIE	October 1, 2023		RETIREE HEALTH PAYMENTS	91.00
					Total :	91.00
134662	10/5/2023	13292 BORDER RECAPPING LLC	23-0106965-008 23-0107114-008	54413 54413	TIRES TIRE REPAIR	1,017.70 32.13
					Total :	1,049.83
134663	10/5/2023	13990 C.P. RICHARDS SIGNS, INC.	63031	54356	AMBULANCE ID MAGNETS	228.18
					Total :	228.18
134664	10/5/2023	10030 CASCADE FIRE EQUIPMENT CO INC	INV7462	54515	SAFETY APPAREL	3,382.48
					Total :	3,382.48
134665	10/5/2023	14985 CHRISTINE'S FLORAL DESIGNS	000402		FLORAL ARRANGEMENT FOR VET	250.00
					Total :	250.00
134666	10/5/2023	10032 CINTAS CORPORATION 694	4167472047	54468	MISC SHOP RENTAL SERVICE	70.13
					Total :	70.13
134667	10/5/2023	11173 CITY OF SAN DIEGO	5847767		AED SERVICE AGREEMENT	302.71
					Total :	302.71
134668	10/5/2023	10268 COOPER, JACKIE	October 1, 2023		RETIREE HEALTH PAYMENT	91.00
					Total :	91.00
134669	10/5/2023	10333 COX COMMUNICATIONS	112256001; SEP23		9130 CARLTON OAKS DR	94.74
					Total :	94.74
134670	10/5/2023	11457 D'ALESIO INC	26954	54361	HELMET SHIELDS	972.51
					Total :	972.51
134671	10/5/2023	14930 DISABILITY ACCESS	23-208	54318	DISABILITY ACCESS CONSULTANT	3,000.00
					Total :	3,000.00

vchlist
10/05/2023 9:12:54AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134672	10/5/2023	11724 ICF JONES & STOKES INC	INV-00000067856	53609	MSCP SUBAREA PLAN	1,495.00
			INV-00000068180	53609	MSCP SUBAREA PLAN	13,985.00
					Total :	15,480.00
134673	10/5/2023	14956 JET ADVERTISING LLC	11242	54561	WEBSITE REDESIGN DOWNPAYME	18,000.00
					Total :	18,000.00
134674	10/5/2023	14922 L.C. PAVING & SEALING, INC.	CIP 2020-01 1P	54325	PROSPECT AVENUE & MESA ROAD	267,457.00
			CIP 2020-01 1R		RETENTION #1	-13,372.85
					Total :	254,084.15
134675	10/5/2023	10997 LAKESIDE FIRE PROTECTION	257		SLEMSA SUPPLIES	575.39
					Total :	575.39
134676	10/5/2023	10204 LIFE ASSIST INC	1361832	54377	EMS SUPPLIES	2,536.49
			1361958	54377	EMS SUPPLIES	2,855.36
			1363269	54377	EMS SUPPLIES	1,071.40
					Total :	6,463.25
134677	10/5/2023	10174 LN CURTIS AND SONS	INV744452	54545	SAFETY EQUIPMENT	3,697.98
					Total :	3,697.98
134678	10/5/2023	10308 O'REILLY AUTO PARTS	2968-176273	54384	VEHICLE SUPPLIES	29.05
			2968-176366	54384	VEHICLE REPAIR PART	21.72
			2968-176544	54384	VEHICLE REPAIR PART	143.68
					Total :	194.45
134679	10/5/2023	10344 PADRE DAM MUNICIPAL WATER DIST	26508965;SEPT23		DIAMOND BACK IRRIGATION	16.35
					Total :	16.35
134680	10/5/2023	12904 PAT DAVIS DESIGN GROUP, INC	7503	54338	GRAPHIC ARTIST	5,625.00
					Total :	5,625.00
134681	10/5/2023	10278 RAMSEY, JOAN	October 1, 2023		RETIREE HEALTH PAYMENTS	91.00
					Total :	91.00
134682	10/5/2023	10095 RASA	5751	54512	MAP CHECK - BC2023-004	640.00
			5752	54512	MAP CHECK - POPEYE'S CHICKEN	320.00

vchlist
10/05/2023 9:12:54AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134682	10/5/2023	10095 10095 RASA	(Continued)			Total : 960.00
134683	10/5/2023	12237 RAYON, KYLE	October 1, 2023		RETIREE HEALTH PAYMENT	91.00
						Total : 91.00
134684	10/5/2023	10407 SAN DIEGO GAS & ELECTRIC	22373580042; SEP23 85097421694; SEP23		TRAFFIC SIGNALS CITY HALL GROUP BILL	8,318.30 18,112.34
						Total : 26,430.64
134685	10/5/2023	13171 SC COMMERCIAL, LLC	2471982-IN 2474150-IN	54395 54395	DELIVERED FUEL DELIVERED FUEL	374.66 412.81
						Total : 787.47
134686	10/5/2023	14945 SDES	12965	54576	CLOCK TOWER PANEL REPLACEM	24,871.00
						Total : 24,871.00
134687	10/5/2023	10110 SECTRAN SECURITY INC	23090571	54445	FY 23/24 ARMORED CAR TRANSPC	155.84
						Total : 155.84
134688	10/5/2023	14797 SEDANO FORD OF LM INC	20462892 CM20457616	54446 54446	VEHICLE REPAIR PARTS CREDIT FOR CORE DEPOSITS	100.84 -100.00
						Total : 0.84
134689	10/5/2023	14797 SEDANO FORD OF LM INC	219053	54446	VEHICLE REPAIR	100.00
						Total : 100.00
134690	10/5/2023	10314 SOUTH COAST EMERGENCY VEHICLE	515378	54417	VEHICLE REPAIR PARTS	670.38
						Total : 670.38
134691	10/5/2023	10250 THE EAST COUNTY	00134430 00134490 00134566		PUBLIC NOTICE - PY 2022 (CAPER NOTICE - INVITATION OF AN RFP NOTICE OF PREPARATION OF DR/	108.50 126.00 409.50
						Total : 644.00
134692	10/5/2023	12930 WILLIAMS, ROCHELLE	October 1, 2023		RETIREE HEALTH PAYMENTS	91.00
						Total : 91.00
134693	10/5/2023	12641 WITTORFF, VICKY DENISE	October 1, 2023		RETIREE HEALTH PAYMENTS	31.00

vchlist
10/05/2023 9:12:54AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134693	10/5/2023	12641 12641 WITTORFF, VICKY DENISE	(Continued)			Total : 31.00
134694	10/5/2023	10318 ZOLL MEDICAL CORPORATION	3810701	54419	EMS SUPPLIES	1,735.85
			3811889	54419	EMS SUPPLIES	956.30
			3813003	54419	EMS SUPPLIES	748.47
					Total :	3,440.62
35 Vouchers for bank code : ubgen						Bank total : 372,183.74
35 Vouchers in this report						Total vouchers : 372,183.74

Prepared by: *Juan M*
Date: 10.9.23
Approved by: *[Signature]*
Date: 10/5/23

vchlist
10/05/2023 10:29:46AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134695	10/5/2023	12724 AMERICAN FIDELITY ASSURANCE	D637664		VOLUNTARY LIFE INS-AM FIDELITY	4,209.54
					Total :	4,209.54
134696	10/5/2023	10334 CHLIC	3248389		HEALTH INSURANCE	213,806.37
					Total :	213,806.37
134697	10/5/2023	14458 METROPOLITAN LIFE INSURANCE	78609583		DENTAL INSURANCE	13,417.14
					Total :	13,417.14
134698	10/5/2023	10785 RELIANCE STANDARD LIFE	October 2023		VOLUNTARY LIFE INSURANCE	476.09
					Total :	476.09
134699	10/5/2023	10424 SANTEE FIREFIGHTERS	PPE 09/27/23		DUES/PEC/BENEVOLENT/BC EXP	4,209.03
					Total :	4,209.03
134700	10/5/2023	10776 STATE OF CALIFORNIA	PPE 9/27/23		WITHHOLDING ORDER	449.53
					Total :	449.53
134701	10/5/2023	10776 STATE OF CALIFORNIA	PPE 09/27/23		WITHHOLDING ORDER	429.69
					Total :	429.69
134702	10/5/2023	10001 US BANK	PPE 9/27/23		PARS RETIREMENT	850.82
					Total :	850.82
134703	10/5/2023	14600 WASHINGTON STATE SUPPORT	PPE 09/27/23		WITHHOLDING ORDER	751.84
					Total :	751.84

9 Vouchers for bank code : ubgen

Bank total : 238,600.05

9 Vouchers in this report

Total vouchers : 238,600.05

Prepared by: *J. Smith*
 Date: 10-5-23
 Approved by: *E. Bull*
 Date: 10-5-23

vchlist
10/09/2023 8:52:43AM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
6583833	10/6/2023	14705 RHS MISSIONSQUARE	PPE 09/27/23		RETIREE HSA	4,616.33
						Total :
						4,616.33
6641417	10/6/2023	14704 457 MISSIONSQUARE	PPE 09/27/23		ICMA - 457	33,721.59
						Total :
						33,721.59
2 Vouchers for bank code : ubgen						Bank total :
						38,337.92
2 Vouchers in this report						Total vouchers :
						38,337.92

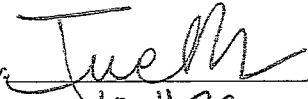

Prepared by: *Jana M*
Date: 10.9.23
Approved by: *E. Balk*
Date: 10-10-23

vchlist
10/11/2023 12:10:13PM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
9234	10/11/2023	10353 PERS	9 23 4		RETIREMENT PAYMENT	145,114.62
						Total : 145,114.62
1 Vouchers for bank code : ubgen						Bank total : 145,114.62
1 Vouchers in this report						Total vouchers : 145,114.62

Prepared by: 
Date: 10-11-23
Approved by: 
Date: 10-11-23

vchlist
10/11/2023 3:37:35PM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134704	10/11/2023	11139 ACE UNIFORMS AND ACCESSORIES	SD0143309	54489	EMT UNIFORM	123.69
					Total :	123.69
134705	10/11/2023	10010 ALLIANT INSURANCE SERVICES INC	2429579		EVENT INSURANCE - MOVIE IN TH	513.00
					Total :	513.00
134706	10/11/2023	15028 AMERICAN BUSINESS BANK	CIP2023-05 ESCROW1		CITYWIDE PAVEMENT REPAIR ANC	23,296.36
					Total :	23,296.36
134707	10/11/2023	10262 AUSTIN, ROY	10032023		RETIREE HEALTH INSURANCE	1,541.49
					Total :	1,541.49
134708	10/11/2023	12951 BERRY, BONNIE	January 1, 2023		RETIREE HEALTH PAYMENTS	91.00
					Total :	91.00
134709	10/11/2023	12189 BROWN, MATTHEW	09142023		EMPLOYEE REIMBURSEMENT	250.00
					Total :	250.00
134710	10/11/2023	10032 CINTAS CORPORATION 694	4166938897 4168181817	54468 54468	STATION SUPPLIES MISC SHOP RENTAL SERVICES	50.42 70.13
					Total :	120.55
134711	10/11/2023	15004 CORNERSTONE AGGREGATES INC	1169	54553	SANDBAGS	6,436.99
					Total :	6,436.99
134712	10/11/2023	12153 CORODATA RECORDS	RS4943286		CORODATA RECORDS MANAGMEN	632.74
					Total :	632.74
134713	10/11/2023	10358 COUNTY OF SAN DIEGO	24CTOFSASN02	54458	800 MHZ RADIO ACCESS	1,938.00
					Total :	1,938.00
134714	10/11/2023	10705 COUNTY OF SAN DIEGO TREASURER	HIRT-2024-016 UDC-2024-016		FY23-24 HIRT MEMBERSHIP FEE FY23/24 UDC MEMBERSHIP FEE	67,369.00 1,816.00
					Total :	69,185.00
134715	10/11/2023	10333 COX COMMUNICATIONS	038997401; OCT23		9951 RIVERWALK DR	57.00

vchlist
10/11/2023 3:37:35PM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134715	10/11/2023	10333	10333 COX COMMUNICATIONS		(Continued)	Total : 57.00
134716	10/11/2023	10046	D MAX ENGINEERING INC	8070	54166 WATER QUALITY MONITORING	15,626.09
						Total : 15,626.09
134717	10/11/2023	11406	DODDS, DANIEL	09282023	EMPLOYEE REIMBURESMENT	250.00
						Total : 250.00
134718	10/11/2023	14412	EKOLOJIK, INC	EKOST-08-2023	54316 SB1383 CONSULTANT	3,507.50
						Total : 3,507.50
134719	10/11/2023	14446	ENTERPRISE FM TRUST	STMT 2733	VEHICLE LEASING PROGRAM	5,743.04
						Total : 5,743.04
134720	10/11/2023	10009	FIRE ETC	181792	54369 WEARING APPAREL & SAFETY EQUI	1,971.83
				183142	54368 VEHICLE SUPPLIES	81.89
						Total : 2,053.72
134721	10/11/2023	10066	GLOBALSTAR USA LLC	000000057229101	SATELLITE PHONE SERVICE	102.88
				57229101	SATELLITE PHONE SERVICE	102.88
						Total : 205.76
134722	10/11/2023	10457	HAZARD CONSTRUCTION ENGR LLC	CIP 2023-05 1P	54471 CITYWIDE PAVEMENT REPAIR AND	465,927.25
				CIP2023-05-1R	RETENTION	-23,296.36
						Total : 442,630.89
134723	10/11/2023	11196	HD SUPPLY FACILITIES	9218282890	54415 STATION SUPPLIES	507.46
				9218431672	54415 STATION SUPPLIES	300.93
				9218667419	54415 STATION SUPPLIES	1,241.25
						Total : 2,049.64
134724	10/11/2023	14459	HMC GROUP	170239	53747 SANTEE COMMUNITY CENTER	101.12
						Total : 101.12
134725	10/11/2023	10256	HOME DEPOT CREDIT SERVICES	5163575	54416 SHOP SUPPLIES	53.85
						Total : 53.85
134726	10/11/2023	10272	JENKINS, CARROLL	10032023	RETIREE HEALTH INSURANCE	3,434.16

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10/11/2023 3:37:35PM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134726	10/11/2023	10272 10272 JENKINS, CARROLL	(Continued)			Total : 3,434.16
134727	10/11/2023	10204 LIFE ASSIST INC	1364253	54377	EMS SUPPLIES	856.17
						Total : 856.17
134728	10/11/2023	10174 LN CURTIS AND SONS	INV746086	54542	PPE SUPPLIES	1,237.19
						Total : 1,237.19
134729	10/11/2023	14742 LOWNDES, BECKY	BB2023		BREWS & BITES CASH ADVANCE	500.00
						Total : 500.00
134730	10/11/2023	10507 MITEL TECHNOLOGIES-INC / GREAT	34967985		MITEL MXE III CONTROLLER SATA	1,588.52
						Total : 1,588.52
134731	10/11/2023	15027 PARKS, SUSIE	1819		WELLNESS EVENT	264.00
						Total : 264.00
134732	10/11/2023	10157 PRIMO DJ'S	15288446		DJ SERVICE AT MOVIE IN THE PAR	625.00
						Total : 625.00
134733	10/11/2023	12828 RICK ENGINEERING COMPANY	18100(38) 18100F(4)	54219 54219	AS-NEEDED ENGINEERING SERVI AS-NEEDED ENGINEERING SERVI	2,387.50 345.00
						Total : 2,732.50
134734	10/11/2023	13171 SC COMMERCIAL, LLC	2476512-IN 2478944-IN	54395 54395	DELIVERED FUEL DELIVERED FUEL	1,188.48 1,153.44
						Total : 2,341.92
134735	10/11/2023	14797 SEDANO FORD OF LM INC	20464159	54446	VEHICLE REPAIR PART	54.89
						Total : 54.89
134736	10/11/2023	10217 STAPLES ADVANTAGE	3547208568 3547438849 3547438850	54403 54402 54335	OFFICE SUPPLIES OFFICE SUPPLIES - P&B, E FY 23/24 OFFICE SUPPLIES - FINAI	171.55 180.99 59.25
						Total : 411.79
134737	10/11/2023	15031 TALLAL INC	100123-S		MOVIE IN THE PARK - SCREEN REI	1,045.00

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10/11/2023 3:37:35PM

Voucher List
CITY OF SANTEE

Bank code : ubgen

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
134737	10/11/2023	15031 15031 TALLAL INC	(Continued)			Total : 1,045.00
134738	10/11/2023	10250 THE EAST COUNTY	00134770		PUBLIC HEARING - HABITAT FOR H	262.50
						Total : 262.50
134739	10/11/2023	10165 TRAD AM ENTERPRISES INC	413		INSTRUCTOR PAYMENT	240.00
						Total : 240.00
134740	10/11/2023	10482 TRISTAR RISK MANAGEMENT	114346	54564	FY 23/24 CLAIMS SERVICES	10,750.00
						Total : 10,750.00
134741	10/11/2023	11194 USAFACT INC	3093322		BACKGROUND CHECKS	62.84
						Total : 62.84
134742	10/11/2023	10325 VALLEY POWER SYSTEM INC	R 15092	54409	VEHICLE REPAIR PARTS	214.99
						Total : 214.99
134743	10/11/2023	14687 WEX BANK	92442807		FLEET CARD FUELING	16,677.30
						Total : 16,677.30
40 Vouchers for bank code : ubgen						Bank total : 619,706.20
40 Vouchers in this report						Total vouchers : 619,706.20

Prepared by: *[Signature]*
 Date: 10/11/23
 Approved by: *[Signature]*
 Date: 10/11/23

MEETING DATE October 25, 2023

ITEM TITLE **APPROVAL OF THE EXPENDITURE OF \$72,440.21 FOR SEPTEMBER 2023 LEGAL SERVICES**

DIRECTOR/DEPARTMENT Heather Jennings, Finance *HJ*

SUMMARY

Legal services invoices proposed for payment for the month of September 2023 total \$72,440.21 as follows:

1) General Retainer Services	\$ 17,180.80
2) Labor & Employment	4,302.50
3) Litigation & Claims	5,760.50
4) Special Projects - General Fund	32,294.61
5) Special Projects – Other Funds	825.00
6) Third-Party Reimbursable Projects	<u>12,076.80</u>
Total	<u>\$ 72,440.21</u>

FINANCIAL STATEMENT

Account Description: Legal Services

General Fund:	<u>AMOUNT</u>	<u>BALANCE</u>
Adopted Budget	\$ 811,300.00	
Revised Budget	811,300.00	
Prior Expenditures	(113,725.83)	
Current Request	(59,538.41)	\$ 638,035.76
Other Funds (excluding third-party reimbursable items):		
Adopted Budget	\$ 95,000.00	
Revised Budget	95,000.00	
Prior Expenditures	(36,555.50)	
Current Request	(825.00)	\$ 57,619.50

CITY ATTORNEY REVIEW N/A Completed

RECOMMENDATION *MSB*

Approve the expenditure of \$72,440.21 for September 2023 legal services and reimbursable costs.

ATTACHMENTS

1. Legal Services Billing Summary September 2023
2. Legal Services Billing Recap FY 2023-24



**LEGAL SERVICES BILLING SUMMARY
SEPTEMBER 2023**

DESCRIPTION	CURRENT AMOUNT	INVOICE NUMBER	NOTES
Retainer 1001.00.1201.51020	\$ 17,180.80 <u>17,180.80</u>	976978	
Labor & Employment: Labor & Employment 1001.00.1201.51020	 4,302.50 <u>4,302.50</u>	 976938	
Litigation & Claims: Litigation & Claims	4,400.00	976939	
Affordable Housing Coalition of San Diego County	235.50	976943	
Allan Family Trust Litigation	715.00	976953	
Lee Receivership	55.00	976949	
Parcel 4 Litigation	55.00	976947	
Sky Ranch Potential Homeowner/HOA Litigation 1001.00.1201.51020	 300.00 <u>5,760.50</u>	 976948	
Special Projects (General Fund): Community Oriented Policing	19,349.36	976940	
Water Quality	660.00	976944	
Prop 218	4,538.75	976945	
Entertainment District	632.50	976946	
Advanced Records Center Services for PRA	2,008.00	976950	
Cannabis	3,373.50	976951	
American Rescue Plan Act (ARPA)	110.00		
General Telecommunications Work 1001.00.1201.51020	 1,622.50 <u>32,294.61</u>	 976952	
Mobile Home Rent Control Commission	825 <u>825.00</u>	976942	2901.04.4106.51020
Third-Party Reimbursable: Lantern Crest (Applicant Initiated)	1,876.80	976955	grd1383a.40.05
MSCP Subarea Plan	3,508.80	976962	spp2101a.93.05
Santee Auto Center CUP	163.20	976957	cup2210a.10.05
Redevelopment of Carlton Oaks Golf Course	2,652.00	976954	cup1906a.10.05
Tyler St. Subdivision	204.00	976964	tm17001a.10.05
Soapy Joe's Car Wash	40.80	976956	mr23001a.10.05
Habitat for Humanity Townhomes	3,427.20	976959	tm23001a.10.05
Super Star Car Wash	204.00 <u>12,076.80</u>	976960	cup2302a.10.05
Total	 \$ 72,440.21		

**LEGAL SERVICES BILLING RECAP
FY 2023-24**

Attachment 2

<u>Category</u>	<u>Adopted Budget</u>	<u>Revised Budget</u>	<u>Previously Spent Year to Date</u>	<u>Available Balance</u>	<u>Current Request Mo./Yr.</u>	<u>Amount</u>
General Fund:						
General / Retainer	\$ 206,310.00	\$ 206,310.00	\$ 34,053.00	\$ 172,257.00	Sept-23	\$ 17,180.80
Labor & Employment	60,000.00	60,000.00	12,347.50	47,652.50	Sept-23	4,302.50
Litigation & Claims	50,000.00	50,000.00	7,505.60	42,494.40	Sept-23	5,760.50
Special Projects	494,990.00	494,990.00	59,819.73	435,170.27	Sept-23	32,294.61
Total	<u>\$ 811,300.00</u>	<u>\$ 811,300.00</u>	<u>\$ 113,725.83</u>	<u>\$ 697,574.17</u>		<u>\$ 59,538.41</u>
Other City Funds:						
MHFP Commission	\$ 10,000.00	\$ 10,000.00	\$ 110.00	\$ 9,890.00	Sept-23	\$ 825.00
Capital Projects	75,000.00	75,000.00	30,807.00	44,193.00	Sept-23	-
SLEMSA JPA	10,000.00	10,000.00	5,638.50	4,361.50	Sept-23	-
Total	<u>\$ 95,000.00</u>	<u>\$ 95,000.00</u>	<u>\$ 36,555.50</u>	<u>\$ 58,444.50</u>		<u>\$ 825.00</u>
Third-Party Reimbursable:						
Total			<u>\$ 17,223.20</u>			<u>\$ 12,076.80</u>

Total Previously Spent to Date FY 2023-24	
General Fund	\$ 113,725.83
Other City Funds	36,555.50
Applicant Deposits or Grants	17,223.20
Total	<u>\$ 167,504.53</u>

Total Proposed for Payment	
General Fund	\$ 59,538.41
Other City Funds	825.00
Applicant Deposits or Grants	12,076.80
Total	<u>\$ 72,440.21</u>

MEETING DATE

October 25, 2023

ITEM TITLE PUBLIC HEARING FOR CONDITIONAL USE PERMIT P2022-10 AND MITIGATED NEGATIVE DECLARATION AEIS2022-11 FOR AN AUTO CENTER WITH TWO DEALERSHIP BUILDINGS, A DETAIL BUILDING, A BODY SHOP, AN AUTOMATIC CARWASH AND RELATED SITE IMPROVEMENTS ON A 13.1-ACRE SITE LOCATED AT 10335 MISSION GORGE ROAD (APN 384-091-01, -13, -14) IN THE GENERAL COMMERCIAL (GC) ZONE (APPLICANT: KEVIN PERRY, HED DESIGN).

DIRECTOR/DEPARTMENT Sandi Sawa / Planning & Building Department 

SUMMARY

The project is a request by Kevin Perry from HED Design for a Conditional Use Permit (CUP) (P2022-10) and Mitigated Negative Declaration (MND) (AEIS2022-11) for a proposed auto center on a 13.1-acre vacant site at 10335 Mission Gorge Road in the General Commercial (GC) Zone (APN 384-091-01, -13, -14). The Municipal Code requires a CUP for automotive services including sales, body work, and carwashes in the General Commercial (GC) Zone. The subject site would be graded into three parcels (Parcel A, B, & C) and developed with new buildings, parking, landscaping, and storm water facilities. Parcel A would include a new auto dealership (33,974 sq. ft.) with an accessory auto detail building (2,549 sq. ft.). Parcel B would include a new auto dealership (33,112 sq. ft.). Parcel C would include a new auto body shop (16,405 sf) with an accessory self-service automatic carwash building (5,400 sq. ft.).

The project would include six driveways into the site, two on Cottonwood Avenue, three on Mission Gorge Road and one on Railroad Avenue. Proposed public improvements include sidewalks and fully landscaped parkways on the project frontage along Mission Gorge Road, Cottonwood Avenue and Railroad Avenue. Six- to eight-foot-high masonry walls with landscape buffers would be provided on the eastern and southern project boundaries that are adjacent to residential uses.

ENVIRONMENTAL REVIEW

Pursuant to the requirements of the California Environmental Quality Act (CEQA), an Initial Study (AEIS2022-11) was completed for the project, which determined that all environmental impacts of the project would be less than significant with mitigation. A Mitigated Negative Declaration (MND) (State Clearinghouse No. 2023080274) was prepared and advertised for public review from August 11, 2023 to September 11, 2023, for the required 30-day public review period. Responses have been provided to two comment letters dated during the public review period, which did not raise any new environmental issues requiring substantial revisions to the MND or further environmental review. The MND and Mitigation Monitoring and Reporting Program (MMRP) are recommended for approval and adoption by the City Council.

FINANCIAL STATEMENT 

Staff costs for application processing are paid on an actual cost recovery basis. Development Impact Fees are estimated to total \$1,839,751.49.



CITY ATTORNEY REVIEW N/A Completed

RECOMMENDATION *MSB*

1. Conduct and close the Public Hearing; and
2. Adopt Mitigated Negative Declaration AEIS2022-11 and the Mitigation Monitoring and Reporting Program as complete and in compliance with the provisions of CEQA per the attached Resolution and authorize filing a Notice of Determination; and
3. Adopt the attached Resolution approving Conditional Use Permit P2022-10.

ATTACHMENTS

Staff Report
Aerial Vicinity Map
Project Plans
MND Resolution with Final MND and MMRP
CUP Resolution

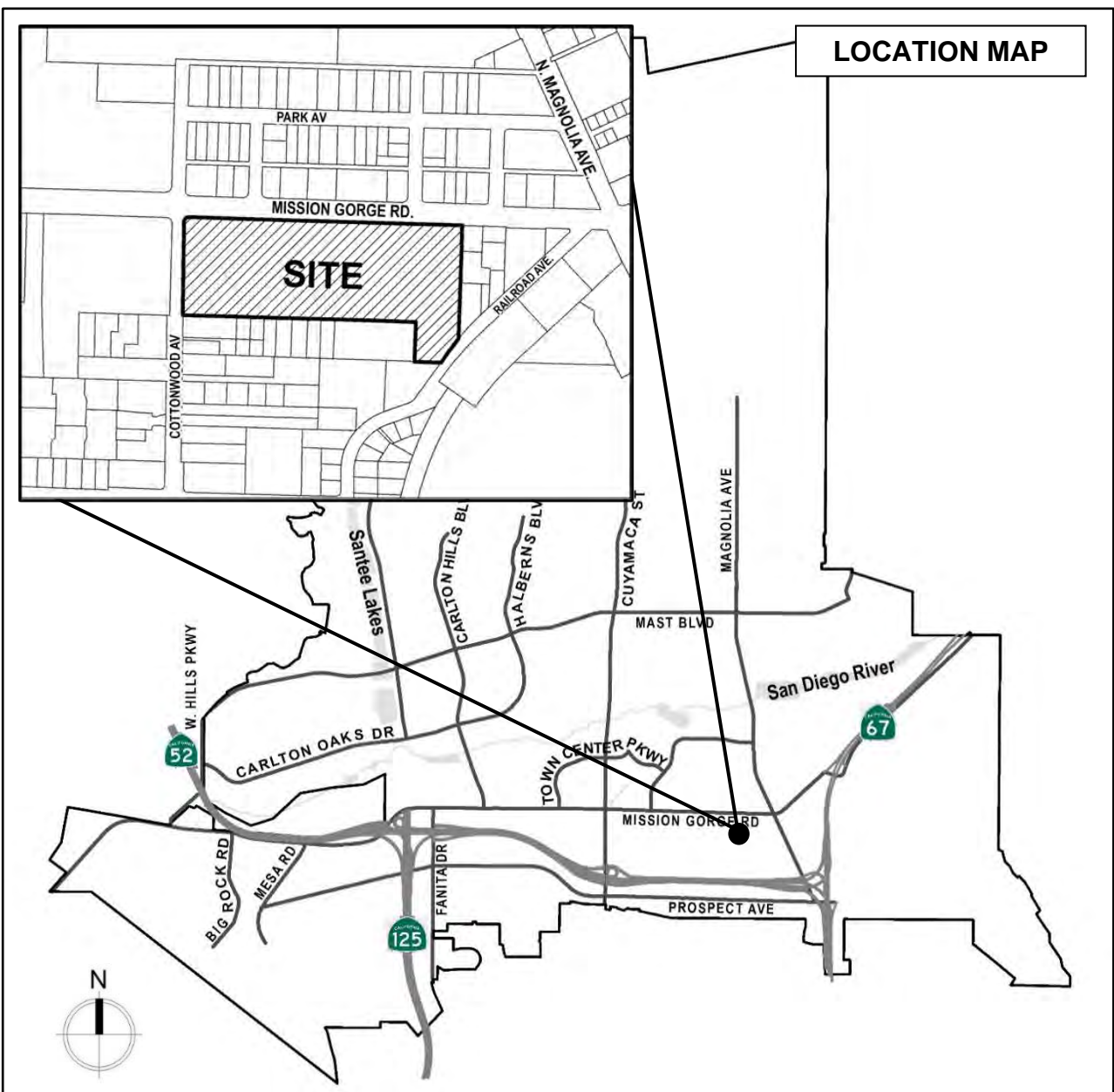
STAFF REPORT

PUBLIC HEARING FOR CONDITIONAL USE PERMIT P2022-10 AND MITIGATED NEGATIVE DECLARATION AEIS2022-11 FOR AN AUTO CENTER WITH TWO DEALERSHIP BUILDINGS, A DETAIL BUILDING, A BODY SHOP, AN AUTOMATIC CARWASH AND RELATED SITE IMPROVEMENTS ON A 13.1-ACRE SITE LOCATED AT 10335 MISSION GORGE ROAD (APN 384-091-01, -13, -14) IN THE GENERAL COMMERCIAL (GC) ZONE.

APPLICANT: KEVIN PERRY (HED DESIGN)

**CITY COUNCIL MEETING
OCTOBER 25, 2023**

Notice of the Public Hearing was published in the East County Californian on October 13, 2023 and 312 adjacent owners or residents of property within 300 feet of the request and other interested parties were notified by U.S. Mail on October 11, 2023.



A. SITUATION AND FACTS

1. Requested by Kevin Perry (HED Design)
2. Land Owner..... James Moxham, The SchoolYard LLC
3. Type and Purpose of Request Automotive Center
4. Location 10335 Mission Gorge Road
5. Site Area 13.1 acre
6. Number of lots Three
7. Hillside Overlay No
8. Existing Zoning..... General Commercial (GC)
9. Surrounding Zoning..... North: General Commercial (GC)
South: Light Industrial (IL)
East: General Commercial (GC)
West: Medium High Density Residential (R-14)
10. General Plan Designation General Commercial (GC)
11. Existing Land Use..... Vacant
12. Surrounding Land Use..... North: Commercial uses (primarily automotive)
South: Single-family homes
East: Apartment building and single-family homes
West: Apartment complex
13. Terrain Generally flat
14. Environmental Status Mitigated Negative Declaration
15. APN..... 384-091-01, -13, -14
16. Within Airport Influence Area..... Within Airport Influence Area 1; within Airport Safety Zones 2 and 4.

B. BACKGROUND

Existing Conditions:

The 13.1-acre project site is located at 10335 Mission Gorge Road, on the southeast corner of Mission Gorge Road and Cottonwood Avenue, approximately 800 feet west of Magnolia Avenue. The project site is adjacent to commercial uses that are primarily automotive in nature to the north, an apartment building and single-family home to the east, single-family homes to the south, and an apartment complex to the west.

C. PROJECT DESCRIPTION

Overview:

The proposed project is for the construction and operation of an auto center on a 13.1-acre site located at 10335 Mission Gorge Road on the south side of Mission Gorge Road, west of Magnolia Avenue and east of Cottonwood Avenue in the General Commercial (GC) Zone and is further identified by Assessor's Parcel Numbers (APN) 384-091-01, -13, &-14. The subject site would be graded into three parcels (Parcel A, B, & C) and developed with new buildings, parking, landscaping, and storm water facilities. Parcel A would include a new auto dealership (33,974 sq. ft.) with an accessory auto detail building (2,549 sq. ft.). Parcel B would include a new auto dealership (33,112 sq. ft.). Parcel C would include a new auto body shop (16,405 sq. ft.) with an accessory car wash building (5,400 sq. ft.). The maximum building height would be approximately 33 feet.

The project would include six driveways into the site, two on Cottonwood Avenue, three on Mission Gorge Road and one on Railroad Avenue. Proposed public improvements include sidewalks and fully landscaped parkways on the project frontage along Mission Gorge Road, Cottonwood Avenue and Railroad Avenue. Six- to eight-foot-high masonry walls with landscape buffers would be provided on the eastern and southern project boundaries that are adjacent to residential uses.

D. ANALYSIS

General Plan and Zoning Ordinance Consistency:

The General Commercial (GC) designation provides for commercial areas with a wide range of retail and service activities. Intended uses include automotive uses and other specialized services. This designation encourages the grouping of commercial outlets into consolidated centers with direct access to major roads, prime arterials or freeways. With direct access to Mission Gorge Road, which is designated as a major arterial in the Mobility Element, the proposed automotive use is aligned with the recommended uses of the General Plan and Zoning Ordinance for the subject site.

Development Standards

The project is consistent with the development standards of the Zoning Ordinance for the General Commercial (GC) Zone.

The maximum height limitation in the General Commercial zone is 40 feet for primary buildings in accordance with SMC Section 13.12.040, and 16 feet for detached auxiliary structures in accordance with SMC Chapter 13.30. The maximum height of all proposed primary buildings is 31 feet 4 inches. The proposed auxiliary trash enclosure is eight feet in height at the uppermost portion of the roof. The project complies with height limitations.

In terms of building setbacks, the Santee Municipal Code requires a 10-foot front setback, 5-foot interior side setback, and 5-foot rear setback. The Community Enhancement Element of the General Plan encourages a landscaped setback of 20 to 40 feet along Mission Gorge Road. Table 1 shows the development standards for the project.

Table 1: Development Standards Summary for General Commercial

	Required	Proposed
Maximum Height	40 feet	33 feet 4 inches
Total Parking	229 spaces (1/400 sq. ft.)	688 spaces
Customer Parking	N/A	81 spaces
EV Parking	11 spaces (5%)	24 spaces
Setbacks (Main Buildings)		
Streets	20 feet along Mission Gorge Road; 10 feet along other streets	Mission Gorge Rd: 155 feet Cottonwood Ave: 205 feet Railroad Ave: 57 feet
Interior side	5 feet	Body shop: 11 feet Carwash: 11 feet
Rear	5 feet	Dealership buildings: 61 feet Body shop: 11 feet

Landscaping:

Landscaping is required within all setback areas and must also be incorporated into the parking lot. The design includes landscape islands within the parking field and landscaping strips around the perimeter of the site. Additionally, the site was designed with an enhanced landscape buffer approximately 30 inches in width with a meandering sidewalk along Mission Gorge Road, as encouraged in the Community Enhancement Element of the General Plan. A total of 104,111 square feet of on-site landscaping (approximately 18% of the project site) will be provided.

On-Site Parking:

The parking requirement for auto dealerships with accessory uses, including body shops and car washes, is one per 400 square feet of building gross floor area. The gross floor area of the building is 91,440 square feet, which would require a total of 229 parking spaces. The site design includes 688 parking spaces, including 94 spaces for display, 305 for inventory, 81 spaces for customers, 186 auto service, and 22 spaces for vacuums. Of these parking spaces, 11 are ADA spaces, 33 are clean air spaces, and 24 are electric vehicle spaces.

Compatibility with Adjacent Land Uses

Compatibility between adjacent land uses in the City is of primary importance to achieve a safe, livable and functional community and to ensure a high quality of

life. Enhancing compatibility between land uses can be achieved through locating uses together that have similar or compatible public service and site planning needs. The proposed auto center groups complimentary auto-oriented uses in one comprehensive development on an appropriately commercially-zoned site. Surrounding properties are commercially and industrially zoned and, as such, the proposed development would be consistent with planned allowable uses in this area. The proposal would be also compatible with existing auto-oriented uses along the north side of Mission Gorge Road. The project location is also suitable for the proposed use as it is along the City's primary commercial corridor with direct access from Mission Gorge Road, a major arterial in the Mobility Element.

Potential incompatibility exists with existing non-conforming residential uses surrounding the site. However, the proposed development incorporates buffering and remedial measures, such as landscape strips and the erection of six- to eight-foot walls along the perimeter of the site adjoining these residential uses in order to minimize this conflict. The proposed meandering sidewalk with landscape buffers along Mission Gorge Road will also help enhance the area and provide residents of existing conforming residential development near the site an improved pedestrian experience along this segment of Mission Gorge Road.

Traffic

A Traffic Impact Analysis was prepared by Linscott, Law & Greenspan Engineers on April 20, 2023, and an analysis on Vehicle Miles Traveled (VMT) was conducted. The project would generate approximately 3,336 daily trips, with 196 trips during AM peak hour and 254 trips during PM peak hour.

Based on the intersection and segment analyses, the project effects were determined at the Mission Gorge Road/Cuyamaca Street and Mission Gorge Road/Cottonwood Avenue intersections. For the Mission Gorge Road / Cuyamaca Street intersection, a northbound right-turn lane is needed to improve operations. This improvement is currently underway as a Capital Improvement Program (CIP) project. Currently, the Mission Gorge Road/Cottonwood Avenue intersection operates with permissive north/south phasing. The project is conditioned to provide an exclusive left-turn lane at the northbound and southbound approaches on Cottonwood Avenue and modify the traffic signal to provide north/south protected phasing.

The project is designed to align the project driveway opposite of Edgemoor Drive with modification to the existing traffic signal and the provision of a 40-foot-wide driveway with one left-turn lane and one shared through/right lane in the northbound direction at this intersection. The project is conditioned to prohibit the southbound left-turn movement and to only allow right-in/right-out and outbound left-turn movements at this driveway. The project also includes stop signs that will be installed at all unsignalized driveways for traffic exiting the driveways.

Drainage

Pre-Project Condition

The pre-project site drains generally northwest to a curb inlet located along the eastern edge Cottonwood Avenue, just south of the intersection of Cottonwood Avenue and Mission Gorge Road. Runoff from the southern part of the site flows from the eastern boundary of the site to the west along Happy Lane before it rounds the corner at Cottonwood Avenue and flows north along the street where it is collected by the curb inlet. Runoff from the northern part of site flows north to Mission Gorge Road then concentrates and flows west along the street until it rounds the corner and travels south at Cottonwood Avenue where it is collected by the curb inlet. After entering the curb inlet, runoff from the project site drains to the public storm drain system and into the San Diego River.

Post-Project Condition

Drainage patterns for the proposed condition will remain similar to drainage patterns in the pre-project condition. In the post-project condition, the project area will be divided into three lots, which each drain to a separate underground storage vault and proprietary compact biofiltration system. It is anticipated that peak flows from a 100-year, 6-hour storm event on the post-project site will remain lower than peak flows from the pre-project condition of the site. After exiting the vaults and compact biofiltration system, the stormwater is conveyed via a private storm drain to a connection with the public storm drain system located at the curb inlet which is on the southeast corner of the intersection of Cottonwood Avenue and Mission Gorge Road. After connecting to the public storm drain system, flows from the site are conveyed to the San Diego River.

Development Impact Fees:

The proposed development would trigger development impact fees as listed below:

Drainage	\$746,276.37
Traffic.....	\$941,595.50
Traffic Signal.....	\$151,879.62
Total	\$1,839,751.49

Environmental Status:

An Initial Study was conducted in accordance with the California Environmental Quality Act (CEQA). The analysis indicated that there will not be significant adverse impacts on the environment with mitigation. Mitigated Negative Declaration (MND) AEIS2022-11 (State Clearinghouse No. 2023080274) was prepared and advertised for public review from August 11, 2023 to September 11, 2023, and is recommended for approval. All comments received have been reviewed and considered, and no substantial new information has been received that has not already been analyzed in the MND. No revisions made to the MND in response to comments constitute substantial revisions as defined in State CEQA Guidelines Section 15073.5. A full discussion of the environmental issues and response to comments is found in the attached MND.

E. STAFF RECOMMENDATION

1. Conduct and close the Public Hearing; and
2. Adopt Mitigated Negative Declaration AEIS2022-11 and the Mitigation Monitoring and Reporting Program (MMRP) as complete and in compliance with the provisions of CEQA per the attached Resolution and authorize filing a Notice of Determination; and
3. Adopt the attached Resolution approving Conditional Use Permit P2022-10.

AERIAL VICINITY MAP

10335 Mission Gorge

Conditional Use Permit P2022-10, Mitigated Negative Declaration AEIS 2022-11



Cameron Brothers Company LLC



CAMERON
— BROTHERS COMPANY LLC —

Santee Auto Center

10335 Mission Gorge Road
Santee, CA 92071

CUP Resubmittal

08/11/2023

HED

BD22-CB036-004

PROJECT DIRECTORY

PROPERTY OWNER
 THE SCHOOLYARD LLC
 10540 PROSPECT AVENUE
 SUITE 200
 SANTEE, CA 92071
 CONTACT: JIM MOXHAM
 TELEPHONE: (619) 562-3030
 EMAIL: jim@cameronbrothers.net

ARCHITECT/PROJECT MANAGER
 HED
 11750 SORRENTO VALLEY ROAD
 SUITE 100
 SAN DIEGO, CA 92121
 CONTACT: KEVIN PERRY
 TELEPHONE: (858) 519-3688
 EMAIL: kperry@hed.design

CIVIL
 RICK ENGINEERING COMPANY
 5620 FRIARS ROAD
 SAN DIEGO, CA 92116
 CONTACT: KELLY DRUSE
 TELEPHONE: (619) 251-0767
 EMAIL: kdruce@rickengineering.com

LANDSCAPE
 HED
 11750 SORRENTO VALLEY ROAD
 SUITE 100
 SAN DIEGO, CA 92121
 CONTACT: LINDSAY CHICONE
 TELEPHONE: (619) 398-3804
 EMAIL: lchicone@hed.design

PROJECT DIRECTORY

DESCRIPTION OF WORK: EXISTING VACANT LOTS TO BE CONVERTED INTO SANTEE AUTO CENTER. SITE DEVELOPMENT INCLUDES GRADING AND LANDSCAPING FOR A TOTAL OF 3 PARCELS WITH NEW BUILDINGS, PARKING LOT, AND BID BASIN ON EACH. PARCEL A INCLUDES A NEW AUTO DEALERSHIP WITH AN ACCESSORY AUTO DETAIL BUILDING. PARCEL B INCLUDES A NEW AUTO DEALERSHIP. PARCEL C INCLUDES A NEW AUTO BODY SHOP WITH AN ACCESSORY CAR WASH BUILDING.

ASSESSOR'S PARCEL NO.: 384-091-01, 384-091-13, 384-091-14

SITE ADDRESS: 10335 MISSION GORGE ROAD
 SANTEE, CA 92071

ZONING: GC - GENERAL COMMERCIAL

BUILDING HEIGHT: 40' PER SANTEE MUNICIPAL CODE 13.12.0404A
 PARCEL A 33'
 PARCEL B 33'
 PARCEL C 38'

SETBACKS: PER SANTEE MUNICIPAL CODE 13.12.0408
 STREET YARD 10'
 REAR YARD 5'
 INTERIOR SIDE OF P.L. 0'

LOT SIZE: ± 13.1 ACRES TOTAL
 PARCEL A ± 6 ACRES
 PARCEL B ± 4.1 ACRES
 PARCEL C ± 3 ACRES

LOT COVERAGE: 15%
 PARCEL A DEALERSHIP 33,112 SF / AUTO DETAIL 2,549 SF
 PARCEL B DEALERSHIP 33,112 SF
 PARCEL C BODY SHOP 16,408 SF / CAR WASH 5,400 SF

PROJECT LOCATION



VICINITY MAP

SHEET INDEX	
SHEET NUMBER	SHEET NAME
GENERAL	
G-000	COVERSHEET
G-011	SHEET INDEX & PROJECT DATA
G-012	VICINITY PLAN
G-019	SURROUNDING PROPERTIES
G-014	GILLESPIE FIELD ALUCP SAFETY ZONES
CIVIL	
C-001	TITLE SHEET
C-002	STREET SECTIONS
C-003	EXISTING CONDITIONS
C-004	PROPOSED MAP EXHIBIT
C-005	SITE GRADING PLAN
C-006	SITE GRADING PLAN
C-007	UTILITY PLAN
C-008	UTILITY PLAN
C-009	FIRE ACCESS
C-010	DETAILS
C-011	OFFSITE DRAINAGE PLAN
LANDSCAPE	
L100	LANDSCAPE TITLE SHEET
L101	(A) HARDSCAPE PLAN
L102	(B) HARDSCAPE PLAN
L103	(C) HARDSCAPE PLAN
L200	(A) PLANTING PLAN
L201	(B) PLANTING PLAN & NOTES
L202	(C) PLANTING PLAN & LEGEND
L203	WATER CALCULATIONS
L204	LANDSCAPE MASTER PLAN
ARCHITECTURAL - SITE	
D001	ISOMETRIC VIEW
D010	MASTER SITE PLAN
D011	TRASH ENCLOSURE
D012	SITE DETAILS
D013	SIGNAGE
ARCHITECTURAL	
D100	(A) VICINITY SITE PLAN
D101	(A) ISOMETRIC VIEW
D102	(A) PROPOSED FLOOR PLANS
D103	(A) PROPOSED ROOF PLAN
D104	(A) PROPOSED BUILDING ELEVATIONS
D105	(A) PROPOSED BUILDING SECTIONS
D106	(A) PROPOSED DETAIL BUILDING
D200	(B) VICINITY SITE PLAN
D201	(B) ISOMETRIC VIEW
D202	(B) PROPOSED FLOOR PLANS
D203	(B) PROPOSED ROOF PLAN
D204	(B) PROPOSED BUILDING ELEVATIONS
D205	(B) PROPOSED BUILDING SECTION
D300	(C) VICINITY SITE PLAN
D301	(C) ISOMETRIC VIEW
D302	(C) PROPOSED FLOOR PLAN - COLLISION
D303	(C) PROPOSED ROOF PLAN - COLLISION
D304	(C) BUILDING ELEVATIONS - COLLISION
D305	(C) BUILDING SECTIONS - COLLISION
D306	(C) PROPOSED CAR WASH FACILITY

APPLICABLE CODES

DESIGN AND CONSTRUCTION SHALL CONFORM TO ALL FEDERAL, STATE, AND LOCAL BUILDING CODES AND ORDINANCES TO INCLUDE BUT NOT LIMITED TO THE MOST CURRENT VERSION OF THE FOLLOWING DOCUMENTS:

- 2019 CALIFORNIA BUILDING CODE PART 2 OF TITLE 24
- 2019 CALIFORNIA ELECTRIC CODE PART 3 OF TITLE 24
- 2019 CALIFORNIA MECHANICAL CODE PART 4 OF TITLE 24
- 2019 CALIFORNIA PLUMBING CODE PART 5 OF TITLE 24
- 2019 CALIFORNIA ENERGY CODE PART 6 OF TITLE 24
- 2019 CALIFORNIA ELEVATOR CODE PART 7 OF TITLE 24
- 2019 CALIFORNIA FIRE CODE PART 9 OF TITLE 24
- 2019 CALIFORNIA EXISTING BUILDING CODE PART 10 OF TITLE 24
- 2019 CALIFORNIA GREEN BUILDING CODE PART 11 OF TITLE 24

- TITLE 19 CFR, PUBLIC SAFETY, STATE FIRE MARSHAL REGULATIONS
- ACCESSIBILITY REGULATIONS AS PRESCRIBED BY THE 2010 CALIFORNIA BUILDING CODE, CHAPTER 11
- AMERICANS WITH DISABILITIES ACT GUIDELINES, AS AMENDED, 28 CFR PART 35 AND 36, CFR 1911.
- ALL CODES AND ORDINANCES ADOPTED BY THE CITY OF SANTEE.



Cameron Brothers Company LLC

Santee Auto Center

10335 Mission Gorge Road
 Santee, CA 92071

Date Issued For
 12/16/2022 CLIP Submittal
 09/25/2023 CLIP Resubmittal
 08/11/2023 CLIP Resubmittal

HED
 11750 Sorrento Valley Rd
 Suite 100
 San Diego, California
 92121 USA
 (858) 398-3808
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CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
06/23/2023 CUP Resubmital
08/11/2023 CUP Resubmital



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BC22-CB036-004

VICINITY PLAN

G-012

TC/AE - TOWN CENTER
ART & ENTERTAINMENT
DISTRICT OVERLAY

TC - TOWN CENTER

GC - GENERAL COMMERCIAL

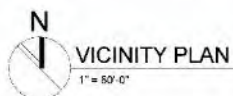
R14 - MEDIUM-HIGH
DENSITY RESIDENTIAL

PROJECT SITE

GC - GENERAL
COMMERCIAL

IL - LIGHT INDUSTRIAL

R2 - LOW-MEDIUM
DENSITY RESIDENTIAL



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MISSION GORGE ROAD

COTTONWOOD AVENUE

HAPPY LANE

GIENKE LANE

LUNAR LANE

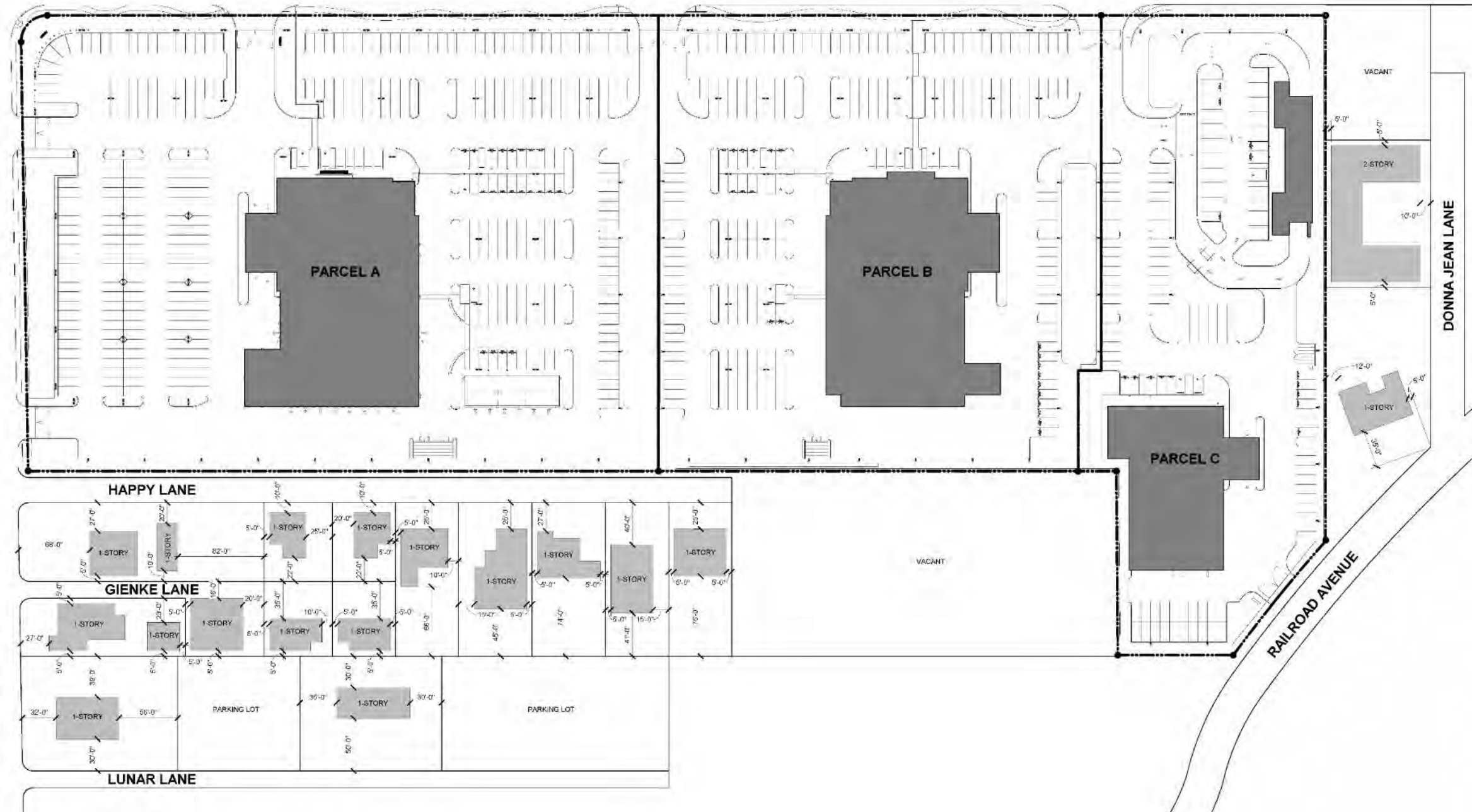
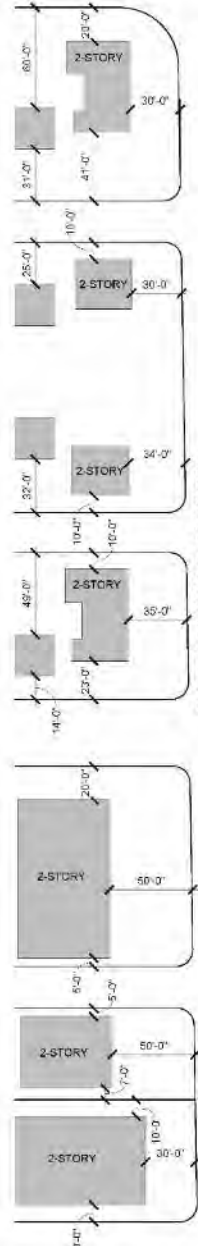
DONNA JEAN LANE

RAILROAD AVENUE

PARCEL A

PARCEL B

PARCEL C



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SURROUNDING
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G-013



Cameron Brothers Company LLC

Santee Auto Center

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DONNA JEAN LANE

ZONE 3 - INNER TURNING ZONE

ZONE 4 - OUTER APPROACH / DEPARTURE ZONE

ZONE 2 - INNER APPROACH / DEPARTURE ZONE

HAPPY LANE

GIENKE LANE

LUNAR LANE

RAILROAD AVENUE

ZONE 2 - INNER APPROACH / DEPARTURE ZONE
70 PEOPLE / ACRE (PER ALUCP 3-48 TABLE III-2)
300,540 SF = 6.9 ACRES = 483 PEOPLE

250 SF / PERSON (PER ALUCP 3-48 TABLE III-2)
483 PEOPLE x 250 SF = 120,750 SF

ZONE 3 - INNER TURNING ZONE
130 PEOPLE / ACRE (PER ALUCP 3-48 TABLE III-2)
118,568 SF = 2.72 ACRES = 354 PEOPLE

200 SF / PERSON (PER ALUCP 3-48 TABLE III-2)
354 PEOPLE x 200 SF = 70,800 SF

ZONE 4 - OUTER APPROACH / DEPARTURE ZONE
130 PEOPLE / ACRE (PER ALUCP 3-48 TABLE III-2)
163,972 SF = 3.76 ACRES = 489 PEOPLE

250 SF / PERSON (PER ALUCP 3-48 TABLE III-2)
489 PEOPLE x 250 SF = 122,250 SF



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6/2023
BD22-CB036-004

GILLESPIE FIELD ALUCP SAFETY ZONES

G-014



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10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
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06/20/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal

RICK
ENGINEERING COMPANY

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SAN DIEGO, CA 92110
619.791.0200
FAX: 619.791.4155
Surrender - Sacramento - San Luis Obispo
Chicago - Denver - Houston - Phoenix
rickengineering.com

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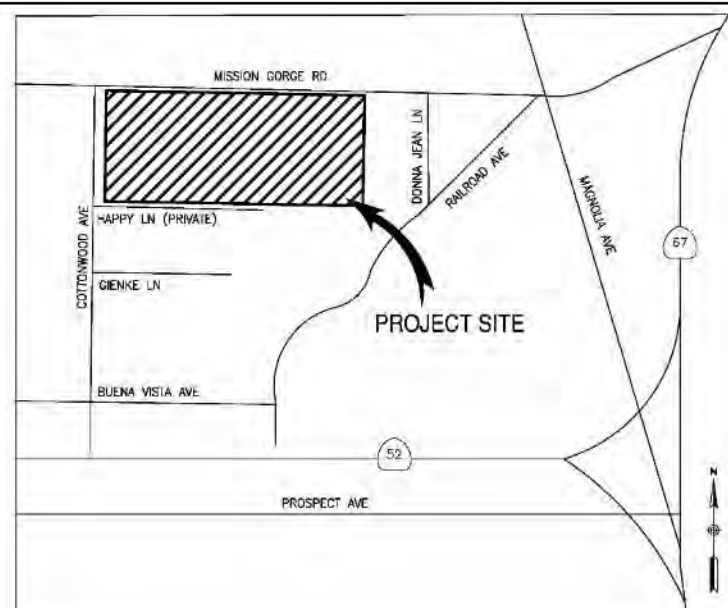
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(619) 398-3200

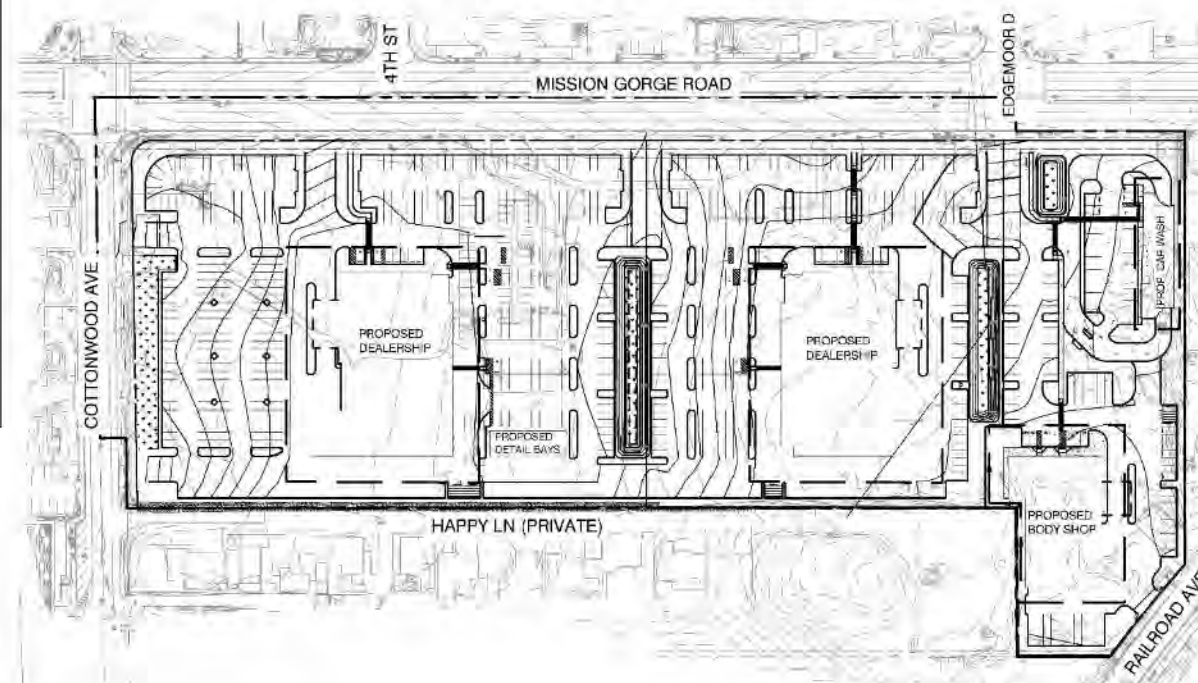
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02022
0022-0006-004
TITLE SHEET
C-001

CUP NO. P2022-10 SANTEE AUTO CENTER



VICINITY MAP



SITE MAP

100 50 0 100
GRAPHIC SCALE: 1"=100'

OWNER / APPLICANT

THE SCHOOLYARD, LLC
10580 PROSPECT AVE
SANTEE, CA 92071

ADDRESS

10335 MISSION GORGE RD
SANTEE, CA 92071

ASSESSOR PARCEL NUMBER

384-091-01, 384-091-13 AND 384-091-14

EARTHWORK/GRADING QUANTITIES

OUT: 24,500 CY FILL: 20,000 CY EXPORT: 4,500 CY

QUANTITIES SHOWN ARE QUANTITIES AND DO NOT INCLUDE THE EFFECT OF REMEDIAL GRADING. THE RAW NUMBERS HAVE NOT BEEN ADJUSTED FOR SHRINKAGE OR BULKAGE.

ABBREVIATIONS

AC	ACRES / ASBESTOS-CEMENT	ID	IDENTIFICATION
ADT	AVERAGE DAILY TRAFFIC	IE	INVERT ELEVATION
APN	ASSESSOR'S PARCEL NUMBER	MAX	MAXIMUM
ASTM	AMERICAN SOCIETY OF TESTING AND MATERIALS	MIN	MINIMUM
BLDG	BUILDING	N.T.S.	NOT TO SCALE
BMP	BEST MANAGEMENT PRACTICE	NO.	NUMBER
BOT	BOTTOM	PERF	PERFORATED
C&G	CURB AND GUTTER	PIP	PROTECT IN PLACE
C.B.	CATCH BASIN	PL	PROPERTY LINE
C.O.	CLEANOUT	PM	PARCEL MAP
COSD	COUNTY OF SAN DIEGO	PROP	PROPOSED
CL	CENTERLINE	PVC	POLYVINYL CHLORIDE
CM/LTC	CEMENT-MORTAR LINED TAPE COATING	FVI	PRIVATE
CYD	CUBIC YARD OF DEBRIS	R/W	RIGHT-OF-WAY
DG	DECOMPOSED GRANITE	RPDA	REDUCED PRESSURE DETECTOR ASSEMBLY
DMA	DRAINAGE MANAGEMENT AREA	RT	RIGHT
DWY	DRIVEWAY	S	SEWER
ELEV	ELEVATION	S/W	SIDEWALK
EMRA	ENCROACHMENT MAINTENANCE AND REMOVAL AGREEMENT	SD	STORM DRAIN
ESMT	EASEMENT	SF	SQUARE FEET
EX/EXIST	EXISTING	SHIT	SHEET
FF	FINISH FLOOR	ST	STREET
FG	FINISH GRADE	TB	THRUST BLOCK
FGBW	FINISH GRADE AT BACK OF WALL	TC	TOP OF CURB
FGFW	FINISH GRADE AT FACE OF WALL	TDO	TOP OF CLEANOUT
FL	FLOWLINE	TO	TOP OF GRADE
FS	FINISH SURFACE / FIRE SERVICE	TOB	TOP OF BASIN
FT	FEET	TW	TOP OF WALL
HDP	HIGH DENSITY POLY ETHYLENE	TYP	TYPICAL
HMP	HYDROMODIFICATION MANAGEMENT PLAN	W	WATER
		W/	WITH

LEGEND

	EXISTING	PROPOSED
BOUNDARY PROPERTY LINE	---	---
CENTERLINE	---	---
CURB	---	---
CURB & GUTTER	---	---
FENCE-GENERAL USE	---	---
FENCE-CHAINLINK	---	---
EASEMENT	---	---
FLOWLINE	---	---
RETAINING WALL	---	---
SLOPE	---	---
CONTOURS	---	---
WATER LINE	---	---
FIRE SERVICE	---	---
SANITARY SEWER LINE	---	---
STORM DRAIN LINE	---	---
GAS LINE	---	---
ELECTRIC LINE	---	---
OVERHEAD WIRES	---	---
FIBER OPTICS	---	---
TELEPHONE	---	---
TELEPHONE OVERHEAD	---	---
JOINT TRENCH	---	---
ASPHALT LIMIT	---	---
ROUND AREA DRAIN (PVT.)	---	---
ENERGY DISSIPATOR (PVT.)	---	---
U-TYPE HEADWALL (PVT.)	---	---
CATCH BASIN, TYPE F (PVT.)	---	---
TYPE A-4 C.O. (PVT.)	---	---
FIRE HYDRANT	---	---
SANITARY SEWER MANHOLE	---	---
LIGHT POST/STREET LIGHT	---	---
SIGNAGE	---	---
8" TRENCH DRAIN WITH TRAFFIC RATED GRATE (PVT.)	---	---
TRAFFIC SIGNAL	---	---
TRUNCATED DOMES	---	---
CURB RAMP	---	---
ADA COMPLIANT WALKWAY	---	---

SHEET INDEX

NO.	DESCRIPTION
C-001	TITLE SHEET
C-002	STREET SECTIONS
C-003	EXISTING CONDITIONS
C-004	PROPOSED MAP EXHIBIT
C-005	SITE GRADING PLAN
C-006	SITE GRADING PLAN
C-007	UTILITY PLAN
C-008	UTILITY PLAN
C-009	FIRE ACCESS
C-010	DETAILS
C-011	OFFSITE DRAINAGE PLAN

PUBLIC UTILITIES

WATER (PUBLIC)	PADRE DAM MUNICIPAL DISTRICT
STORM DRAIN	CITY OF SANTEE
GAS & ELECTRIC	SDG&E
CABLE T.V.	COX CABLE
POLICE & FIRE	CITY OF SANTEE
SEWER (PUBLIC)	PADRE DAM MUNICIPAL DISTRICT
SCHOOL DISTRICT	CROSSMOUNT UNION HIGH SCHOOL DISTRICT SANTEE ELEMENTARY SCHOOL DISTRICT

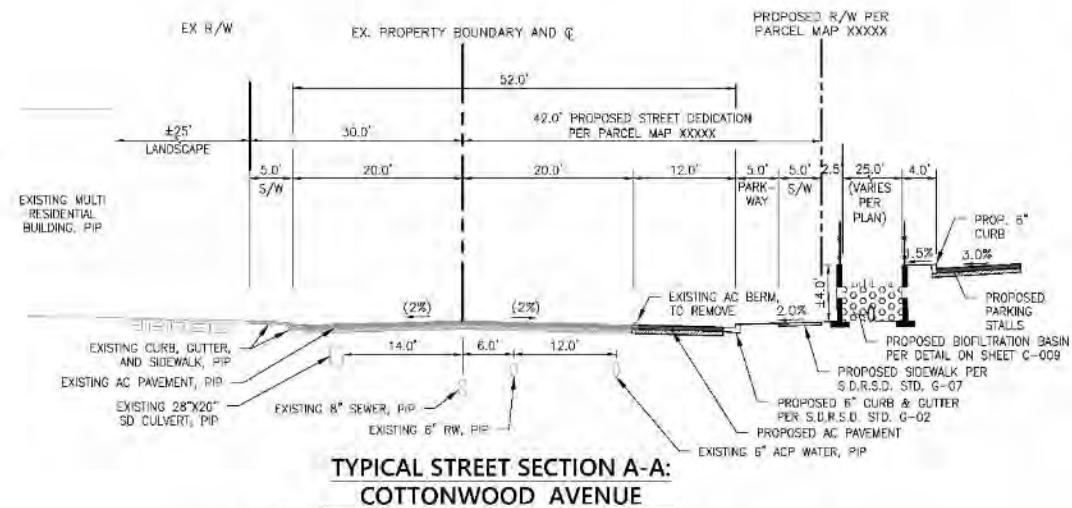
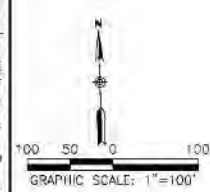
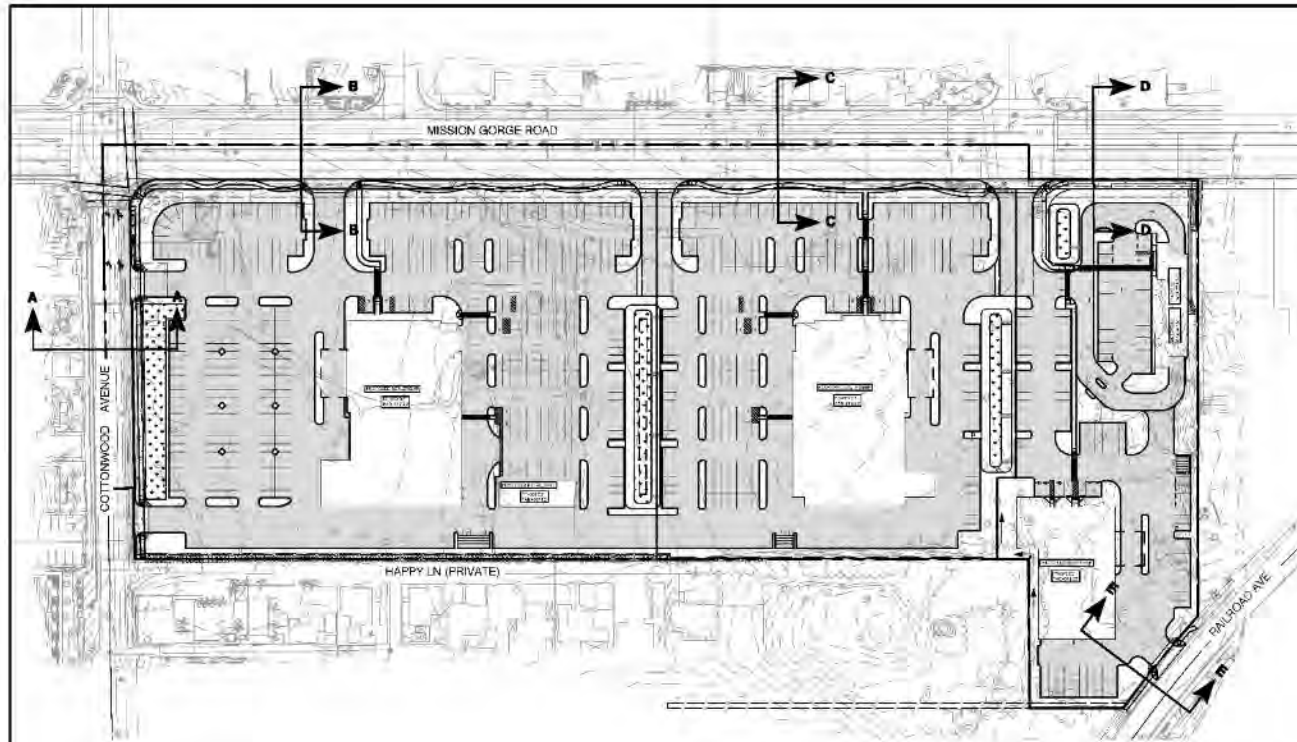
ENGINEER OF WORK:



KELLY L. DRUSE R.C.E. NO. 70998 EXP 12-31-22

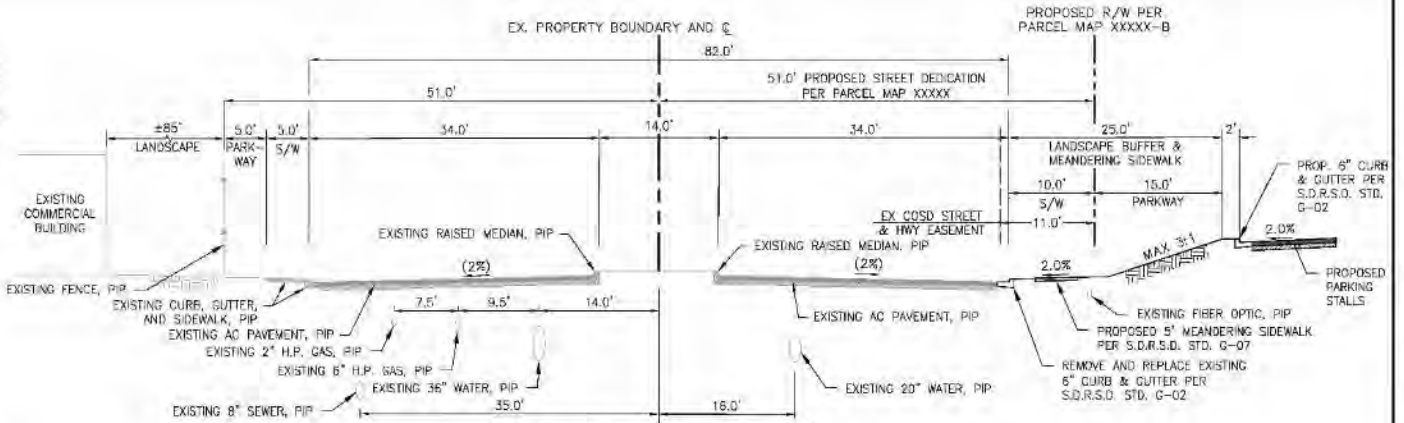
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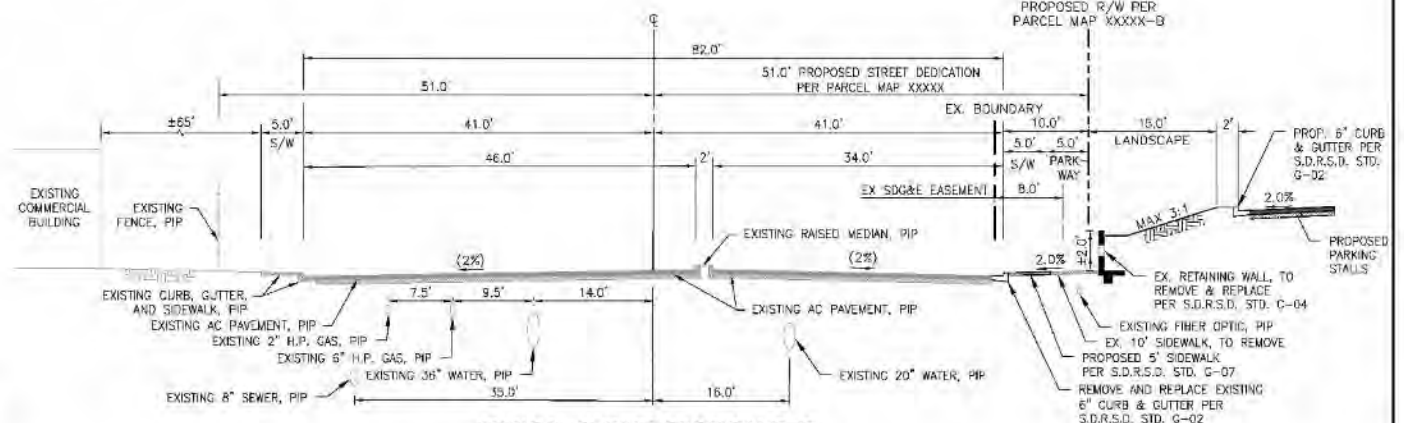
**TYPICAL STREET SECTION A-A:
COTTONWOOD AVENUE**
N.T.S.

NOTE:
REPLACE FAILED OR INADEQUATE PAVEMENT TO THE CENTERLINE AND/OR SIDEWALK ADJACENT TO THE SITE ON COTTONWOOD AVENUE TO THE SATISFACTION OF THE DIRECTOR OF DEVELOPMENT SERVICES



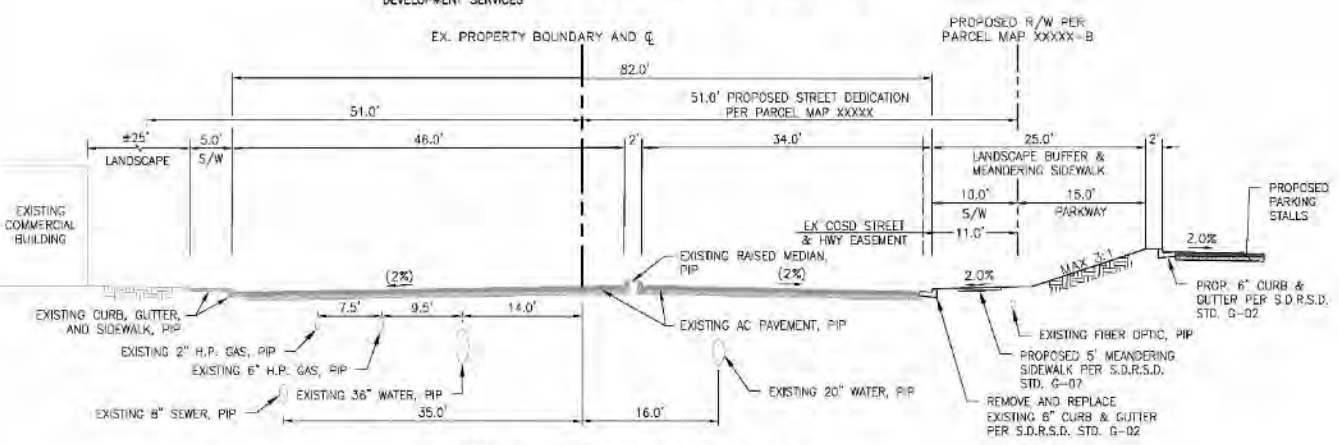
**TYPICAL STREET SECTION C-C:
MISSION GORGE ROAD**
N.T.S.

NOTE:
REPLACE FAILED OR INADEQUATE PAVEMENT TO THE CENTERLINE AND/OR SIDEWALK ADJACENT TO THE SITE ON MISSION GORGE ROAD TO THE SATISFACTION OF THE DIRECTOR OF DEVELOPMENT SERVICES



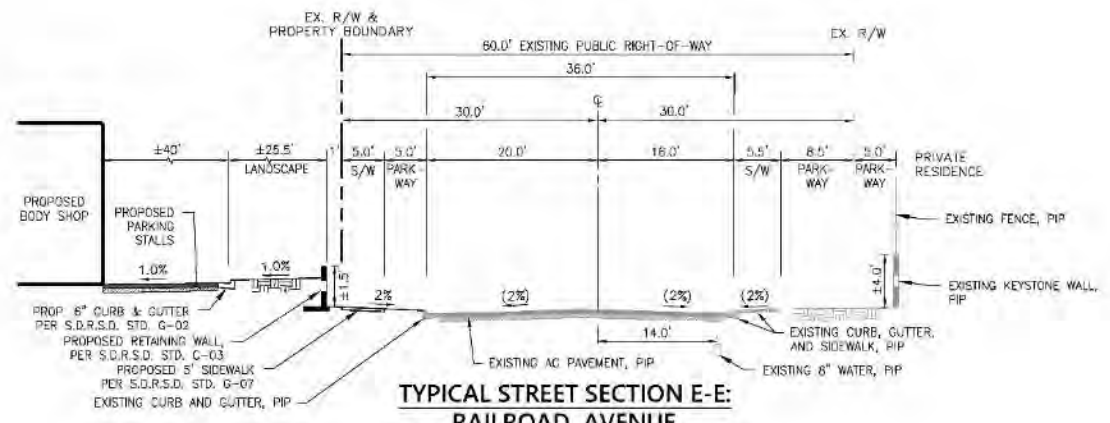
**TYPICAL STREET SECTION D-D:
MISSION GORGE ROAD**
N.T.S.

NOTE:
REPLACE FAILED OR INADEQUATE PAVEMENT TO THE CENTERLINE AND/OR SIDEWALK ADJACENT TO THE SITE ON MISSION GORGE ROAD TO THE SATISFACTION OF THE DIRECTOR OF DEVELOPMENT SERVICES



**TYPICAL STREET SECTION B-B:
MISSION GORGE ROAD**
N.T.S.

NOTE:
REPLACE FAILED OR INADEQUATE PAVEMENT TO THE CENTERLINE AND/OR SIDEWALK ADJACENT TO THE SITE ON MISSION GORGE ROAD TO THE SATISFACTION OF THE DIRECTOR OF DEVELOPMENT SERVICES



**TYPICAL STREET SECTION E-E:
RAILROAD AVENUE**
N.T.S.

NOTE:
REPLACE FAILED OR INADEQUATE PAVEMENT TO THE CENTERLINE AND/OR SIDEWALK ADJACENT TO THE SITE ON RAILROAD AVENUE TO THE SATISFACTION OF THE DIRECTOR OF DEVELOPMENT SERVICES



Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
SanDiego, CA 92071

Date Issued For
12/18/2022 CUP Submittal
06/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal



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619.291.0700
RICK@19.791-4165
Riverside - Sacramento - San Luis Obispo
Orange - Denver - Jackson - Phoenix
rickengineering.com



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92121 USA
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8022-08006-004

STREET
SECTIONS
C-002

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Cameron Brothers
Company LLC

Santee Auto
Center

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SanDiego, CA 92171

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08/11/2023 CUP Resubmittal

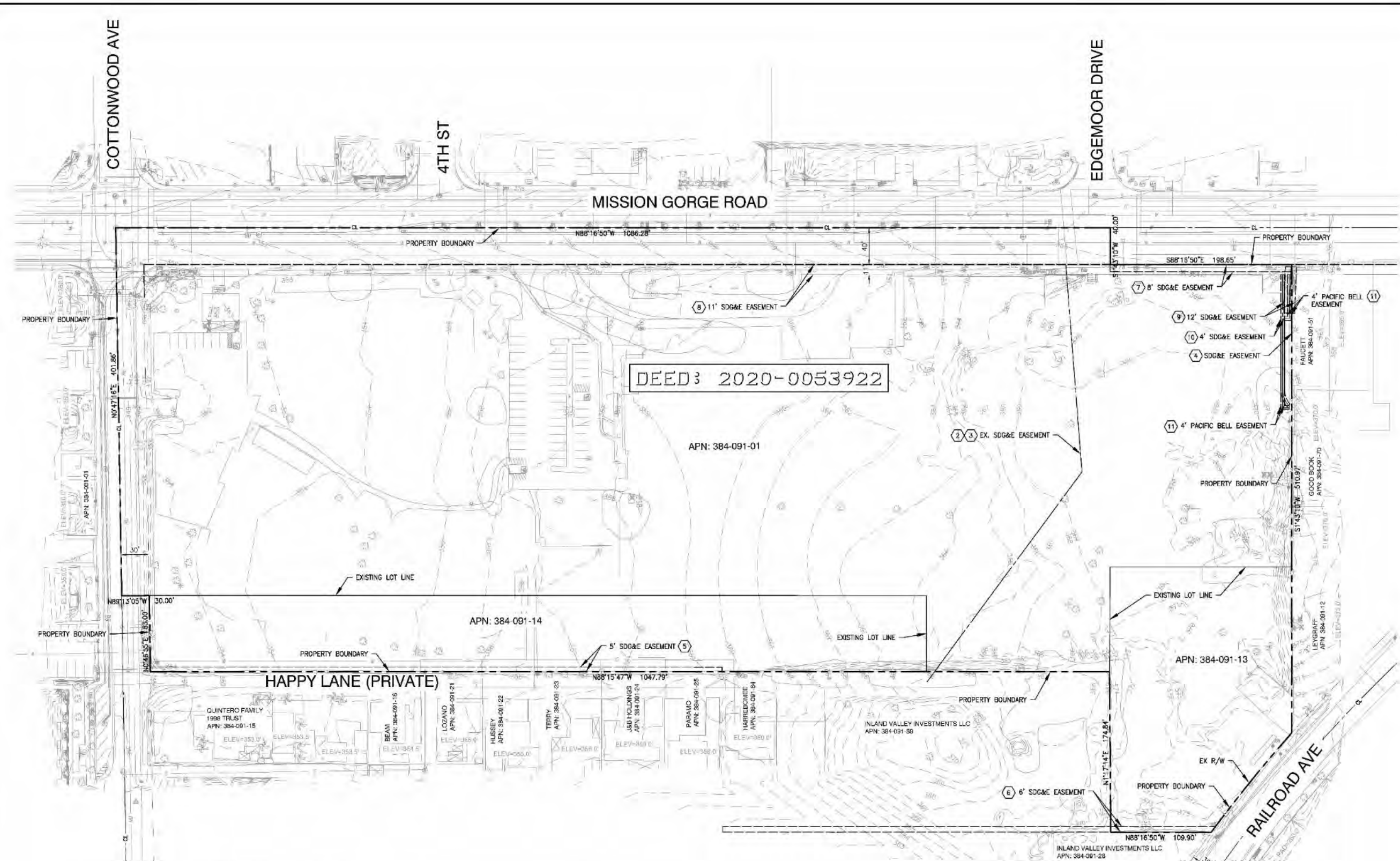
RICK
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(FAX) 619.291.4165
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EXISTING
CONDITIONS
C-003



DEED 2020-0053922

APN: 384-091-01

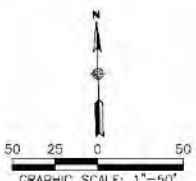
APN: 384-091-14

APN: 384-091-13

INLAND VALLEY INVESTMENTS LLC
APN: 384-091-28

EASEMENT NOTES

- (2) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 07-06-1937, IN BK 656, PG 499 TO BE VACATED
- (3) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 05-12-1937, IN BK 655, PG 71 TO BE VACATED
- (4) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 10-18-1938, IN BK 830, PG 205 TO REMAIN
- (5) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 09-27-1960, DOC. 193847 TO REMAIN
- (6) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 11-28-1961, DOC. 205695 TO REMAIN
- (7) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 03-25-1962, DOC. 50648 TO REMAIN
- (8) COUNTY OF SAN DIEGO PUBLIC STREET & HWY EASEMENT, RECORDED 04-31-1973, 73-087395 TO REMAIN
- (9) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 08-21-1973, 73-0234303 TO REMAIN
- (10) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 07-10-1984, 84-0258770 TO REMAIN
- (11) PACIFIC BELL EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 08-13-1984, 84-0307390 TO REMAIN



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CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10355 Mission Gorge Road
Santee, CA 92071

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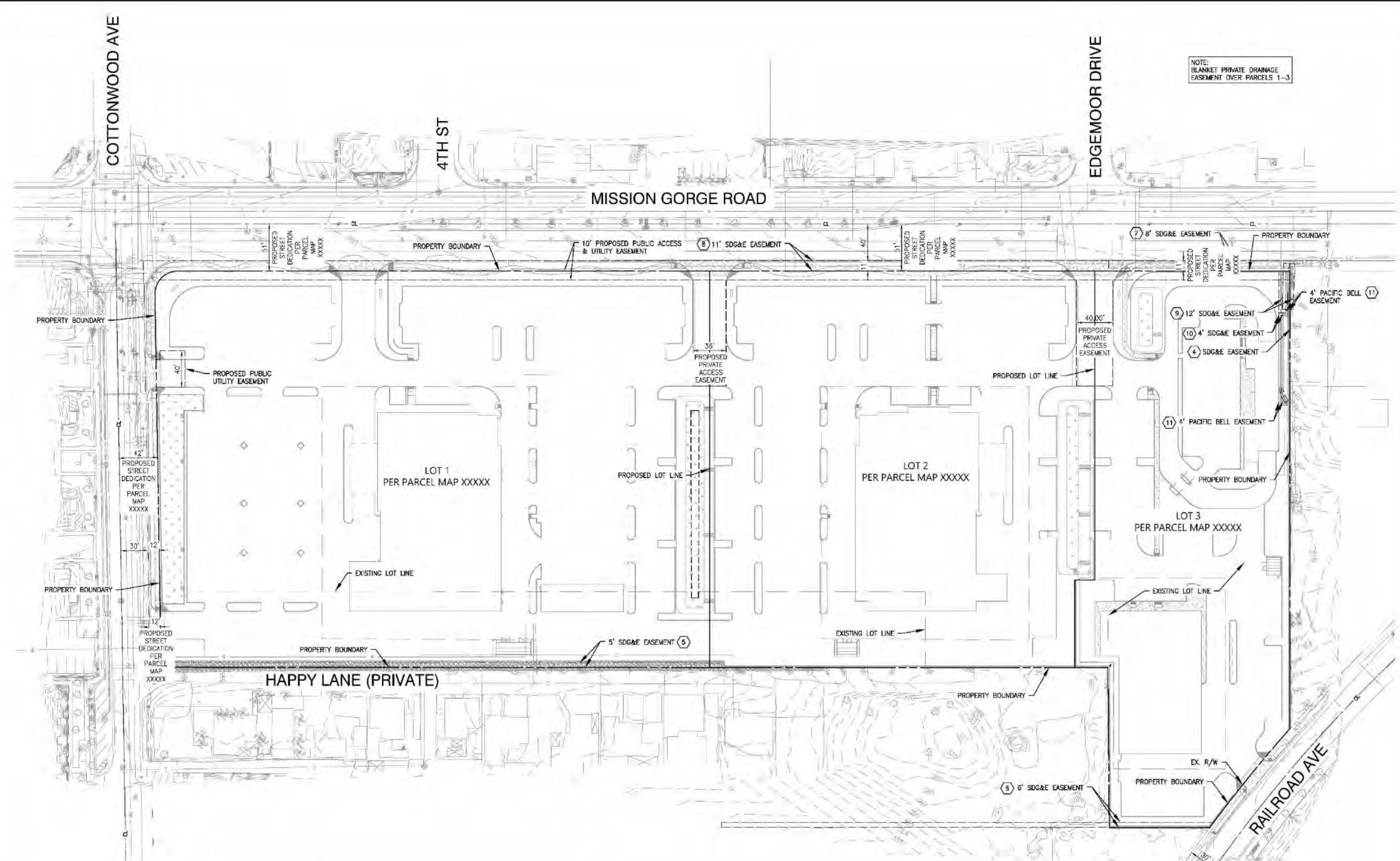
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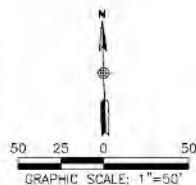
PROPOSED
MAP EXHIBIT

C-004

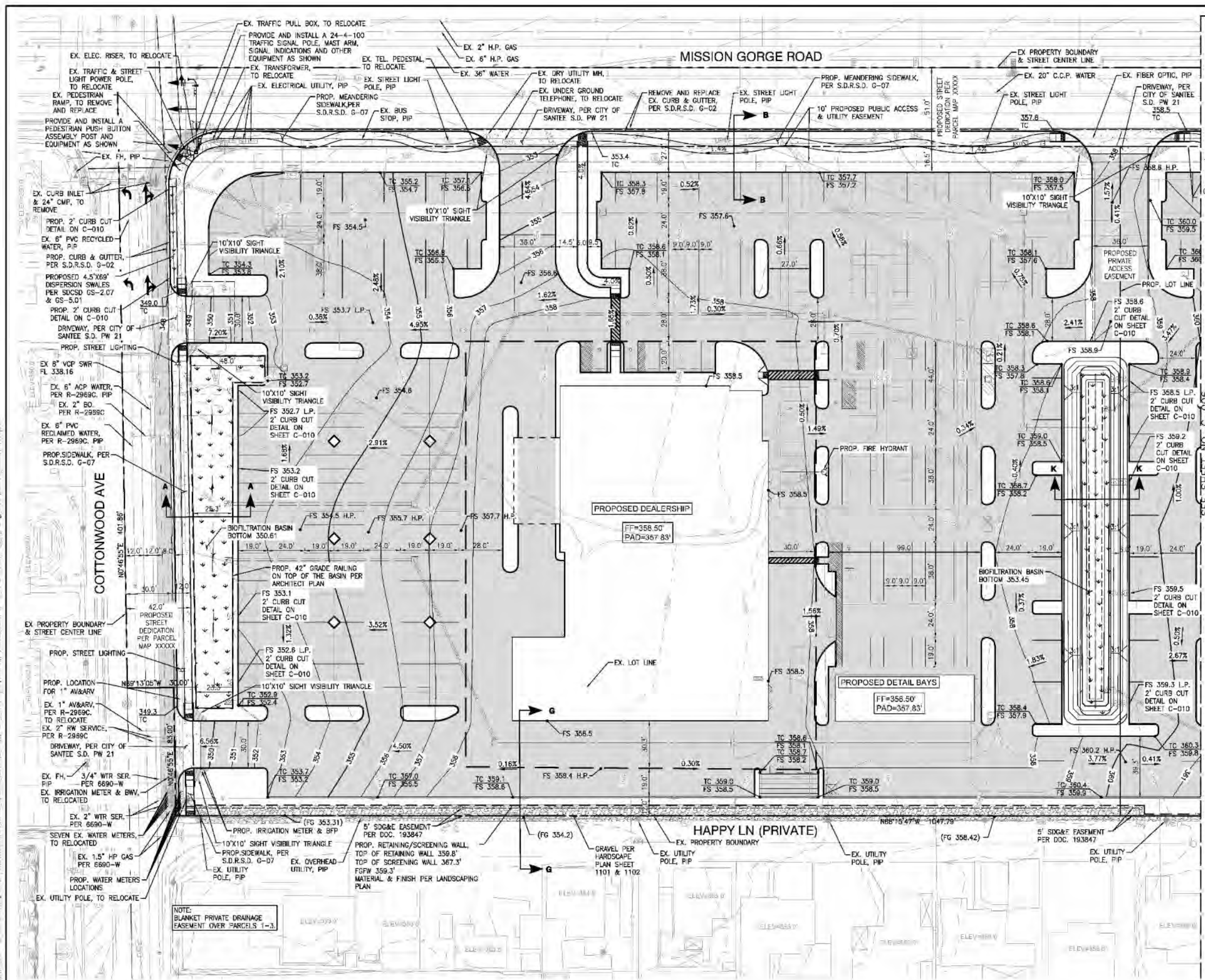


EASEMENT NOTES

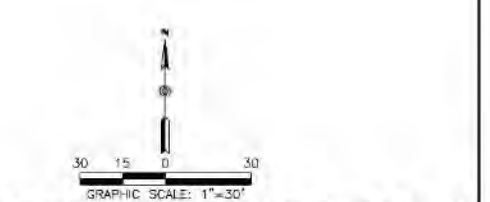
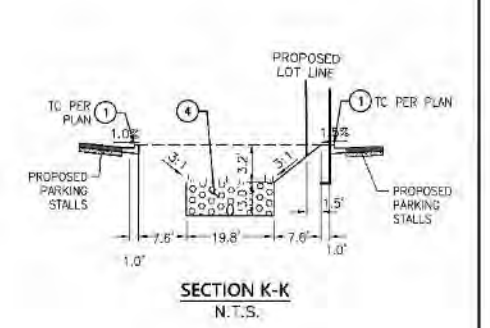
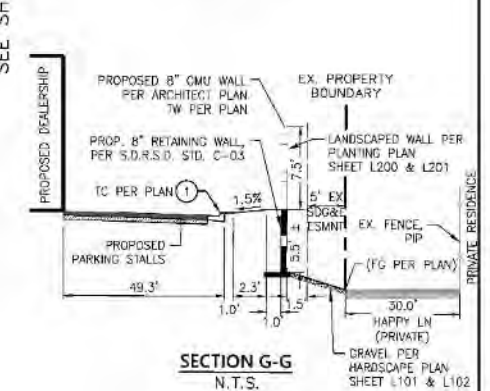
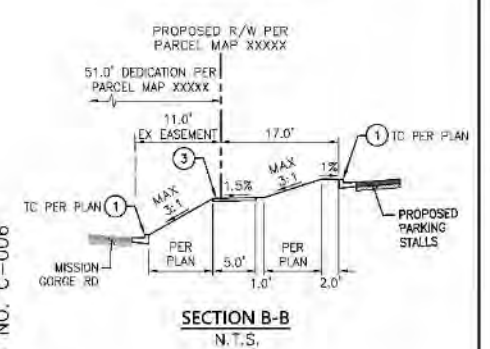
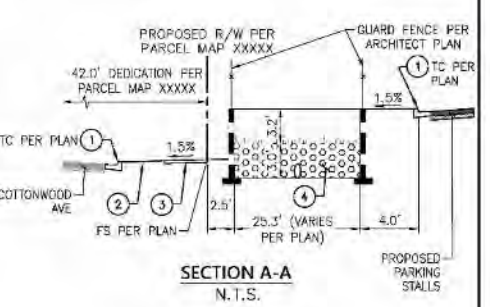
- (2) NOT USED
- (3) NOT USED
- (4) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 10-18-1938, IN BK 830, PG 205 TO REMAIN
- (5) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 08-27-1960, DOC. 193847 TO REMAIN
- (6) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 11-28-1961, DOC. 205695 TO REMAIN
- (7) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 03-26-1962, DOC. 50648 TO REMAIN
- (8) COUNTY OF SAN DIEGO PUBLIC STREET & HWY EASEMENT, RECORDED 04-31-1973, 73-087395 TO REMAIN
- (9) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 08-21-1973, 73-0234303 TO REMAIN
- (10) SDG&E EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 07-10-1984, 84-0258770 TO REMAIN
- (11) PACIFIC BELL EASEMENT FOR PUBLIC UTILITIES, INGRESS & EGRESS, RECORDED 08-13-1984, 84-0307390 TO REMAIN



**FOR REFERENCE ONLY
NOT FOR CONSTRUCTION**



- CONSTRUCTION NOTE:
1. PROP. 6" CURB & GUTTER PER S.D.R.S.D. STD. G-02
 2. PROP. 5" PARKWAY
 3. PROP. 5" SIDEWALK PER S.D.R.S.D. STD. C-07
 4. BIOFILTRATION BASIN, SEE DETAIL ON SHEET C-010



Cameron Brothers Company LLC

Santee Auto Center

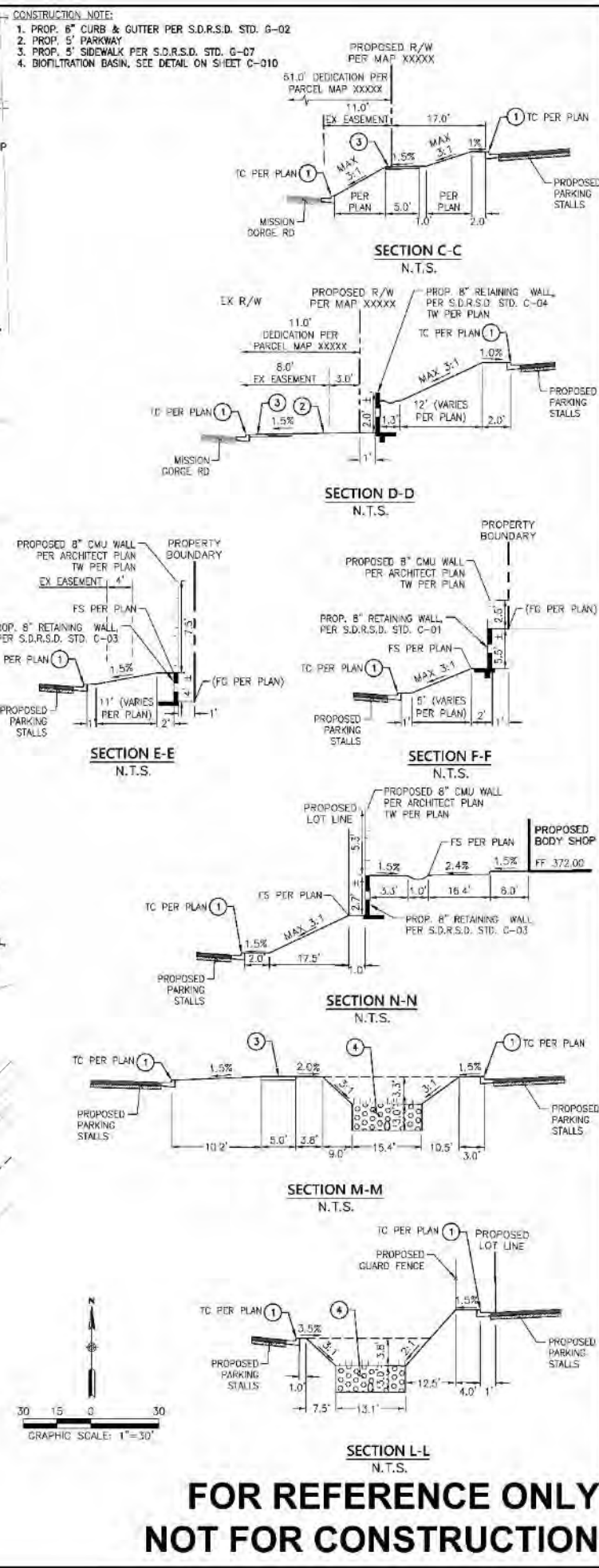
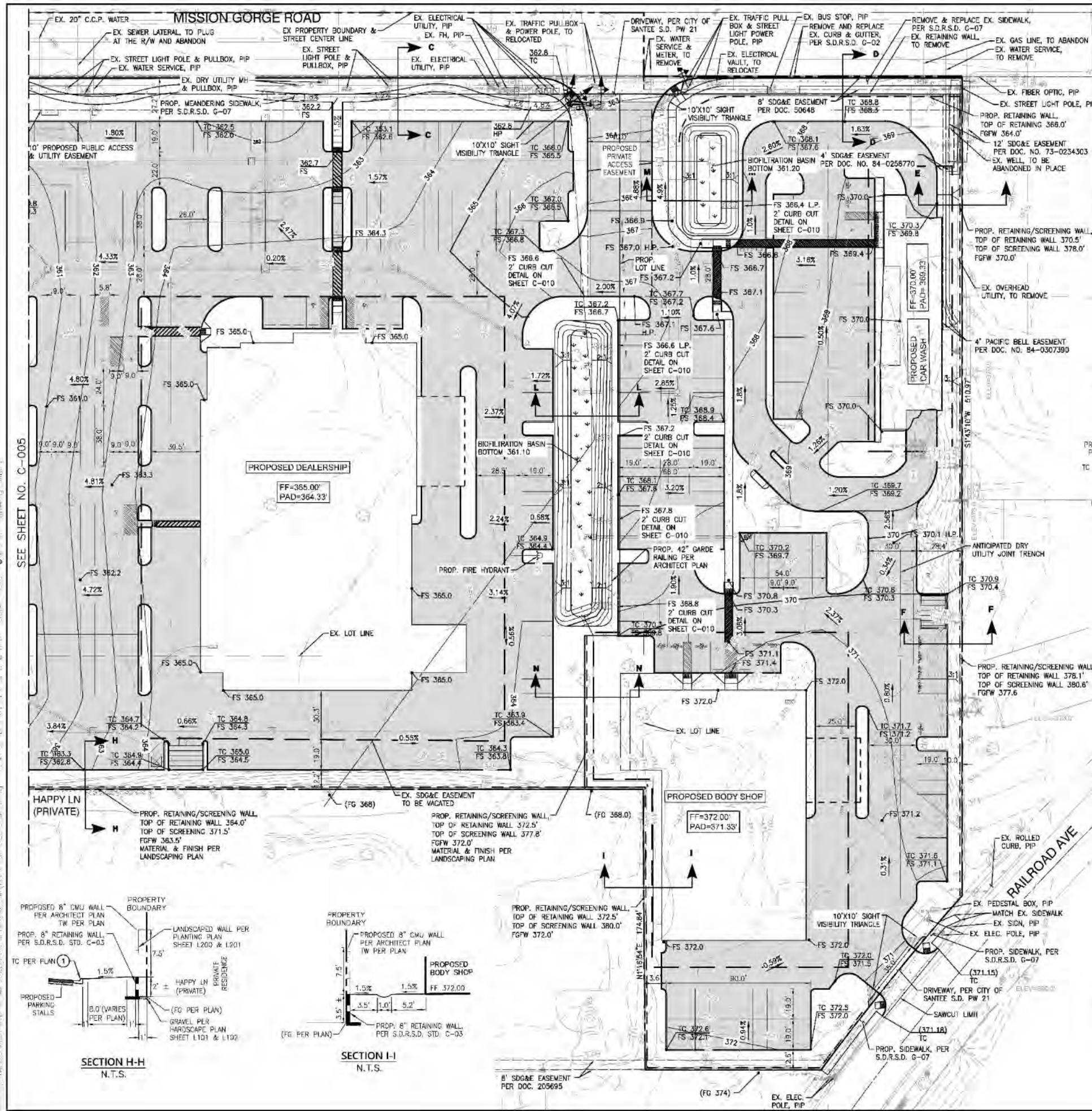
10335 Mission Gorge Road
San Jose, CA 95071

Date Issued For
12/18/2022 CUP Submittal
06/23/2023 CUP Re-submittal
08/11/2023 CUP Re-submittal



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SITE GRADING PLAN
C-005



Cameron Brothers Company LLC

Santee Auto Center

10335 Mission Gorge Road
San Luis Obispo, CA 93071

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8022-02035-004

SITE GRADING PLAN
C-006

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Cameron Brothers Company LLC

Santee Auto Center

10935 Mission Gorge Road
Santee, CA 92071

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08/11/2023 CUP Resubmittal

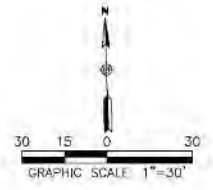
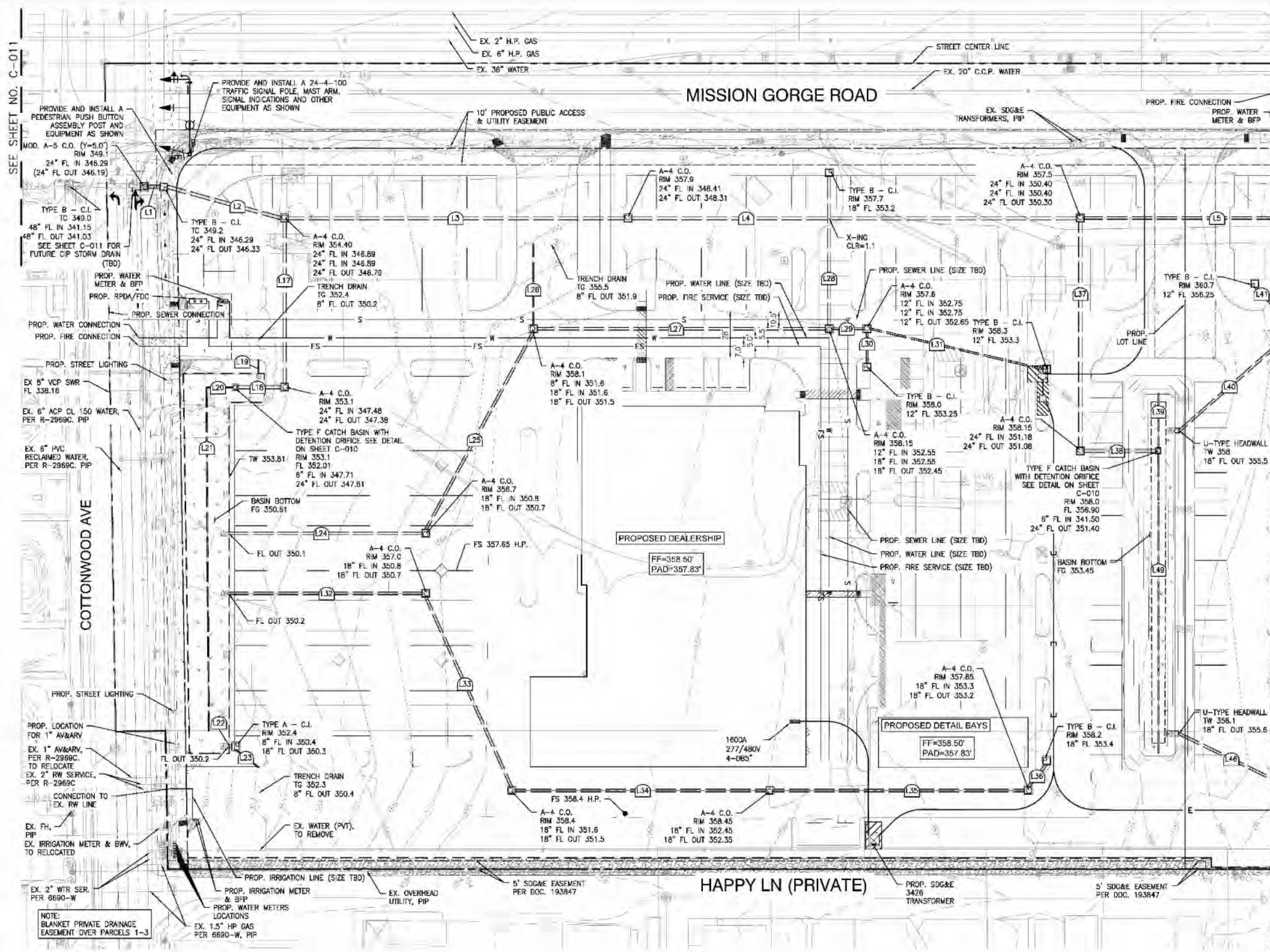


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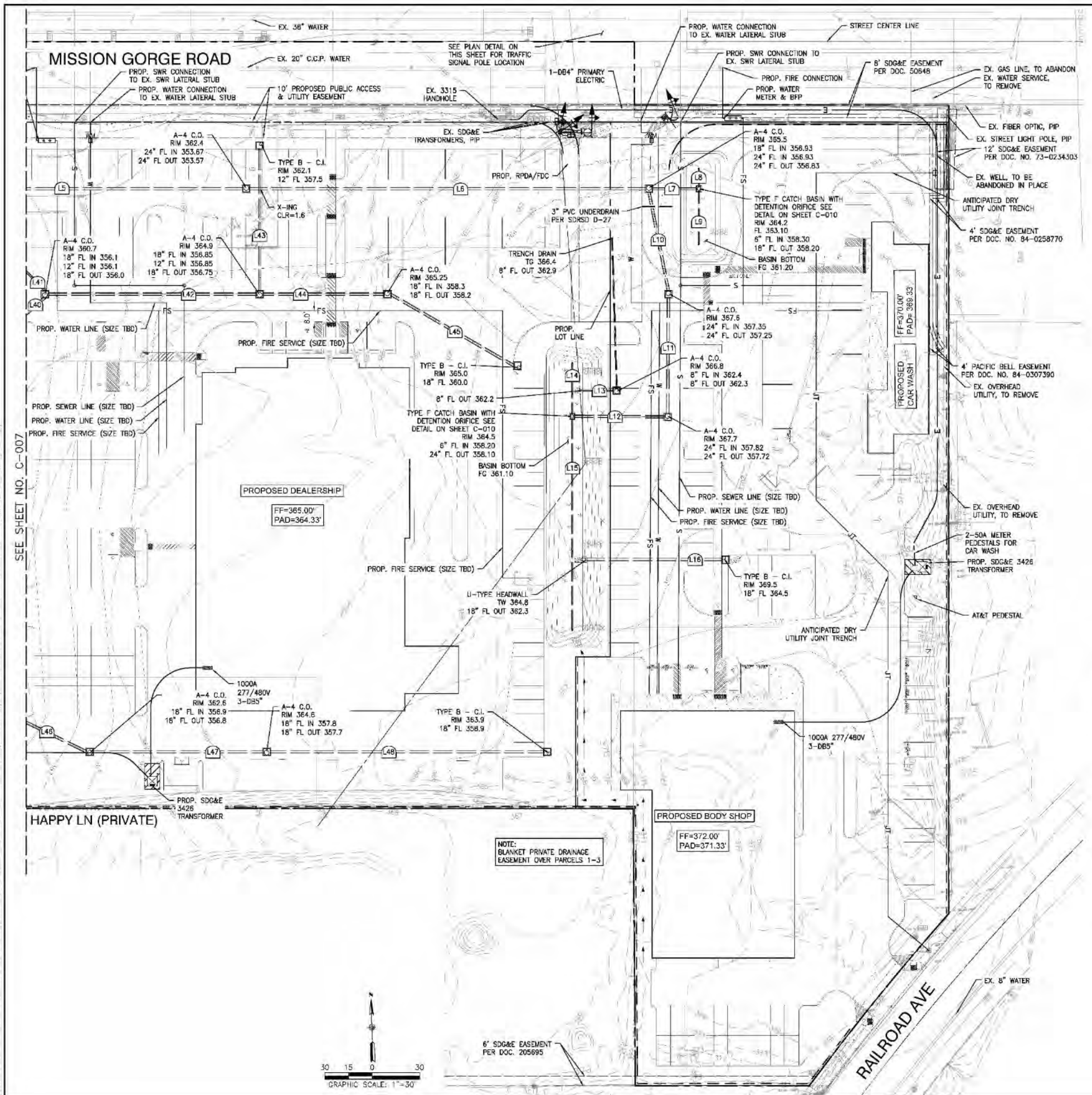
STORM DRAIN DATA (PVT)				
LINE #	LENGTH	BEARING	REMARK	SLOPE (%)
L1	7.32	N88°07'49"W	24" PVC	0.5
L2	71.14	N72°58'15"W	24" PVC	0.5
L3	202.53	N88°17'00"W	24" PVC	0.7
L4	288.22	N88°17'00"W	24" PVC	0.7
L5	250.37	N88°17'17"W	24" PVC	1.3
L17	97.82	S1°43'00"W	8" PVC	0.5
L18	26.34	N88°25'15"W	18" PVC	0.5
L19	8.87	S1°43'10"W	8" PVC	2.2
L20	15.87	N88°42'51"W	6" PERF. PVC	0.1
L21	206.49	S1°19'37"W	6" PERF. PVC	0.1
L22	2.25	S88°16'50"E	24" PVC	4.0
L23	17.42	S50°31'06"E	8" PVC	0.5
L24	116.50	N88°16'50"W	24" PVC	0.5
L25	135.46	S30°15'46"W	18" PVC	0.5
L26	49.02	S1°43'05"W	8" PVC	0.5
L27	173.97	S88°16'50"E	18" PVC	0.5
L28	89.75	S1°43'10"W	18" PVC	0.7
L29	18.89	S88°16'50"E	12" PVC	0.5
L30	19.01	N1°42'59"E	12" PVC	3.3
L31	106.79	N75°09'24"W	12" PVC	0.5
L32	116.50	N88°16'50"W	24" PVC	0.5
L33	125.63	N22°34'33"W	18" PVC	0.5
L34	151.41	N88°17'01"W	18" PVC	0.5
L35	151.40	N88°17'01"W	18" PVC	0.5
L36	17.78	S40°24'33"W	18" PVC	0.5
L37	135.92	S1°43'10"W	24" PVC	0.5
L38	43.78	N88°16'49"W	24" PVC	0.5
L39	32.93	S1°43'04"W	6" PERF. PVC	0.1
L40	90.03	S50°26'44"W	24" PVC	0.5
L41	32.24	S42°07'55"E	12" PVC	0.5
L46	104.61	N62°22'33"W	18" PVC	1.0
L49	179.88	S1°42'32"W	6" PERF. PVC	0.1



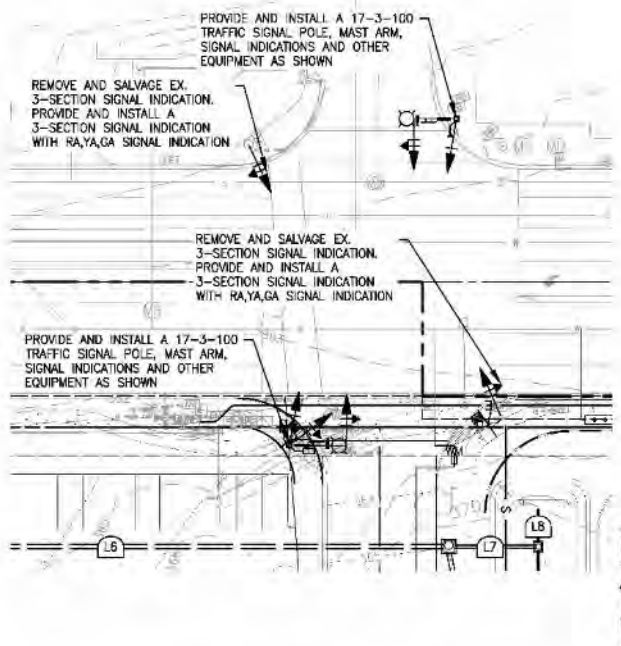
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UTILITY PLAN

C-007



STORM DRAIN DATA (PVT)				
LINE #	LENGTH	BEARING	REMARK	SLOPE (%)
L5	250.37	N88°17'17"W	24" PVC	1.3
L6	250.37	N88°17'17"W	18" PVC	1.3
L7	27.89	S88°16'33"E	12" PVC	4.5
L8	11.13	S1°43'07"W	6" PERF. PVC	0.1
L9	33.58	S1°43'07"W	8" PERF. PVC	0.1
L10	63.74	N9°23'57"W	18" PVC	0.5
L11	73.53	S2°05'27"W	18" PVC	0.5
L12	56.96	S87°54'33"E	18" PVC	0.5
L13	19.49	N88°17'01"W	8" PVC	0.5
L14	32.15	N1°42'59"E	6" PERF. PVC	0.1
L15	133.69	N1°42'59"E	8" PERF. PVC	0.1
L16	85.72	N88°17'00"W	18" PVC	2.5
L40	90.03	S50°26'44"W	24" PVC	0.5
L41	32.24	S42°07'55"E	12" PVC	0.5
L42	131.64	S88°16'50"E	18" PVC	0.5
L43	89.75	S1°43'10"W	12" PVC	0.7
L44	76.80	S88°16'50"E	18" PVC	0.5
L45	90.29	N58°11'11"W	18" PVC	1.9
L46	104.61	N62°22'33"W	18" PVC	1.0
L47	107.70	S88°16'50"E	18" PVC	0.7
L48	173.36	S88°16'50"E	18" PVC	0.7



PLAN DETAIL:
EDGEMOOR DR & MISSION GORGE RD
SCALE: 1" = 30'

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Cameron Brothers
Company LLC

Santee Auto
Center

10235 Mission Gorge Road
Santee, CA 92071

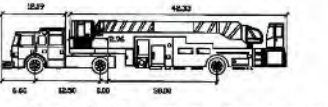
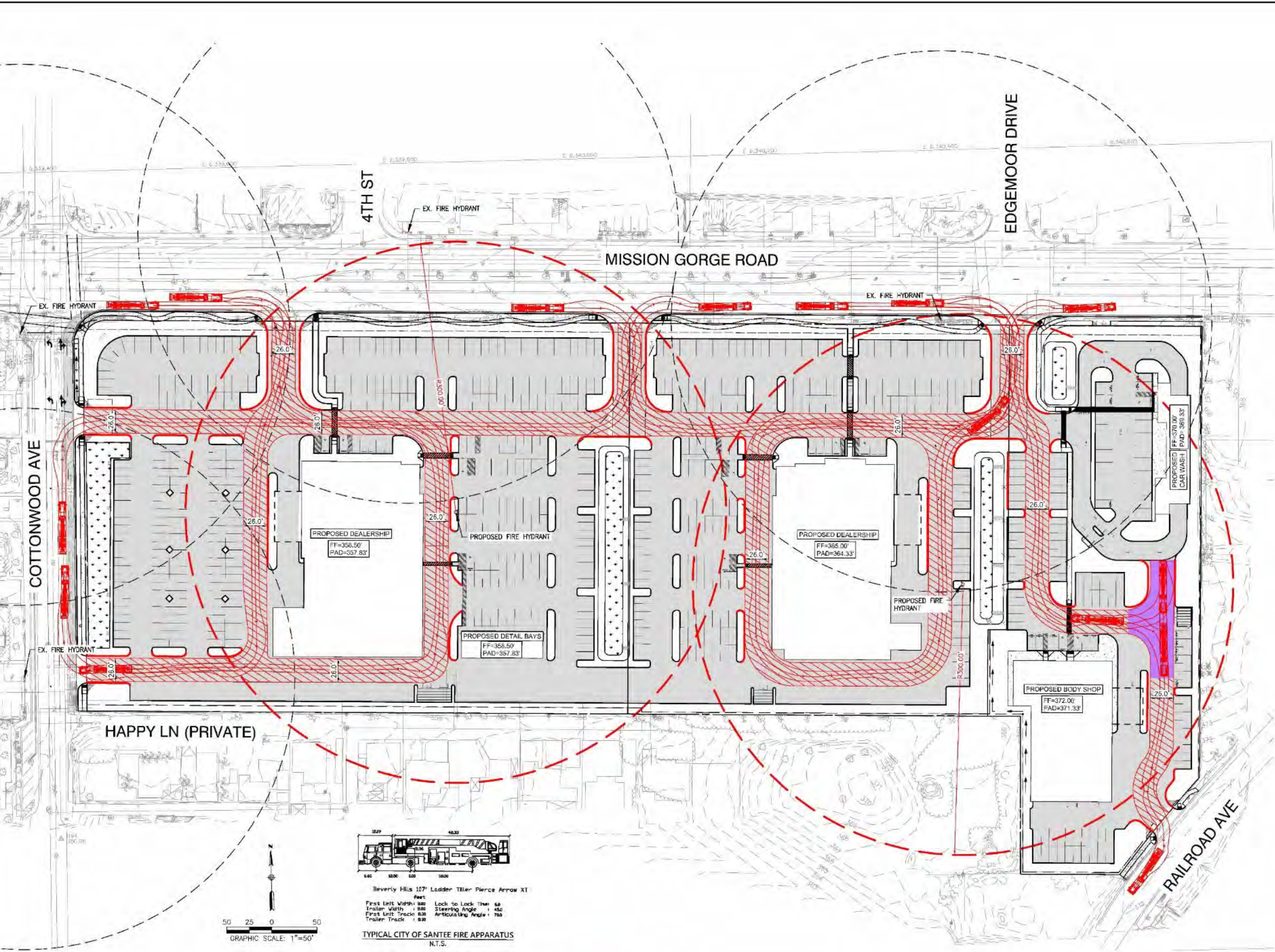
Date Issued For
12/19/2022 CUP Submittal
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RD22-CR036-004

UTILITY PLAN

C-008



Beverly Hills 107' Ladder Tiller Pierce Arrow XT
 First Unit Width: 80' Lock to Lock Turn: 60'
 First Unit Track: 8.00' Steering Angle: 45°
 Trailer Width: 8.00' Articulating Angle: 75°
 Trailer Track: 8.00'

TYPICAL CITY OF SANTEE FIRE APPARATUS
 N.T.S.

LEGEND:
 FIRE ACCESS LANE
 FIRE TRUCK HAMMER HEAD OR TURNAROUND LIMITS
 RED CURB PER FPB POLICY A-14-1



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CAMERON
 BROTHERS COMPANY LLC

Cameron Brothers
 Company LLC

Santee Auto
 Center

10335 Mission Gorge Road
 Santee, CA 92071

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(658) 398-3900
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 B022-CB038-004

FIRE ACCESS

C-009



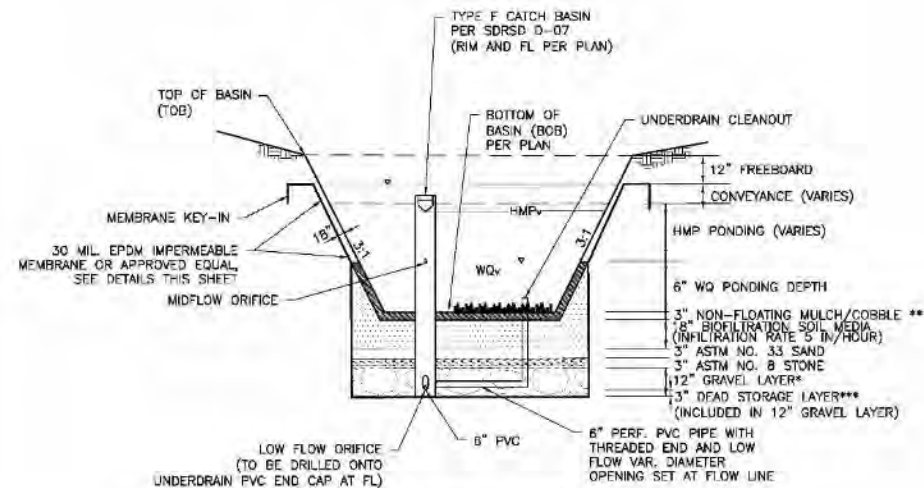
CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

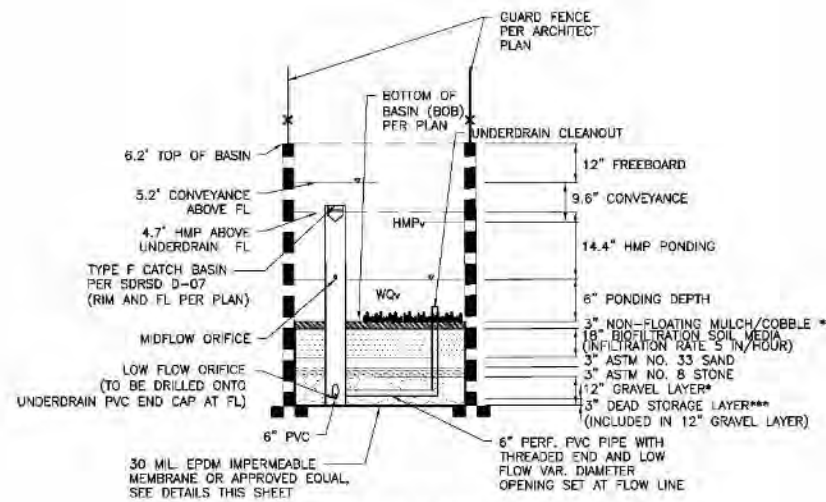
10935 Mission George Road
Santee, CA 92071

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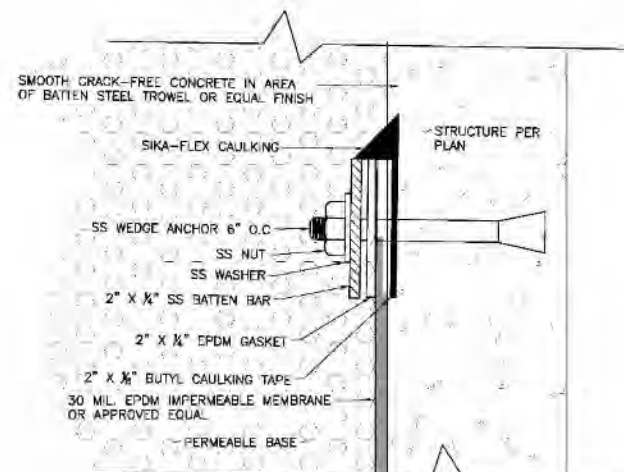
- * GRAVEL LAYER = ASTM NO. 57 STONE, PROVIDE 4" MIN. AROUND INLETS.
- ** NON FLOATING SHREDDED HARDWOOD (MULCH/COBBLE)
- *** 3" DEAD STORAGE LAYER INCLUDED IN 12" GRAVEL LAYER

BIOFILTRATION BASIN (PVT)
NOT TO SCALE

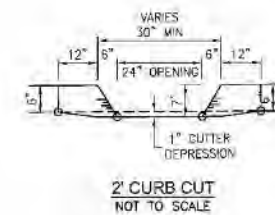


- * GRAVEL LAYER = ASTM NO. 57 STONE, PROVIDE 4" MIN. AROUND INLETS.
- ** NON FLOATING SHREDDED HARDWOOD (MULCH/COBBLE)
- *** 3" DEAD STORAGE LAYER INCLUDED IN 12" GRAVEL LAYER

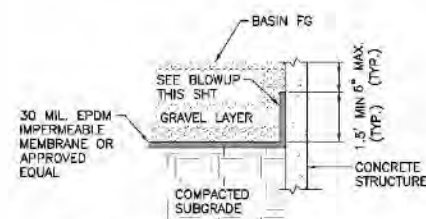
BIOFILTRATION BASIN W/ WALL (PVT)
NOT TO SCALE



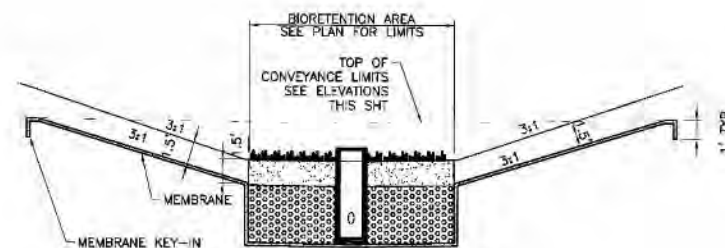
MEMBRANE CONNECTION DETAIL BLOWUP (PVT)
NOT TO SCALE



2' CURB CUT
NOT TO SCALE



MEMBRANE CONNECTION DETAIL (PVT)
NOT TO SCALE



MEMBRANE KEY-IN DETAIL (PVT)
NOT TO SCALE

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DETAILS

C-010

3D22-CB009-004

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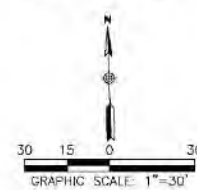
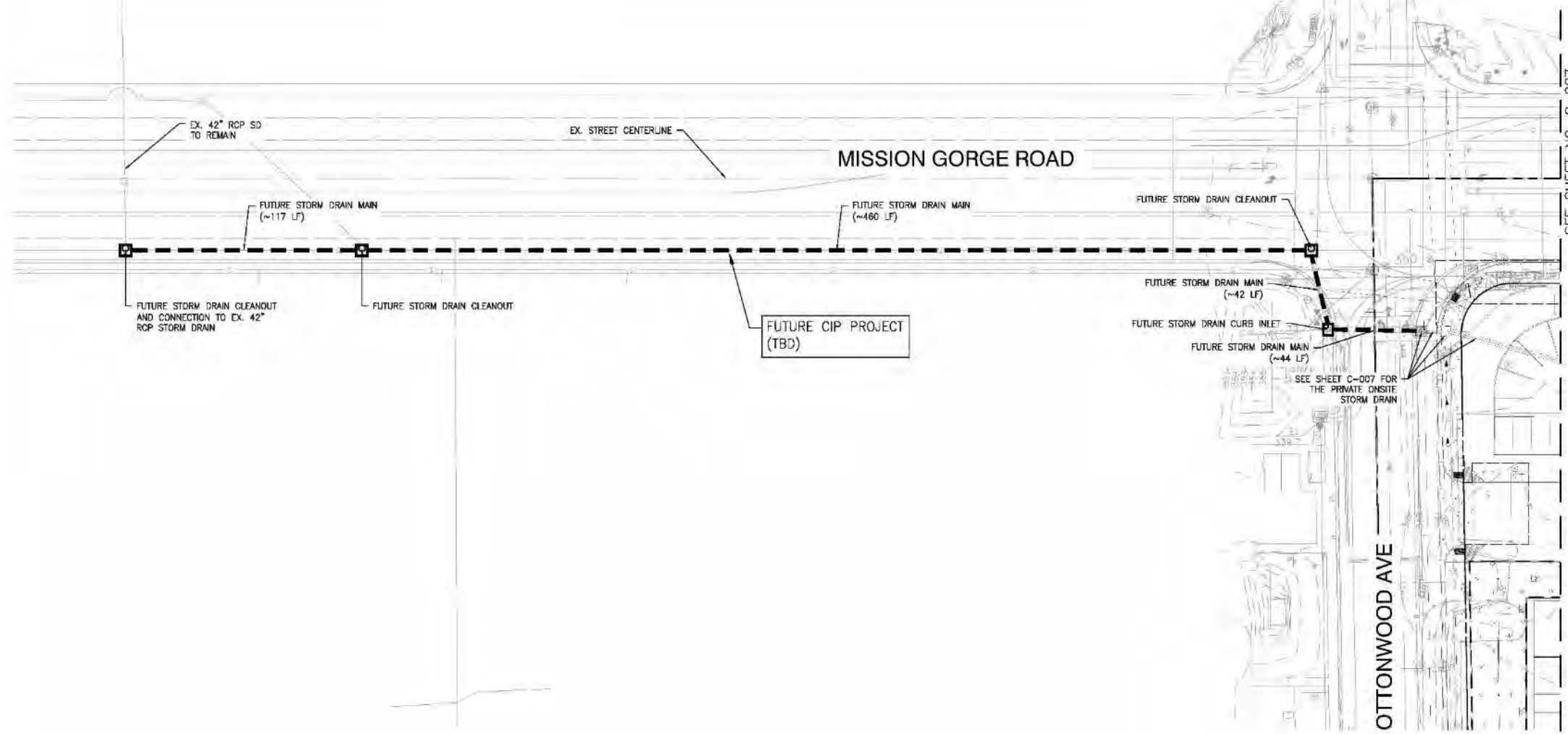


Cameron Brothers Company LLC

Santee Auto Center

10355 Mission Gorge Road
Santee, CA 92071

Date	Issued For
12/19/2022	CUP Submittal
06/23/2023	CUP Resubmittal
08/11/2023	CUP Resubmittal



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11700 Sorrento Valley Rd
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San Diego, California
92121 USA
(858) 368-3600
WWW.HED.DESIGN

©2022
B022-08098-004
**OFFSITE
DRAINAGE
PLAN
C-011**

PRELIMINARY LANDSCAPE PLANS: SANTEE AUTO CENTER

10335 MISSION GORGE ROAD, SANTEE, CA 92071



CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CUP Submit
05/23/2023 CUP Resubmit
08/11/2023 CUP Resubmit

NOTES	ABBREVIATIONS	OWNER	SHEET INDEX		
<p>GENERAL NOTES</p> <p>A. VISIT SITE PRIOR TO SUBMITTING BIDS.</p> <p>B. VERIFY ALL EXISTING CONDITIONS, DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH THE WORK. NOTIFY LANDSCAPE ARCHITECT IMMEDIATELY SHOULD FIELD CONDITIONS VARY FROM THOSE SHOWN ON PLANS.</p> <p>C. DO NOT SCALE DRAWINGS</p> <p>D. ALL WORK CONSTRUCTION AND MATERIALS SHALL COMPLY WITH ALL PROVISIONS OF THE LATEST BUILDING CODE AND WITH OTHER RULES, REGULATIONS AND ORDINANCES GOVERNING THE LOCATION OF THE WORK. BUILDING CODE REQUIREMENTS TAKE PRECEDENCE OVER THE DRAWINGS AND IT SHALL BE THE RESPONSIBILITY OF ANYONE SUPPLYING LABOR OR MATERIALS OR BOTH TO BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT ANY DISCREPANCIES OR CONFLICT BETWEEN THE REQUIREMENTS OF THE CODE AND THE DRAWINGS.</p> <p>E. REFERENCE TO ANY DETAIL OR DRAWING IS FOR CONVENIENCE ONLY AND DOES NOT LIMIT THE APPLICATION OF SUCH DETAIL OR DRAWINGS</p> <p>F. DISCREPANCIES IN THE DRAWINGS OR BETWEEN THE DRAWINGS AND ACTUAL FIELD CONDITIONS SHALL BE REPORTED TO THE LANDSCAPE ARCHITECT. CORRECTED DRAWINGS OR INSTRUCTIONS SHALL BE ISSUED PRIOR TO THE CONTINUATION OF THIS WORK. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR ALL NECESSARY CORRECTIONS DUE TO FAILURE TO REPORT KNOWN DISCREPANCIES.</p> <p>G. GENERAL CONTRACTOR IS RESPONSIBLE FOR ALL WORK SHOWN ON THESE DRAWINGS AND SPECIFICATIONS UNLESS SPECIFICALLY NOTED OTHERWISE.</p> <p>H. THE CONTRACT DRAWINGS AND SPECIFICATIONS REPRESENT THE FINISHED STRUCTURE UNLESS OTHERWISE SHOWN. THEY DO NOT INDICATE THE METHOD OF CONSTRUCTION. THE GENERAL CONTRACTOR SHALL SUPERVISE AND DIRECT THE WORK AND HE SHALL BE SOLELY RESPONSIBLE FOR ALL CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES AND PROCEDURES. OBSERVATION VISITS TO THE SITE BY FIELD REPRESENTATIVES OF THE LANDSCAPE ARCHITECT SHALL NOT INCLUDE INSPECTIONS OF THE PROTECTIVE MEASURES OR THE CONSTRUCTION PROCEDURES REQUIRED FOR SAME, WHICH ARE THE SOLE RESPONSIBILITY OF THE GENERAL CONTRACTOR. ANY SUPPORT SERVICES PERFORMED BY THE LANDSCAPE ARCHITECT DURING CONSTRUCTION SHALL BE DISTINGUISHED FROM CONTINUOUS AND DETAILED INSPECTION SERVICES WHICH ARE FURNISHED BY OTHERS. THESE SUPPORT SERVICES PERFORMED BY THE LANDSCAPE ARCHITECT, WHETHER OF MATERIAL OR OTHER, PERFORMED BEFORE, DURING OR AFTER COMPLETION OF CONSTRUCTION ARE PERFORMED SOLELY FOR THE PURPOSE OF ASSISTING IN QUALITY CONTROL AND IN ACHIEVING CONFORMANCE WITH CONTRACT DRAWINGS AND SPECIFICATIONS, BUT THEY DO NOT GUARANTEE GENERAL CONTRACTOR'S PERFORMANCE AND SHALL NOT BE CONSTRUED AS SUPERVISION OF CONSTRUCTION.</p> <p>I. A PROTECTION FENCE SHALL BE CONSTRUCTED AND MAINTAINED DURING CONSTRUCTION CONFORMING TO THE REQUIREMENTS OF THE BUILDING CODE.</p> <p>J. MAINTAIN SANITARY TOILET FACILITIES DURING CONSTRUCTION AS REQUIRED BY APPLICABLE REGULATIONS.</p> <p>K. THE GENERAL CONTRACTOR WARRANTS TO THE OWNER AND THE LANDSCAPE ARCHITECT THAT ALL MATERIALS AND EQUIPMENT FURNISHED WILL BE NEW UNLESS OTHERWISE SPECIFIED AND THAT ALL WORK WILL BE OF GOOD QUALITY, FREE FROM FAULTS AND DEFECTS.</p> <p>L. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS WORK AND/OR EQUIPMENT SUPPLIED BY THE OWNER.</p> <p>M. PROVIDE FACILITIES FOR THE PHYSICALLY HANDICAPPED IN ACCORDANCE WITH C.A.C. TITLE 24 AND AS REQUIRED BY THE LATEST VERSION OF THE CALIFORNIA BUILDING CODE</p> <p>N. IT SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO LOCATE ALL EXISTING UTILITIES WHETHER SHOWN HEREON OR NOT AND TO PROTECT THEM FROM DAMAGE. THE GENERAL CONTRACTOR SHALL BEAR ALL EXPENSE OF REPAIR OR REPLACEMENT IN CONJUNCTION WITH THE EXECUTION OF THIS WORK.</p> <p>O. HAVING MASONRY AND CONCRETE SUBCONTRACTORS ARE TO COORDINATE WITH THE ELECTRICIAN, DRAINLINE, SUEVIC CONTRACTOR AND IRRIGATION SUBCONTRACTOR FOR SLEEVING, PIPING AND/OR CONDUIT INSTALLATION UNDER OR THROUGH HARDSCAPE ELEMENTS.</p> <p>P. VERIFY ALL PROPERTY LINES OR OTHER LIMIT OF WORK LINES PRIOR TO COMMENCING WORK</p> <p>Q. IN THE CASE OF DISCREPANCIES IN THE DRAWINGS, SPECIFICATIONS TAKE PRECEDENCE OVER DETAILS, AND DETAILS TAKE PRECEDENCE OVER PLANS.</p> <p>R. SUBSTITUTIONS MUST BE APPROVED IN WRITING BY THE OWNER.</p> <p>S. THE GENERAL CONTRACTOR SHALL ENSURE THAT ALL TRADES ARE PROVIDED WITH CURRENT DRAWINGS AND SPECIFICATIONS APPROVED FOR CONSTRUCTION. DO NOT ALLOW DOCUMENTS NOT APPROVED FOR CONSTRUCTION TO BE USED IF SEEN ON SITE.</p> <p>T. REPAIR OR REPLACE ANY DAMAGE TO ADJACENT PROPERTIES, CURBS, WALKS, PLANTING, WALLS, ETC. AT NO ADDITIONAL COST TO THE OWNER.</p> <p>U. LOCATIONS OF N.I.C. CONSTRUCTION ELEMENTS SUCH AS LIGHTS, SIGNS, VENTS, HYDRANTS, TRANSFORMERS, ETC. ARE APPROXIMATE. NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY SHOULD THE LOCATION OF THESE ITEMS INTERFERE WITH THE PROPER EXECUTION OF WORK.</p> <p>V. PROVIDE THE OWNER WITH ALL WARRANTIES, GUARANTEES, AND INSTRUCTION MANUALS FOR EQUIPMENT, APPLIANCES, FIXTURES, ETC. AS DESCRIBED IN THE SPECIFICATIONS.</p> <p>W. NOTIFY THE CITY'S AUTHORIZED REPRESENTATIVE 48 HOURS PRIOR TO COMMENCEMENT OF WORK TO COORDINATE PROJECT INSPECTION SCHEDULES</p> <p>X. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS REQUIRED TO PERFORM THE WORK INDICATED HEREIN BEFORE BEGINNING WORK.</p> <p>Y. CONTRACTOR SHALL OBTAIN THE PERTINENT ENGINEERING OR ARCHITECTURAL PLANS BEFORE BEGINNING WORK.</p> <p>Z. ALL LANDSCAPE AND IRRIGATION SHALL CONFORM TO THE STANDARDS OF THE CITY-WIDE LANDSCAPE REGULATIONS, SANTEE MUNICIPAL CODE 13.36, AND THE CITY OF SANTEE LAND DEVELOPMENT MANUAL, LANDSCAPE STANDARDS AND ALL OTHER LANDSCAPE RELATED CITY AND REGIONAL STANDARDS.</p> <p>AA. ALL LANDSCAPED AREAS SHALL HAVE A BREATHABLE WEED BARRIER.</p>	<p>@ AT</p> <p>CL CENTERLINE</p> <p># NUMBER</p> <p>AC ASPHALTIC CONCRETE</p> <p>CF CUBIC FOOT</p> <p>CLR CLEAR</p> <p>CONC CONCRETE</p> <p>CTR CENTER</p> <p>DIA DIAMETER</p> <p>DIM DIMENSION</p> <p>EJ EXPANSION JOINT</p> <p>EQ EQUAL</p> <p>E.W. EACH WAY</p> <p>EX. EXISTING</p> <p>FG FINISH GRADE</p> <p>FS FINISH SURFACE</p> <p>GA GAUGE</p> <p>GALV GALVANIZED</p> <p>HORIZ HORIZONTAL</p> <p>HT HEIGHT</p> <p>ID INSIDE DIAMETER</p> <p>INCL INCLUDING</p> <p>INV INVERT ELEVATION</p> <p>L.O.W. LIMIT OF WORK</p> <p>M METER</p> <p>MAX MAXIMUM</p> <p>MFR MANUFACTURER</p> <p>MH MANHOLE</p> <p>MIN MINIMUM</p> <p>MISC MISCELLANEOUS</p> <p>N.A.P. NOT A PART</p> <p>NCN NO COMMON NAME</p> <p>NIC NOT IN CONTRACT</p> <p>NTS NOT TO SCALE</p> <p>O.C. ON CENTER</p> <p>O.D. OUTSIDE DIAMETER</p> <p>PL PROPERTY LINE</p> <p>PA PLANTER AREA</p> <p>P.I.P. POURED IN PLACE</p> <p>R RADIUS</p> <p>REV REVISION</p> <p>R.O.W. RIGHT OF WAY</p> <p>SHT SHEET</p> <p>SPEC SPECIFICATION</p> <p>SF SQUARE FOOT</p> <p>SQ SQUARE</p> <p>SS STAINLESS STEEL</p> <p>STD STANDARD</p> <p>T TRANSFORMER</p> <p>T.A. TOP OF CURB</p> <p>T.B. TOP OF DRAIN</p> <p>T.R. TOP OF RAILING</p> <p>T.S. TOP OF STEP</p> <p>T.W. TOP OF WALL</p> <p>TYP TYPICAL</p> <p>VERT VERTICAL</p> <p>W WITH</p> <p>WI WROUGHT IRON</p> <p>WT WEIGHT</p>	<p>CAMERON BROTHERS</p> <p>10580 PROSPECT AVE, STE 200 SANTEE, CA 92071 EMAIL: JIM@CAMERONBROS.NET</p> <p>CONTACT: JIM MOXHAM PH: (619) 562-3050</p>	<p>SHEET NO DESCRIPTION SCALE</p> <p>L100 LANDSCAPE TITLE SHEET --</p> <p>L101 PARCEL A HARDSCAPE PLAN 1" = 30'-0"</p> <p>L102 PARCEL B HARDSCAPE PLAN 1" = 30'-0"</p> <p>L103 PARCEL C HARDSCAPE PLAN 1" = 30'-0"</p> <p>L200 PARCEL A PLANTING PLAN 1" = 30'-0"</p> <p>L201 PARCEL B PLANTING PLAN & NOTES 1" = 30'-0"</p> <p>L202 PARCEL C PLANTING PLAN & LEGEND 1" = 30'-0"</p> <p>L203 WATER CALCULATIONS --</p> <p>L204 LANDSCAPE MASTER PLAN 1" = 50'-0"</p>		
		<p>ARCHITECT / PROJECT MANAGER</p> <p>HED</p> <p>11750 SORRENTO VALLEY RD, STE 100 SAN DIEGO, CA 92121 EMAIL: KPERRY@HED.DESIGN</p> <p>CONTACT: KEVIN PERRY PH: (858) 519-3988</p>	<p>LANDSCAPE ARCHITECT</p> <p>HED</p> <p>11750 SORRENTO VALLEY RD, STE 100 SAN DIEGO, CA 92121 EMAIL: LCHICCONE@HED.DESIGN</p> <p>CONTACT: LINDSAY CHICCONI PH: (619) 398-3804</p>	<p>SITE MAP</p> <p>SCALE: NOT TO SCALE</p>	<p>VICINITY MAP</p> <p>SCALE: NOT TO SCALE</p>
		<p>SCOPE OF WORK</p> <p>SITE IMPROVEMENTS INCLUDING HARDSCAPE, IRRIGATION AND PLANTING FOR NEW BUILDINGS AND PARKING AREA</p>	<p>DECLARATION OF RESPONSIBILITY</p> <p>I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLAN</p> <p>LINDSAY CHICCONI LANDSCAPE ARCHITECT PLA 6271</p> <p>DATE</p>		

HED

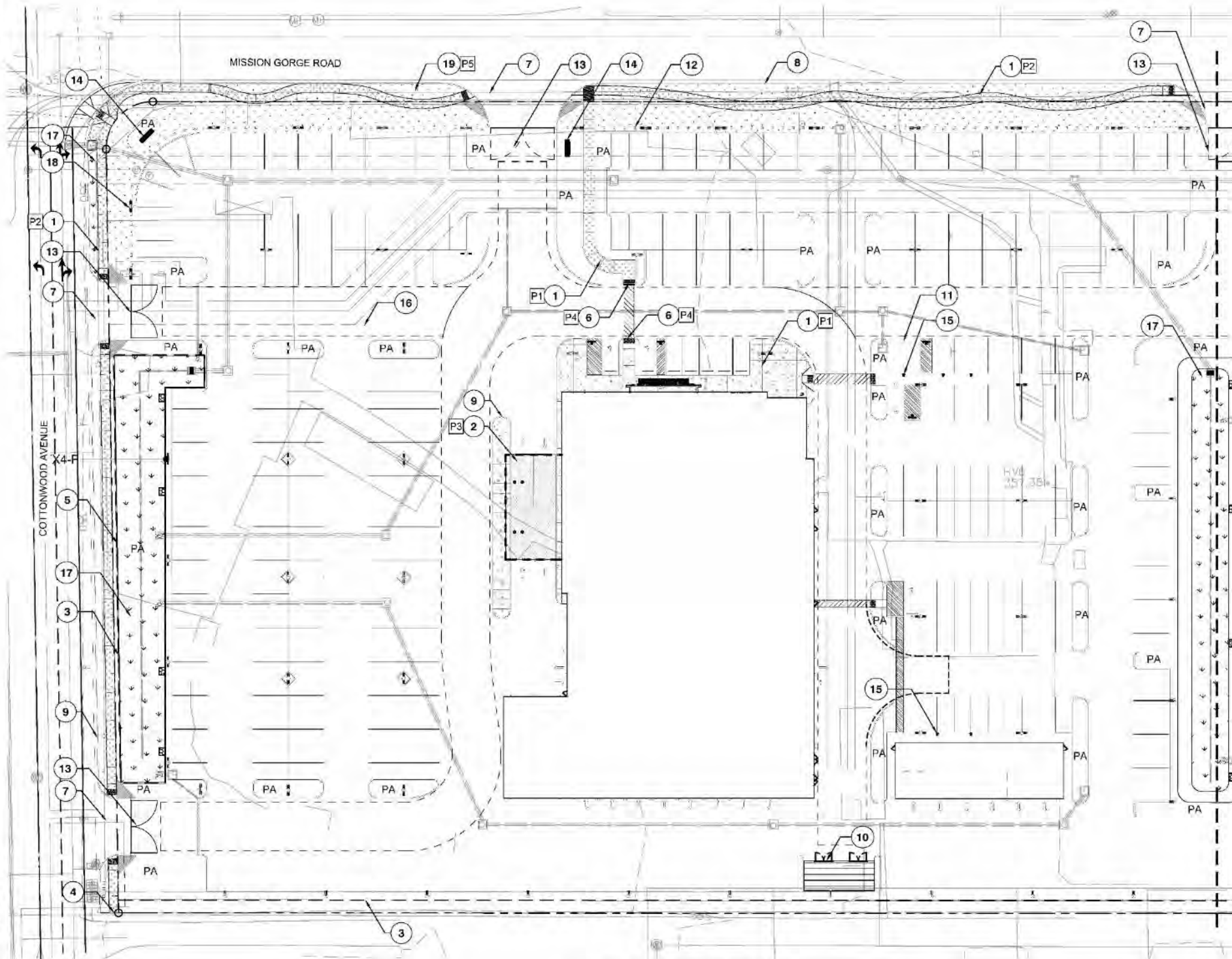
11750 Sorrento Valley Rd
Suite 100
San Diego, California
92121 USA
(858) 366-3800
WWW.HED.DESIGN

02/22
B022-CB036-004

LANDSCAPE
TITLE SHEET

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L100

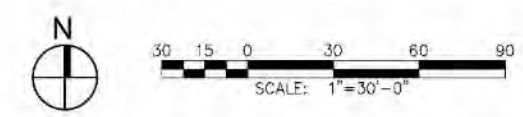


1 PARCEL A - HARDSCAPE PLAN
1" = 30'-0"

COLOR AND FINISH SCHEDULE							
SYMBOL	KEY	DESCRIPTION	MANUFACTURER	MODEL	COLOR	FINISH	COMMENTS
PAVING							
	P1	PEDESTRIAN CONCRETE PAVING TYPE 1	-	-	NATURAL	TOP-CAST #3	JOINTS: SAWCUT AND DOWELED CONSTRUCTION
	P2	PEDESTRIAN CONCRETE PAVING TYPE 2	-	-	NATURAL	BROOM FINISH	JOINTS: SAWCUT AND DOWELED CONSTRUCTION
	P3	INTEGRAL COLOR VEHICULAR CONCRETE PAVING	-	-	NATURAL	TOP-CAST #3	JOINTS: SAWCUT AND DOWELED CONSTRUCTION
	P4	PRE-CAST TRUNCATED DOMES PAVERS	ACKER-STONE 951-674-0047	PALAZZO12"x12" TRUNCATED DOMES - FM PTD-807	FEDERAL YELLOW	STACKED BOND PATTERN	SEAL PAVERS AND GROUT WITH MANUFACTURER RECOMMENDED SEALER
	P5	ARTIFICIAL TURF	SYNLAWN 866-756-5296	SYNAUGUSTINE 347	PE / FIELD GREEN / OLIVE / APPLE	-	-

REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION
1	CONSTRUCT PEDESTRIAN RATED CONCRETE PAVING
2	CONSTRUCT VEHICULAR RATED CONCRETE PAVING
3	RETAINING WALL PER CIVIL DRAWINGS
4	CONSTRUCT NEW TO EXISTING PAVING CONNECTION
5	GUARDRAIL
6	FURNISH AND INSTALL PRE-CAST CONCRETE TRUNCATED DOMES
7	CONSTRUCT NEW CONCRETE DRIVEWAY PER CIVIL DRAWINGS
8	EXISTING CONCRETE CURB - PROTECT IN PLACE, TYP. WHERE OCCURS
9	CONCRETE CURB PER CIVIL DRAWINGS
10	ENCLOSED DUMPSTER PER ARCHITECTURE DRAWINGS
11	PARKING STRIPING PER CIVIL DRAWINGS
12	FURNISH AND INSTALL CONCRETE BOLLARDS PER ARCHITECTURE DRAWINGS
13	SWING GATE WITH OPTICOM OR EQUAL FIRE DEPT. ACCESS PER ARCHITECTURE PLANS
14	MONUMENT SIGNAGE PER ARCHITECTURE DRAWINGS
15	ELECTRIC VEHICLE CHARGER, TYP.
16	ASPHALT PAVING PER CIVIL DRAWINGS
17	BMP TREATMENT BASIN PER CIVIL DRAWINGS
18	SITE LIGHTING PER ARCHITECT'S PLANS, TYP.
19	ARTIFICIAL TURF



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CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CUP Submittal
08/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal

HED

11750 Sanerito Valley Rd
Suite 100
San Diego, California
92121 USA
(658) 358-3600
WWW.HED DESIGN

BDC22-CB036-004

(A) HARDSCAPE
PLAN

L101



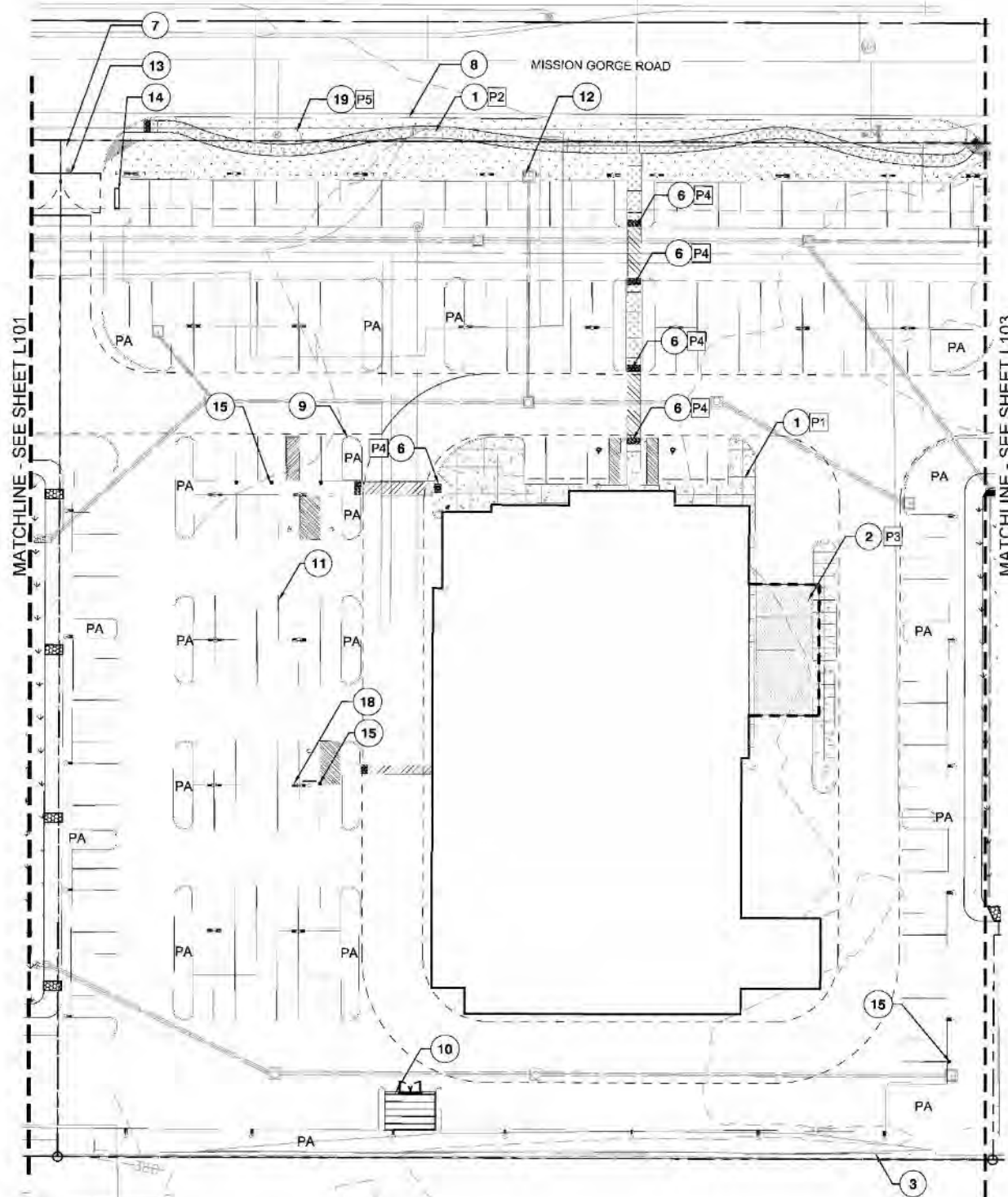
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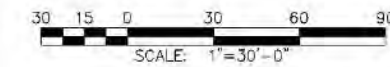


REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION
1	CONSTRUCT PEDESTRIAN RATED CONCRETE PAVING
2	CONSTRUCT VEHICULAR RATED CONCRETE PAVING
3	RETAINING WALL PER CIVIL DRAWINGS
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9	CONCRETE CURB PER CIVIL DRAWINGS
10	ENCLOSED DUMPSTER PER ARCHITECTURE DRAWINGS
11	PARKING STRIPING PER CIVIL DRAWINGS
12	FURNISH AND INSTALL CONCRETE BOLARDS PER ARCHITECTURE DRAWINGS
13	SWING GATE WITH OPTICOM DR EQUAL FIRE DEPT. ACCESS PER ARCHITECTURE PLANS
14	MONUMENT SIGNAGE PER ARCHITECTURE DRAWINGS
15	ELECTRIC VEHICLE CHARGER, TYP.
16	ASPHALT PAVING PER CIVIL DRAWINGS
17	BMP TREATMENT BASIN PER CIVIL DRAWINGS
18	SITE LIGHTING PER ARCHITECT'S PLANS, TYP.
19	ARTIFICIAL TURF

COLOR AND FINISH SCHEDULE

SYMBOL	KEY	DESCRIPTION	MANUFACTURER	MODEL	COLOR	FINISH	COMMENTS
PAVING							
	P1	PEDESTRIAN CONCRETE PAVING TYPE 1	-	-	NATURAL	TOP-CAST #3	JOINTS SAWCUT AND DOWELED CONSTRUCTION
	P2	PEDESTRIAN CONCRETE PAVING TYPE 2	-	-	NATURAL	BROOM FINISH	JOINTS SAWCUT AND DOWELED CONSTRUCTION
	P3	INTEGRAL COLOR VEHICULAR CONCRETE PAVING	-	-	NATURAL	TOP-CAST #3	JOINTS SAWCUT AND DOWELED CONSTRUCTION
	P4	PRE-CAST TRUNCATED DOMES PAVERS	ACKER-STONE 951-674-0047	PALAZZO 12"X12" TRUNCATED DOMES - FM P1D-607	FEDERAL YELLOW	STACKED BOND PATTERN	SEAL PAVERS AND GROUT WITH MANUFACTURER RECOMMENDED SEALER
	P5	ARTIFICIAL TURF	SYNLAWN 866-798-5298	SYNAUGUSTINE 347	PE / FIELD GREEN / OLIVE / APPLE	-	-



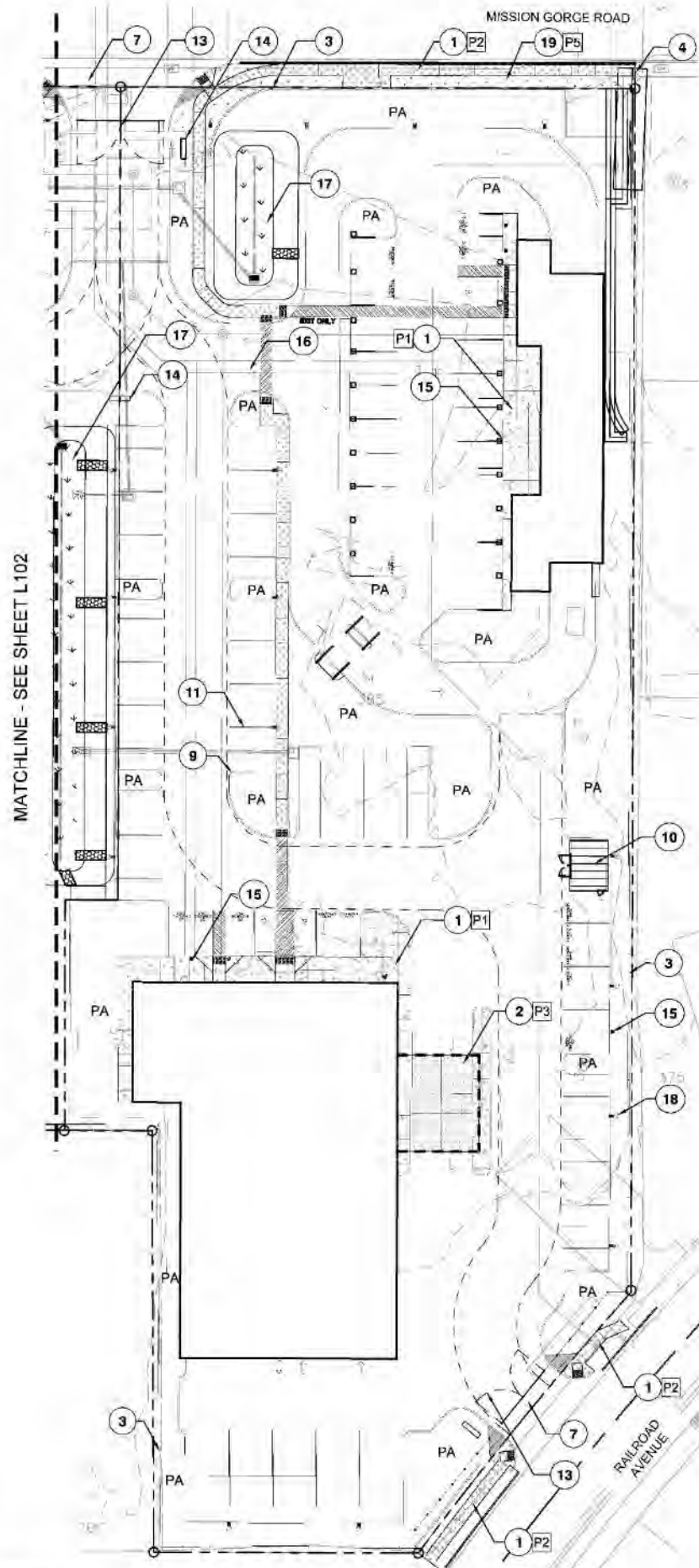
1 PARCEL B - HARDSCAPE PLAN
1" = 30'-0"

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BD22-CB036-004

(B) HARDSCAPE
PLAN

L102



REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION
1	CONSTRUCT PEDESTRIAN RATED CONCRETE PAVING
2	CONSTRUCT VEHICULAR RATED CONCRETE PAVING
3	RETAINING WALL PER CIVIL DRAWINGS
4	CONSTRUCT NEW TO EXISTING PAVING CONNECTION
5	GUARDRAIL
6	FURNISH AND INSTALL PRE-CAST CONCRETE TRUNCATED DOMES
7	CONSTRUCT NEW CONCRETE DRIVEWAY PER CIVIL DRAWINGS
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13	SWING GATE WITH OPTICOM OR EQUAL FIRE DEPT. ACCESS PER ARCHITECTURE PLANS
14	MONUMENT SIGNAGE PER ARCHITECTURE DRAWINGS
15	ELECTRIC VEHICLE CHARGER, TYP.
16	ASPHALT PAVING PER CIVIL DRAWINGS
17	BMP TREATMENT BASIN PER CIVIL DRAWINGS
18	SITE LIGHTING PER ARCHITECT'S PLANS, TYP.
19	ARTIFICIAL TURF

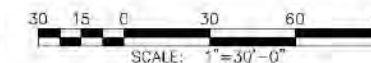
HARDSCAPE NOTES:

- A. DRAWING IS DIAGRAMMATIC. VERIFY ALL CONDITIONS AND LOCATIONS ON SITE PRIOR TO THE START OF CONSTRUCTION. NOTIFY LANDSCAPE ARCHITECT IMMEDIATELY IF ANY ERRORS OR DISCREPANCIES ARE FOUND. BEFORE PROCEEDING WITH ANY WORK DO NOT SCALE DRAWINGS. USE DIMENSIONS AS INDICATED ON PLAN.
- B. DO NOT WILLFULLY PROCEED WITH CONSTRUCTION WHEN IT IS OBVIOUS THAT OBSTRUCTIONS AND/OR GRADE DIFFERENCES EXIST THAT MAY NOT HAVE BEEN APPARENT DURING DESIGN. OBTAIN DIRECTION BEFORE PROCEEDING WITH CONSTRUCTION IN SUCH CASES.
- C. THE LOCATION OF ALL SERVICE RUNS, SUCH AS WATER SUPPLY, ELECTRICAL (OVERHEAD AND UNDERGROUND), TELEPHONE, SANITARY SEWER, ETC. SHOULD BE ASCERTAINED BEFORE WORK IS STARTED. WHERE THEY WILL BE AFFECTED BY EXCAVATION OR WHERE MACHINES MAY BE WORKING NEARBY, THEY SHOULD BE CAREFULLY SEALED, PROTECTED OR DIVERTED. NOTIFY ARCHITECT OF ANY CONFLICT WITH PROPOSED IMPROVEMENTS. COORDINATE ALL UTILITY RUNS AND INSTALLATIONS, EXISTING AND NEW, PRIOR TO PAVING INSTALLATION. VERIFY ALL SLEEVES AND ELECTRIC SOURCES FOR IRRIGATION CLOCK AND LIGHTING WITH PROPER SUB-CONTRACTORS.
- D. CLEAN OUT AND ADJUST EXISTING DRAINS TO ACCOMMODATE PROPOSED DRAINAGE. ADJUST LATERAL DRAIN LINES AS NECESSARY TO BEST LINK EXISTING MAIN LINES.
- E. OBTAIN INSPECTION AND APPROVAL OF ALL FORMS PRIOR TO PLACING CONCRETE.
- F. TRANSITIONS FROM EXISTING PAVING TO NEW PAVING SHALL BE FLUSH.
- G. REFER TO PLANS AND GENERAL NOTES FOR ADDITIONAL NOTES AND INFORMATION REGARDING THE CONSTRUCTION OF THIS PROJECT.
- H. CONSTRUCT ALL WALLS, PAVING, DRAIN LINES, SLEEVING, ETC. PER PLAN AND DETAILS.
- I. CONCRETE TO SLOPE FROM FOUNDATION TOWARDS DRAIN INLETS AND DRAINAGE SWALES AT A MINIMUM RATE OF 1/8" PER FOOT. INSTALL CONCRETE FORMS WITH LONG, SMOOTH GRADIENTS TO ELIMINATE DIPS, RIDGES, ABRUPT CHANGES OF GRADE, AND SHARP TRANSITIONS.
- J. ALL CONCRETE PAVING BANDS AND CONCRETE CAPS SHALL HAVE CONTROL JOINTS AT 5'-0" ON CENTER MAXIMUM UNLESS NOTED OTHERWISE ON THE PLANS. INSTALL EXPANSION JOINTS AT ALL VERTICAL ELEMENTS. INSTALL CRACK CONTROL JOINTS AT MAX. 12'-0". Z/P JOINTS WILL NOT BE ALLOWED. INSTALL EJS AND CG'S AT 90° TO THE EDGE OF CONCRETE PAVING. SEE PLAN FOR ADDITIONAL INFORMATION AND LOCATIONS.
- K. PRIOR TO PLACING CONCRETE, SUFFICIENTLY MOISTEN THE SUB-GRADE AND PROVIDE SUB-GRADE PREPARATIONS PER THE SOILS ENGINEER.
- L. INSTALL TYPE II OR TYPE V CONCRETE THAT OBTAINS A MINIMUM COMPRESSIVE STRENGTH OF 2,500 P.S.I. AT 28 DAYS UNLESS OTHERWISE SPECIFIED ON THESE PLANS AND DETAILS. SUBMIT DESIGN MIXES AND CONCRETE SAMPLE TO OWNER AND LANDSCAPE ARCHITECT PRIOR TO LAYING OUT PAVING. INSTALL CONCRETE FLATWORK PER RECOMMENDATIONS OF SOILS ENGINEER AND SOILS IN CASE OF CONFLICT BETWEEN SOILS

- M. BACKFILL AND COMPACT BEHIND ALL WALLS AND MASONRY STRUCTURES, AS REQUIRED ON DRAWINGS. AND DETAILS. BACKFILL PLANTERS WITH TOP GRADE SANDY-LOAM TOPSOIL PER PLANTING NOTES.
- N. CLEAN UP AND REMOVE ALL DEBRIS FROM SITE PRIOR TO REQUESTING FINAL APPROVAL. GRADE ALL AREAS WITHIN 3 INCH OF FINISH GRADES. NOTE AREAS OF ADDITIONAL GRADING SHOWN. COORDINATE WITH LANDSCAPE ARCHITECT THE FINAL FORM OF LANDSCAPE MOUNDS.
- O. ALL LF AND SF DIMENSIONS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND CONFIRMED WITH OWNERS REP. PRIOR TO ORDERING HARDSCAPE MATERIALS.
- P. ALL MATERIAL COLORS ARE TO BE VERIFIED WITH SDGE PRIOR TO ORDER AND INSTALL. ALL GREY CONCRETE SHALL MATCH EXISTING STANDARD GREY CONCRETE IN COLOR AND FINISH. ALL TAN INTEGRAL COLOR CONCRETE SHALL MATCH EXISTING TAN COLOR CONCRETE AND FINISH ON WALLS.
- Q. COMPLY WITH ALL PROVISIONS OF THE LATEST BUILDING CODE, CURRENT EDITION OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN, AND WITH OTHER CURRENT RULES, REGULATIONS AND ORDINANCES GOVERNING THE PLACE OF WORK. BUILDING CODE REQUIREMENTS TAKE PRECEDENCE OVER THE DRAWINGS AND IT SHALL BE THE RESPONSIBILITY OF ANYONE SUPPLYING LABOR OR MATERIALS OR BOTH TO BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT ANY DISCREPANCIES OR CONFLICTS BETWEEN THE REQUIREMENTS OF THE CODE AND THE DRAWINGS.
- R. VERIFY ALL PAVING AND HARDSCAPE CONSTRUCTION DRAWINGS WITH SOIL ENGINEER'S REPORT WITH REGARD TO BASE PREPARATION AND FOOTING REQUIREMENTS. NOTIFY THE OWNER IMMEDIATELY IF SOILS REPORT RECOMMENDATIONS DIFFER FROM DRAWINGS. THE SOILS REPORT RECOMMENDATIONS, IF MORE STRINGENT THAN THE DRAWINGS, SHALL TAKE PRECEDENCE.
- S. BE RESPONSIBLE FOR COORDINATION BETWEEN SUBCONTRACTORS FOR PROPER AND TIMELY PLACEMENT OF SLEEVING, PIPING AND / OR CONDUIT INSTALLATION UNDER OR THROUGH LANDSCAPE ELEMENTS.
- T. LANDSCAPE LIGHT FIXTURE LOCATIONS AS INDICATED ON THESE PLANS ARE APPROXIMATE. FINAL LOCATION TO BE VERIFIED BY LANDSCAPE ARCHITECT ON SITE.
- U. PROJECT WALKS SHALL NOT EXCEED A SLOPE OF 20:1 (5% GRADIENT) UNLESS OTHERWISE INDICATED.
- V. HANDICAP RAMPS SHALL NOT EXCEED 12:1 OR 8.33%.
- W. PLANTER AREAS SHALL NOT EXCEED 2:1 SLOPE UNLESS OTHERWISE INDICATED.
- X. HOLD FINISH GRADE A MINIMUM OF 8" BELOW FINISH FLOOR, UNLESS OTHERWISE INDICATED.
- Y. CONSTRUCT ALL CURVE TO CURVE AND CURVE TO TANGENT LINES TO BE NEAT, TRIM, SMOOTH AND UNIFORM.
- Z. PROVIDE THE OWNER WITH ALL WARRANTIES, GUARANTEES, AND INSTRUCTION MANUALS FOR EQUIPMENT, APPLIANCES, FIXTURES, ETC. AS DESCRIBED IN THE SPECIFICATIONS.
- AA. DO NOT SCALE DRAWINGS.
- AB. VERIFY ALL PAVING AND HARDSCAPE CONSTRUCTION DRAWINGS WITH SOIL ENGINEER'S REPORT WITH REGARD TO BASE PREPARATION AND FOOTING REQUIREMENTS. NOTIFY THE OWNER IMMEDIATELY IF SOILS REPORT RECOMMENDATIONS DIFFER FROM DRAWINGS. THE SOILS REPORT RECOMMENDATIONS, IF MORE STRINGENT THE DRAWINGS, SHALL TAKE PRECEDENCE.

COLOR AND FINISH SCHEDULE

SYMBOL	KEY	DESCRIPTION	MANUFACTURER	MODEL	COLOR	FINISH	COMMENTS
PAVING							
	P1	PEDESTRIAN CONCRETE PAVING TYPE 1	-	-	NATURAL	TOP-CAST #3	JOINTS: SAWCUT AND DOWELED CONSTRUCTION
	P2	PEDESTRIAN CONCRETE PAVING TYPE 2	-	-	NATURAL	BROOM FINISH	JOINTS: SAWCUT AND DOWELED CONSTRUCTION
	P3	INTEGRAL COLOR VEHICULAR CONCRETE PAVING	-	-	NATURAL	TOP-CAST #3	JOINTS: SAWCUT AND DOWELED CONSTRUCTION
	P4	PRE-CAST TRUNCATED DOMES PAVERS	ACKER-STDNE 851-674-0047	PALAZZO 12"X12" TRUNCATED DOMES - FM FTD-607	FEDERAL YELLOW	STACKED BOND PATTERN	SEAL PAVERS AND GROUT WITH MANUFACTURER RECOMMENDED SEALER
	P5	ARTIFICIAL TURF	SYNLAWN 886-798-5298	SYNAUGUSTINE 347	PE / FIELD GREEN / OLIVE / APPLE	-	-



1 PARCEL C - HARDSCAPE PLAN
1" = 30'-0"

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Cameron Brothers Company LLC

Santee Auto Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CUP Submittal
08/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal

HED

11750 Sorrento Valley Rd
Suite 100
San Diego, California
92121 USA
(656) 368-3600
WWW.HEDDESIGN

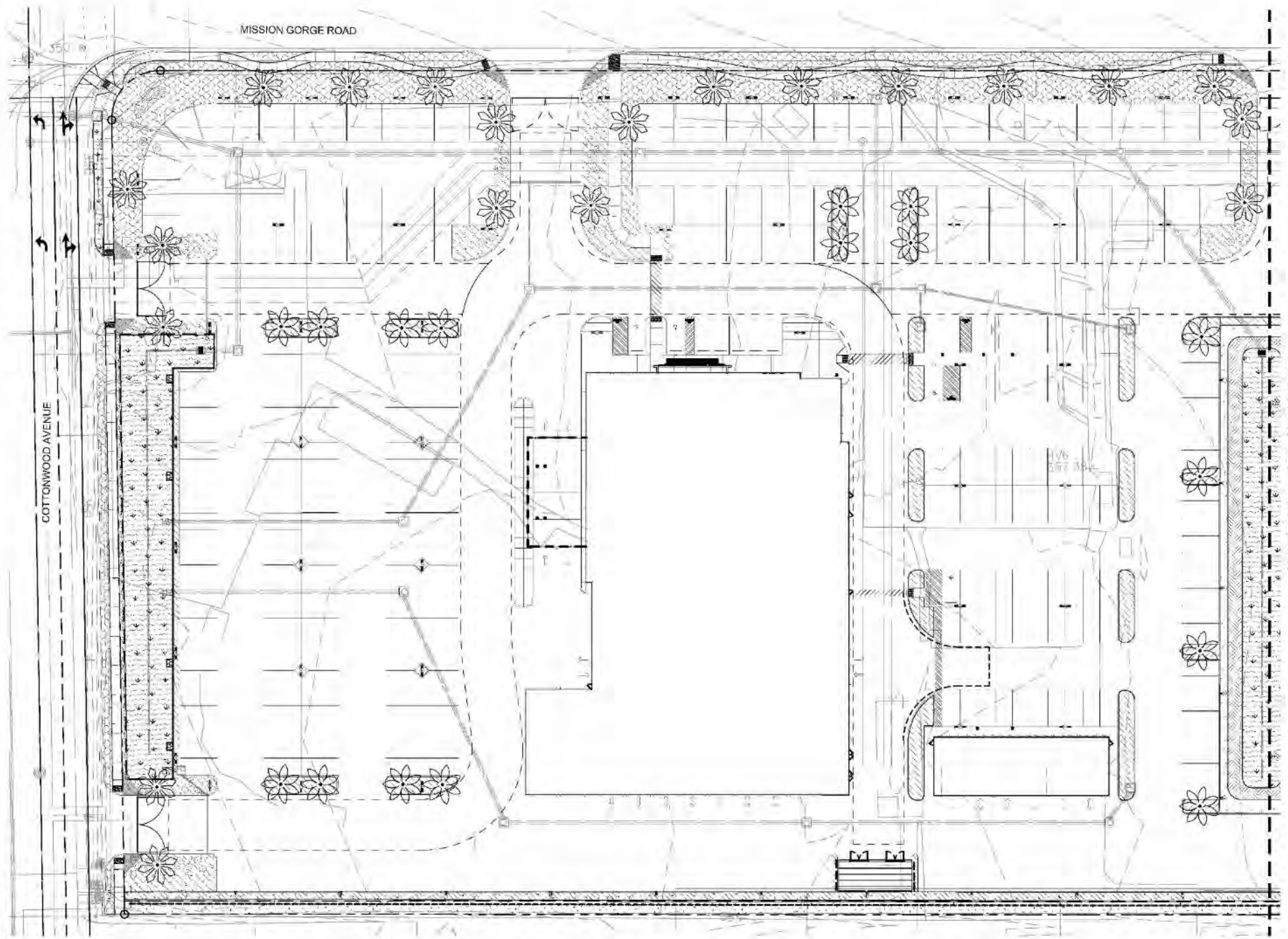
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(C) HARDSCAPE PLAN

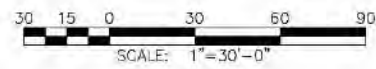
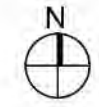
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TREE COUNT:
 PARKING REQ PER ARCH DRAWINGS: 91 SPACES
 TREES PROVIDED: 36



1 PARCEL A - PLANTING PLAN
 1" = 30'-0"

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 BROTHERS COMPANY LLC

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 Company LLC

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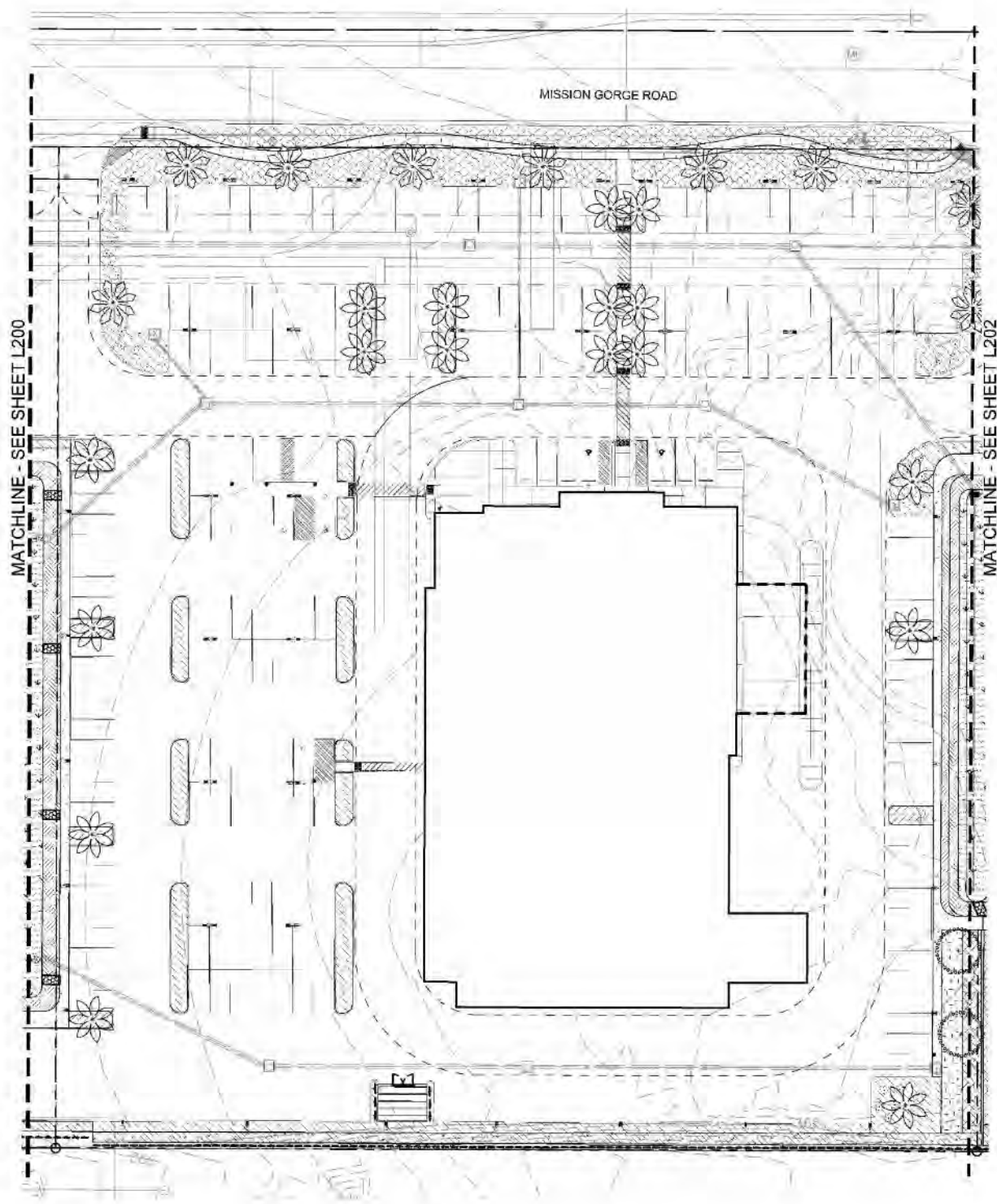
HED

11750 Sorento Valley Rd
 Suite 100
 San Diego, California
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 (858) 368-3800
 WWW.HED.DESIGN

9/2022
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(A) PLANTING
 PLAN

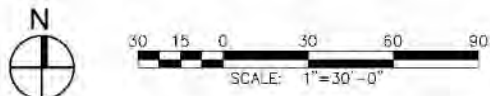
L200



PLANTING NOTES

- A. REFER TO PLANTING SPECIFICATIONS AND DETAILS FOR SOIL PREPARATION FERTILIZATION, MULCHING AND OTHER PLANTING INFORMATION.
- B. NOTIFY THE OWNER'S AUTHORIZED REPRESENTATIVE AND THE CITY INSPECTOR 48 HOURS PRIOR TO COMMENCEMENT OF WORK TO COORDINATE PROJECT INSPECTION SCHEDULES.
- C. VERIFY ALL EXISTING CONDITIONS, DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH THE WORK. NOTIFY LANDSCAPE ARCHITECT IMMEDIATELY SHOULD FIELD CONDITIONS VARY FROM THOSE SHOWN ON PLAN.
- D. REPORT DISCREPANCIES IN THE DRAWINGS OR BETWEEN THE DRAWINGS AND ACTUAL FIELD CONDITIONS TO THE LANDSCAPE ARCHITECT. CORRECTED DRAWINGS OR INSTRUCTION SHALL BE ISSUED PRIOR TO THE CONTINUATION OF THIS WORK. ASSUME FULL RESPONSIBILITY FOR ALL NECESSARY CORRECTIONS DUE TO FAILURE TO REPORT KNOWN DISCREPANCIES.
- E. LOCATE ALL EXISTING UTILITIES WHETHER SHOWN HEREON OR NOT AND PROTECT THEM FROM DAMAGE. NOTIFY THE OWNER'S REPRESENTATIVE IMMEDIATELY AND ASSUME FULL RESPONSIBILITY FOR EXPENSE OF REPAIR OR REPLACEMENT IN CONJUNCTION WITH DAMAGED UTILITIES.
- F. LOCATION OF N.I.C. CONSTRUCTION ELEMENTS SUCH AS LIGHTS, SIGNS, VENTS, HYDRANTS, TRANSFORMERS, ETC. ARE APPROXIMATE. NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY SHOULD THE LOCATION OF THESE ITEMS INTERFERE WITH THE PROPER EXECUTION OF WORK.
- G. OBTAIN ALL SOIL FOR LANDSCAPE PLANTING AREAS OR BERMS FROM ON-SITE EXCAVATIONS. SHOULD IMPORT SOIL BE NECESSARY, SUBMIT IMPORT SOIL TESTING RESULTS FOR APPROVAL PRIOR TO IMPORTATION. SOIL SHALL BE SANDY LOAM CONTAINING NO TOXIC CHEMICALS OR ELEMENTS WHICH MAY INHIBIT OR RETARD NORMAL PLANT GROWTH.
- H. BEFORE BIDDING AND AFTER ROUGH GRADES HAVE BEEN ESTABLISHED IN PLANTING AREAS, HAVE SOIL SAMPLES TAKEN AT THE LOCATIONS INDICATED ON Y. PLANTING PLAN. HAVE SAMPLES TESTED BY WAYPOINT ANALYTICAL (714) 282-8777, FOR SOIL FERTILITY. TAKE TWO SAMPLES AT EACH LOCATION: (1) GROUND LEVEL TO 10" DEEP, (2) 24" TO 36" DEEP. EACH SAMPLE SHALL CONTAIN APPROXIMATELY 1 QUART OF SOIL TO BE LABELED PER LOCATION AND DEPTH. INSTALL SOIL PREPARATION AND BACK FILL MIX TO CONFORM TO THESE RECOMMENDATIONS ONLY UPON RECEIPT OF WRITTEN CHANGE ORDER FROM THE OWNER. SUBMIT SOIL REPORT TO LANDSCAPE ARCHITECT PRIOR TO PLANTING.
- I. KILL AND REMOVE ALL EXISTING WEEDS FROM SITE AREAS PER SPECIFICATIONS.
- J. ASSURE POSITIVE DRAINAGE IN ALL PLANTING AREAS, 2% MINIMUM.
- K. LOCATE AND TAG ALL PLANT MATERIAL. MATERIAL SHALL BE IN CONFORMANCE WITH PLANTING PLAN DESCRIPTIONS AND SPECIFICATIONS. ALL PLANT MATERIAL IS SUBJECT TO REVIEW AND APPROVAL PRIOR TO INSTALLATION. PROVIDE PHOTOS OF REPRESENTATIVE EXAMPLES OF EACH TAGGED BLOCK TO LANDSCAPE ARCHITECT MINIMUM 21 DAYS BEFORE ANTICIPATED DELIVERY. PHOTOS SHALL INCLUDE A PERSON FOR SCALE PURPOSES. LANDSCAPE ARCHITECT MAY OPT TO REVIEW MATERIAL AT GROWING NURSERY. MATERIAL DELIVERED TO THE SITE MAY BE REJECTED BASED ON UNHEALTHY APPEARANCE OR NON-CONFORMANCE WITH SPECIFICATIONS EVEN IF PREVIOUSLY REVIEWED BY THE LANDSCAPE ARCHITECT OR THE OWNER.
- L. ALL TREES WITHIN A SPECIES SHALL HAVE MATCHING FORM.
- M. FINAL LOCATION OF ALL PLANT MATERIALS SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER'S AUTHORIZED REPRESENTATIVE.
- N. PLANTING QUANTITIES ARE GIVEN FOR CONVENIENCE ONLY. PLANT SYMBOLS AND SPECIFIED SPACING SHALL TAKE PRECEDENCE.
- O. AT EDGES OF PLANTING AREAS, THE CENTER LINE OF THE LAST ROW OF SHRUBS AND/OR GROUND COVER SHALL BE LOCATED NO FARTHER FROM THE EDGE THAN ONE-HALF THE SPECIFIED ON CENTER SPACING.
- P. INSTALL GROUND COVER AND/OR SHRUB MASSES WITH TRIANGULAR SPACING UNLESS OTHERWISE INDICATED.
- Q. ALL CURVE TO CURVE AND CURVE TO TANGENT LINES SHALL BE NEAT, TRIM, SMOOTH AND UNIFORM.
- R. REMOVE ALL NURSERY STAKES AND ESPALIER RACKS IMMEDIATELY AFTER INSTALLATION UPON PROVIDING SUPPORT PER PLAN.
- S. DURING THE LENGTH OF THE GUARANTEE PERIOD, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER STAKING AND/OR GUYING OF TREES TO ENSURE STABILITY.
- T. MULCH ALL LANDSCAPE AREAS (EXCLUDING TURF) HAVING LESS THAN A 2:1 SLOPE WITH A 3" DEEP LAYER OR 2" DEEP LAYER AT SUCCULENT AND CACTI OF MEDIUM GRIND BARK MULCH PER SPECIFICATIONS, AT THE CONCLUSION OF PLANTING OPERATIONS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION.
- U. CONTRACTOR SHALL REPLACE ANY EXISTING PLANT MATERIAL WHICH IS DAMAGED BY CONSTRUCTION OPERATIONS. REPLACEMENT PLANT MATERIAL MUST BE OF MATCHING SPECIES, INSTALLED FROM THE FOLLOWING MINIMUM SIZE: 15-GALLON TREE, 1-GALLON SHRUB, FLATTED GROUND COVER AND SODDED TURF.
- V. INSTALLATIONS THAT ARE ADJACENT OPEN SPACE, NATURALIZED SLOPES OR UNDEVELOPED LAND ARE SUBJECT TO DAMAGE BY RODENTS OR DEER AND SHALL BE TREATED WITH AN APPROPRIATE REPELLENT IN A SPRAY AND/OR TABLET FORM. REPELLEX BY GROPOWER OR APPROVED EQUAL, THAT PROVIDES IMMEDIATE AND LONG TERM PROTECTION, SHALL BE USED.
- W. ROOT BARRIERS SHALL BE INSTALLED AT ALL TREES WITHIN 5 FEET OF ANY HARDSCAPE, PAVEMENT OR CURB. ROOT BARRIERS ARE TO BE UB24-2" BY DEEP ROOT CORPORATION, (800) 458-7668, INSTALLED PER MANUFACTURER'S SPECIFICATIONS. NOTE: ROOT BARRIERS SHALL NOT BE WRAPPED AROUND THE ROOTBALL. ROOT BARRIERS INSTALLED ADJACENT TO A BIOSWALE SHALL NOT INTERFERE WITH DRAINAGE TO OR FROM THE BIOSWALE SYSTEM.
- X. FIRE PREVENTION NOTE: MINIMUM VERTICAL CLEARANCE OF 13'-6" SHALL BE PROVIDED FROM TOP OF FIRE ACCESS ROADWAY TO LOWEST BRANCHES OF TREE AND MINIMUM OF 28 FEET IN WIDTH.
- Y. THE SHRUBS SHALL BE ALLOWED TO GROW IN THEIR NATURAL FORMS.
- Z. ALL LANDSCAPE IMPROVEMENTS SHALL FOLLOW CITY OF SANTEE GUIDELINES:
 - STREET TREES AND OTHER TREES SHALL BE SPACED:
 - 20 FEET FROM TRAFFIC SIGNALS / STOP SIGNS
 - 5 FEET (10 FEET FOR SEWER) FROM UNDERGROUND UTILITIES
 - 10 FEET FROM ABOVE GROUND UTILITY STRUCTURES
 - 10 FEET (5' FOR RESIDENTIAL <25 MPH) FROM DRIVEWAY ENTRIES
 - 25 FEET FROM INTERSECTIONS (INTERSECTING CURB LINES OF TWO STREETS)
 - 10 FEET FROM EASEMENT BOUNDARIES (SEWER, WATER, STORM DRAINS, ACCESS OR OTHER UTILITIES)
 - STREET TREES SHALL BE PLANTED 3' OUTSIDE RIGHT-OF-WAY IF THE RIGHT-OF-WAY DOES NOT ALLOW SPACE, SUBJECT TO THE CITY ENGINEER'S APPROVAL.
 - SCREEN ALL UTILITIES ACCORDING TO SPECIFIC AGENCY REQUIREMENTS
 - AA. MAINTENANCE: ALL REQUIRED LANDSCAPE AREAS SHALL BE MAINTAINED BY THE OWNER. LANDSCAPE AND IRRIGATION AREAS IN THE PUBLIC RIGHT-OF-WAY SHALL BE MAINTAINED BY THE OWNER. THE LANDSCAPE AREAS SHALL BE MAINTAINED FREE OF DEBRIS AND LITTER, AND ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY GROWING CONDITION. DISEASED OR DEAD PLANT MATERIAL SHALL BE SATISFACTORILY TREATED OR REPLACED WITHIN 30 DAYS OR A FINAL LANDSCAPE INSPECTION.
 - AB. A MINIMUM ROOT ZONE OF 40SF IN AREA SHALL BE PROVIDED FOR ALL TREES. THE MINIMUM DIMENSION FOR THIS AREA SHALL BE 5 FEET.
 - AC. TREES SHALL BE MAINTAINED SO THAT ALL BRANCHES OVER PEDESTRIAN WALKWAYS ARE 8 FEET ABOVE THE WALKWAY GRADE AND BRANCHES OVER VEHICULAR TRAVEL WAYS ARE 16 FEET ABOVE THE GRADE OF THE TRAVEL WAY.

TREE COUNT:
 PARKING REQ PER ARCH DRAWINGS: 84 SPACES
 TREES PROVIDED: 28



1 PARCEL B - PLANTING PLAN
 1" = 30'-0"

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Cameron Brothers
 Company LLC

Santee Auto
 Center

10335 Mission Gorge Road
 Santee, CA 92071

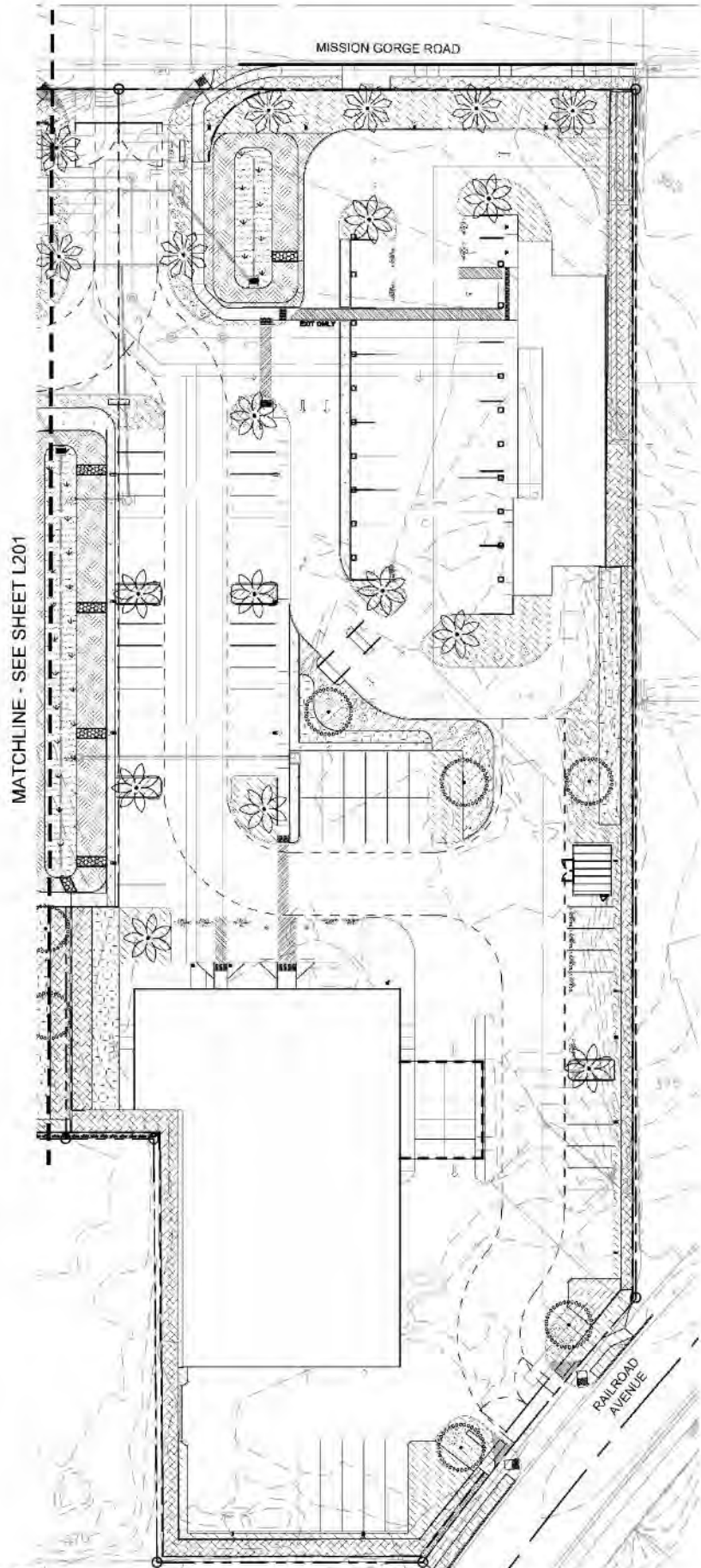
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BD22-CB036-004

(B) PLANTING
 PLAN & NOTES

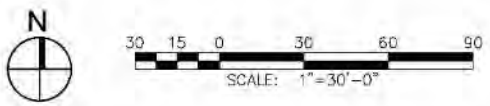
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PLANT SCHEDULE

TREES	BOTANICAL / COMMON NAME	SIZE	WUCOLS	QTY	
	LOPHOSTEMON CONFERTUS / BRISBANE BOX	24" BOX	M	7	
	PHOENIX DACTYLIFERA / DATE PALM - NON FRUITING MALE	14" BTH	L	34	
	SYAGRUS ROMANZOFFIANA / QUEEN PALM	12" BTH	M	44	
SHRUBS	BOTANICAL / COMMON NAME	SIZE	WUCOLS	QTY	
	AGAVE ATTENUATA / FOXTAIL AGAVE	5 GAL	L	99	
	BOUCAINVILLEA X 'SAN DIEGO RED' / SAN DIEGO RED BOUGAINVILLEA-STAKED	5 GAL ESPALIER	L	280	
	GALLIANDRA HAEMATOCEPHALA / PINK POWDERPUFF	5 GAL ESPALIER	L	12	
	FICUS PUMILA / CREEPING FIG	5 GAL ESPALIER	M	77	
SHRUB AREAS	BOTANICAL / COMMON NAME	SIZE	WUCOLS	SPACING	QTY
	CAREX PRAEGRACILIS / CALIFORNIA FIELD SEDGE	FLAT	L	12" o.c.	14,021 SF
	CARISSA MACROCARPA 'GREEN CARPET' / GREEN CARPET NATAL PLUM	1 GAL	L	24" o.c.	2,025 SF
	CHONDROPETALUM TECTORUM 'EL CAMPO' / EL CAMPO CAPE RUSH	5 GAL	L	36" o.c.	3,121 SF
	DIANELLA CAERULEA 'DRB03' TM / CASSA BLUE FLAX LILY	1 GAL	M	24" o.c.	1,268 SF
	LIGUSTRUM JAPONICUM 'TEXANUM' / TEXAS JAPANESE PRIVET	1 GAL	M	36" o.c.	14,025 SF
	LIGUSTRUM JAPONICUM 'TEXANUM' / TEXAS JAPANESE PRIVET	5 GAL	M	36" o.c.	4,406 SF
	LOMANDRA LONGIFOLIA 'LM300' / BREEZE™ MAT RUSH	FLAT	L	24" o.c.	10,667 SF
	RHAPHIOLEPIS INDICA 'CLARA' / CLARA INDIAN HAWTHORN	5 GAL	L	36" o.c.	7,370 SF
GROUND COVERS	BOTANICAL / COMMON NAME	SIZE	WUCOLS	SPACING	QTY
	1-2" MEXICAN BEACH PEBBLE BUFF MULCH	—	—	—	4,860 SF
	ANNUAL FLOWERS / TO BE SELECTED BY OWNER	—	—	—	112 SF
	ARTIFICIAL TURF PER HARDSCAPE LEGEND	—	—	—	22,478 SF
	DIANELLA REVOLUTA 'DR5000' / LITTLE REV™ FLAX LILY	1 GAL	L	24" o.c.	10,177 SF
	ROSMARINUS OFFICINALIS 'PROSTRATUS' / DWARF ROSEMARY	FLAT	L	36" o.c.	19,769 SF

TREE COUNT:
 PARKING REQ PER ARCH DRAWINGS: 57 SPACES
 TREES PROVIDED: 21



Cameron Brothers Company LLC

Santee Auto Center

10338 Mission Gorge Road
 Santee, CA 92071

Date Issued For
 12/16/2022 CLP Submittal
 09/23/2023 CLP Resubmittal
 06/11/2023 CLP Resubmittal



BD22-CB036-004

(C) PLANTING PLAN & LEGEND

L202

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1 PARCEL C - PLANTING PLAN
 1" = 30'-0"

Appendix B – WATER EFFICIENT LANDSCAPE WORKSHEET

WATER EFFICIENT LANDSCAPE WORKSHEET

DEPARTMENT OF DEVELOPMENT SERVICES
10601 Magnolia Avenue, Santee, CA 92071-1266, (619) 258-4100 ext. 168

This worksheet is filled out by the project applicant for each Point of Connection/Water Meter. Please complete all sections of the worksheet and use additional worksheets if necessary.

1. Project Information

Applicant: CAMERON BROTHERS Phone: 619-662-3050
Address: 10601 PROSPECT AVE, STE 200 Fax: [REDACTED]
Santee, CA 92071 Email: JIM@CAMERONBROS.NET

Property Owner: CAMERON BROTHERS Phone: 619-662-3050
Address: 10601 PROSPECT AVE, STE 200 Fax: [REDACTED]
Santee, CA 92071 Email: JIM@CAMERONBROS.NET

Project Address: 10355 MISSION GORGE RD, SANTEE, CA 92081 Project Type: Commercial
Assessor's Parcel Number: 285-01-010-010 Water Supply Type: (Potable, recycled, well) Potable
Total Landscape Area: 44,721 Water Purveyor: Padsco Dam

2. Applicant's/Property Owner's Certification

The design of this project complies with the requirements of the City of Santee Water Efficient Landscape Ordinance.

Lindsay Chicoine [Signature] Date: 12/17/22
Applicant's/Property Owner's Signature

3. Landscape Documentation Package Checklist:

Water Efficient Landscape Worksheet
 Soil Management Report
 Landscape Design Plan
 Irrigation Design Plan
 Grading Design Plan

4. Maximum Applied Water Allowance (MAWA) Reference Evapotranspiration (ET₀) 51.2

Hydrozone # (Planting Description)	Plant Factor (PF)	Irrigation Method	Irrigation Efficiency (IE)	ETAF (ET ₀ x PF x IE)	Landscape Area (sq. ft.)	ETAF x Area	Estimated Total Water Use (ETWU)*
Regular Landscape Areas							
LOW SHRUBS	0.4	DRIP	0.81	0.25	28,168	5,342	150,031
LOW SHRUBS	0.2	SPRAY	0.75	0.27	18,529	2,084	36,278
LOW TREE	0.2	DRIP	0.81	0.25	232	125	3,368
MED TREE	0.4	DRIP	0.81	0.25	441	795	8,222
MED SHRUB	0.4	DRIP	0.81	0.25	1,135	1,641	52,155
Special Landscape Areas							
[REDACTED]				1			
[REDACTED]				1			
[REDACTED]				1			
Totals					(C)	(D)	317,627
ETWU Total							317,627
Maximum Allowed Water Allowance (MAWA)							524,809

*** Hydrozone #/Planting Description:** 1) 1" drip line, 2) 1/2" water use (potable), 3) medium water use (potable)

*** Irrigation Method:** Overhead spray, 0.80

*** Irrigation Efficiency:** 0.75 for spray head, 0.81 for drip

*** ETWU (Annual Gallons Required) =** (ETAF x 6.2 x Area) or (0.62 x ETAF x Area) for residential areas

*** MAWA (Annual Gallons Allowed) =** (ETAF x 6.2 x Area) or (0.62 x ETAF x Area) for residential areas

ETAF Calculations

Regular Landscape Areas

Total ETAF x Area	(B)	5,945
Total Area	(A)	22,086
Average ETAF	B ÷ A	0.27

All Landscape Areas

Total ETAF x Area	(B + D)	5,945
Total Area	(A + C)	22,086
Sitewide ETAF	(B + D) ÷ (A + C)	0.27

MAWA = (51.2) (0.62) (0.45 X 36,739) = 524,809

Appendix B – WATER EFFICIENT LANDSCAPE WORKSHEET

WATER EFFICIENT LANDSCAPE WORKSHEET

DEPARTMENT OF DEVELOPMENT SERVICES
10601 Magnolia Avenue, Santee, CA 92071-1266, (619) 258-4100 ext. 168

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Address: 10601 PROSPECT AVE, STE 200 Fax: [REDACTED]
Santee, CA 92071 Email: JIM@CAMERONBROS.NET

Property Owner: CAMERON BROTHERS Phone: 619-662-3050
Address: 10601 PROSPECT AVE, STE 200 Fax: [REDACTED]
Santee, CA 92071 Email: JIM@CAMERONBROS.NET

Project Address: 10355 MISSION GORGE RD, SANTEE, CA 92081 Project Type: Commercial
Assessor's Parcel Number: 285-01-010-010 Water Supply Type: (Potable, recycled, well) Potable
Total Landscape Area: 26,668 Water Purveyor: Padsco Dam

2. Applicant's/Property Owner's Certification

The design of this project complies with the requirements of the City of Santee Water Efficient Landscape Ordinance.

Lindsay Chicoine [Signature] Date: 12/17/22
Applicant's/Property Owner's Signature

3. Landscape Documentation Package Checklist:

Water Efficient Landscape Worksheet
 Soil Management Report
 Landscape Design Plan
 Irrigation Design Plan
 Grading Design Plan

4. Maximum Applied Water Allowance (MAWA) Reference Evapotranspiration (ET₀) 51.2

Hydrozone # (Planting Description)	Plant Factor (PF)	Irrigation Method	Irrigation Efficiency (IE)	ETAF (ET ₀ x PF x IE)	Landscape Area (sq. ft.)	ETAF x Area	Estimated Total Water Use (ETWU)*
Regular Landscape Areas							
LOW SHRUBS	0.2	DRIP	0.81	0.25	15,124	1,781	120,021
LOW SHRUBS	0.2	SPRAY	0.75	0.27	12,717	621	15,867
LOW TREE	0.2	DRIP	0.81	0.25	200	52	1,517
MED TREE	0.4	DRIP	0.81	0.25	1,425	244	8,622
MED SHRUB	0.4	DRIP	0.81	0.25	4,005	1,282	40,595
Special Landscape Areas							
[REDACTED]				1			
[REDACTED]				1			
[REDACTED]				1			
Totals					(C)	(D)	165,115
ETWU Total							165,115
Maximum Allowed Water Allowance (MAWA)							315,208

*** Hydrozone #/Planting Description:** 1) 1" drip line, 2) 1/2" water use (potable), 3) medium water use (potable)

*** Irrigation Method:** Overhead spray, 0.80

*** Irrigation Efficiency:** 0.75 for spray head, 0.81 for drip

*** ETWU (Annual Gallons Required) =** (ETAF x 6.2 x Area) or (0.62 x ETAF x Area) for residential areas

*** MAWA (Annual Gallons Allowed) =** (ETAF x 6.2 x Area) or (0.62 x ETAF x Area) for residential areas

ETAF Calculations

Regular Landscape Areas

Total ETAF x Area	(B)	5,945
Total Area	(A)	22,086
Average ETAF	B ÷ A	0.27

All Landscape Areas

Total ETAF x Area	(B + D)	5,945
Total Area	(A + C)	22,086
Sitewide ETAF	(B + D) ÷ (A + C)	0.27

MAWA = (51.2) (0.62) (0.45 X 22,086) = 315,208

Appendix B – WATER EFFICIENT LANDSCAPE WORKSHEET

WATER EFFICIENT LANDSCAPE WORKSHEET

DEPARTMENT OF DEVELOPMENT SERVICES
10601 Magnolia Avenue, Santee, CA 92071-1266, (619) 258-4100 ext. 168

This worksheet is filled out by the project applicant for each Point of Connection/Water Meter. Please complete all sections of the worksheet and use additional worksheets if necessary.

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Address: 10601 PROSPECT AVE, STE 200 Fax: [REDACTED]
Santee, CA 92071 Email: JIM@CAMERONBROS.NET

Property Owner: CAMERON BROTHERS Phone: 619-662-3050
Address: 10601 PROSPECT AVE, STE 200 Fax: [REDACTED]
Santee, CA 92071 Email: JIM@CAMERONBROS.NET

Project Address: 10355 MISSION GORGE RD, SANTEE, CA 92081 Project Type: Commercial
Assessor's Parcel Number: 285-01-010-010 Water Supply Type: (Potable, recycled, well) Potable
Total Landscape Area: 32,722 Water Purveyor: Padsco Dam

2. Applicant's/Property Owner's Certification

The design of this project complies with the requirements of the City of Santee Water Efficient Landscape Ordinance.

Lindsay Chicoine [Signature] Date: 12/17/22
Applicant's/Property Owner's Signature

3. Landscape Documentation Package Checklist:

Water Efficient Landscape Worksheet
 Soil Management Report
 Landscape Design Plan
 Irrigation Design Plan
 Grading Design Plan

4. Maximum Applied Water Allowance (MAWA) Reference Evapotranspiration (ET₀) 51.2

Hydrozone # (Planting Description)	Plant Factor (PF)	Irrigation Method	Irrigation Efficiency (IE)	ETAF (ET ₀ x PF x IE)	Landscape Area (sq. ft.)	ETAF x Area	Estimated Total Water Use (ETWU)*
Regular Landscape Areas							
LOW SHRUBS	0.2	DRIP	0.81	0.25	36,274	6,581	174,907
LOW SHRUBS	0.2	SPRAY	0.75	0.27	3,721	208	7,429
LOW TREE	0.2	DRIP	0.81	0.25	150	76	1,205
MED TREE	0.4	DRIP	0.81	0.25	400	795	8,222
MED SHRUB	0.4	DRIP	0.81	0.25	3,423	725	24,931
Special Landscape Areas							
[REDACTED]				1			
[REDACTED]				1			
[REDACTED]				1			
Totals					(C)	(D)	240,385
ETWU Total							240,385
Maximum Allowed Water Allowance (MAWA)							430,933

*** Hydrozone #/Planting Description:** 1) 1" drip line, 2) 1/2" water use (potable), 3) medium water use (potable)

*** Irrigation Method:** Overhead spray, 0.80

*** Irrigation Efficiency:** 0.75 for spray head, 0.81 for drip

*** ETWU (Annual Gallons Required) =** (ETAF x 6.2 x Area) or (0.62 x ETAF x Area) for residential areas

*** MAWA (Annual Gallons Allowed) =** (ETAF x 6.2 x Area) or (0.62 x ETAF x Area) for residential areas

ETAF Calculations

Regular Landscape Areas

Total ETAF x Area	(B)	7,825
Total Area	(A)	30,159
Average ETAF	B ÷ A	0.26

All Landscape Areas

Total ETAF x Area	(B + D)	7,825
Total Area	(A + C)	30,159
Sitewide ETAF	(B + D) ÷ (A + C)	0.26

MAWA = (51.2) (0.62) (0.45 X 30,168) = 430,933



Cameron Brothers Company LLC

Santee Auto Center

10355 Mission Gorge Road, Santee, CA 92071

Date Issued For: 12/16/2022 CUP Submittal, 08/29/2023 CUP Resubmittal, 08/11/2023 CUP Resubmittal



11750 Sorrento Valley Rd, Suite 100, San Diego, California 92121 USA, (858) 388-3800, WWW.HEDDESIGN.COM

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L203

WATER CALCULATIONS



CAMERON
BROTHERS COMPANY LLC

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Company LLC

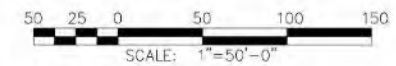
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Santee, CA 92071

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12/16/2022 CUP Submittal
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COLOR SCHEDULE	
SYMBOL	DESCRIPTION
PLANTING	
	PROPOSED LANDSCAPE AREA
	PROPOSED BIOWASH REF. CIVIL
	PROPOSED TREE
	PROPOSED PALM



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Suite 100
San Diego, California
92121 USA

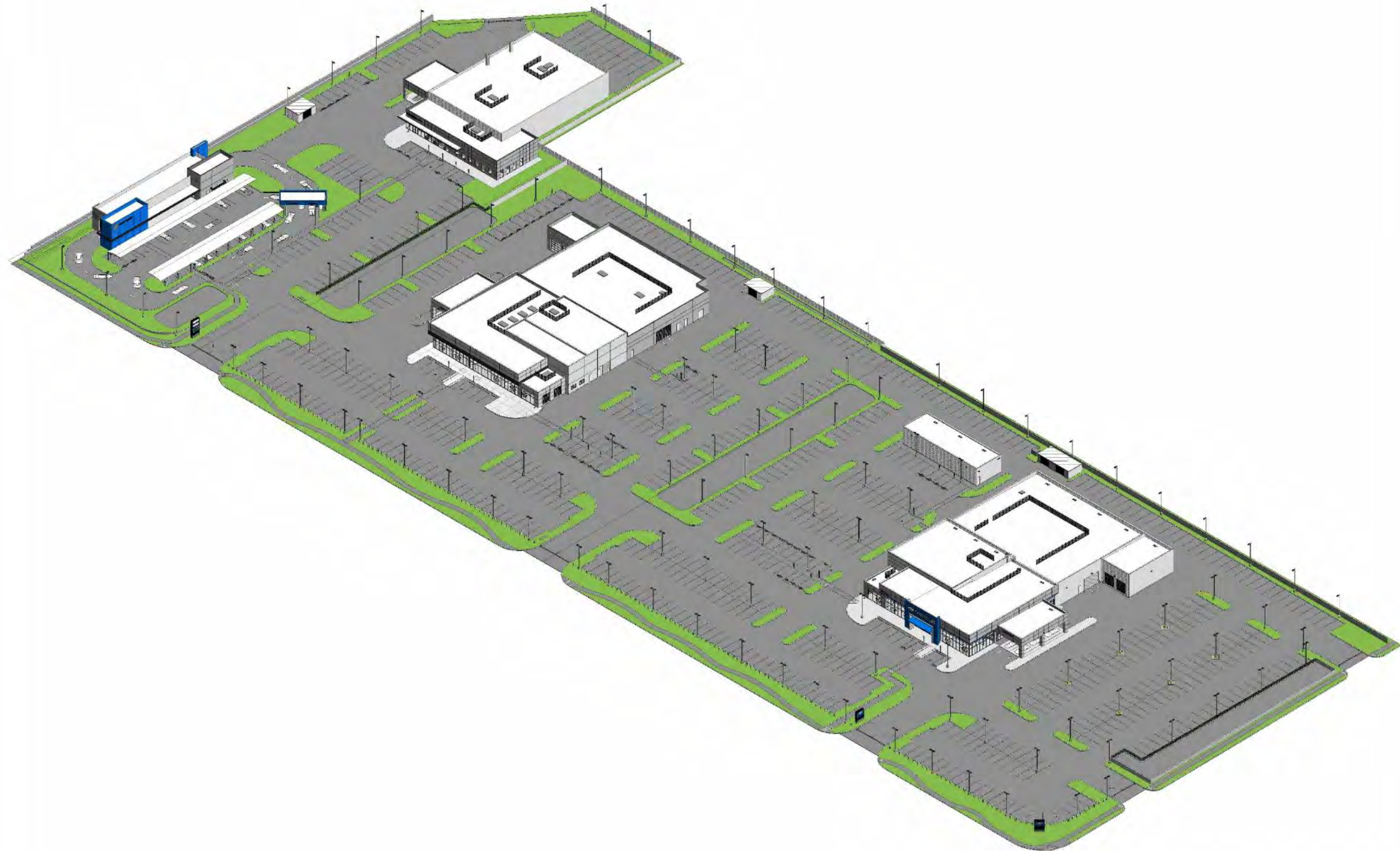
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LANDSCAPE
MASTER PLAN

L204



CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date	Issued For
12/16/2022	CUP Submittal
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WWW.HEDDESIGN.COM

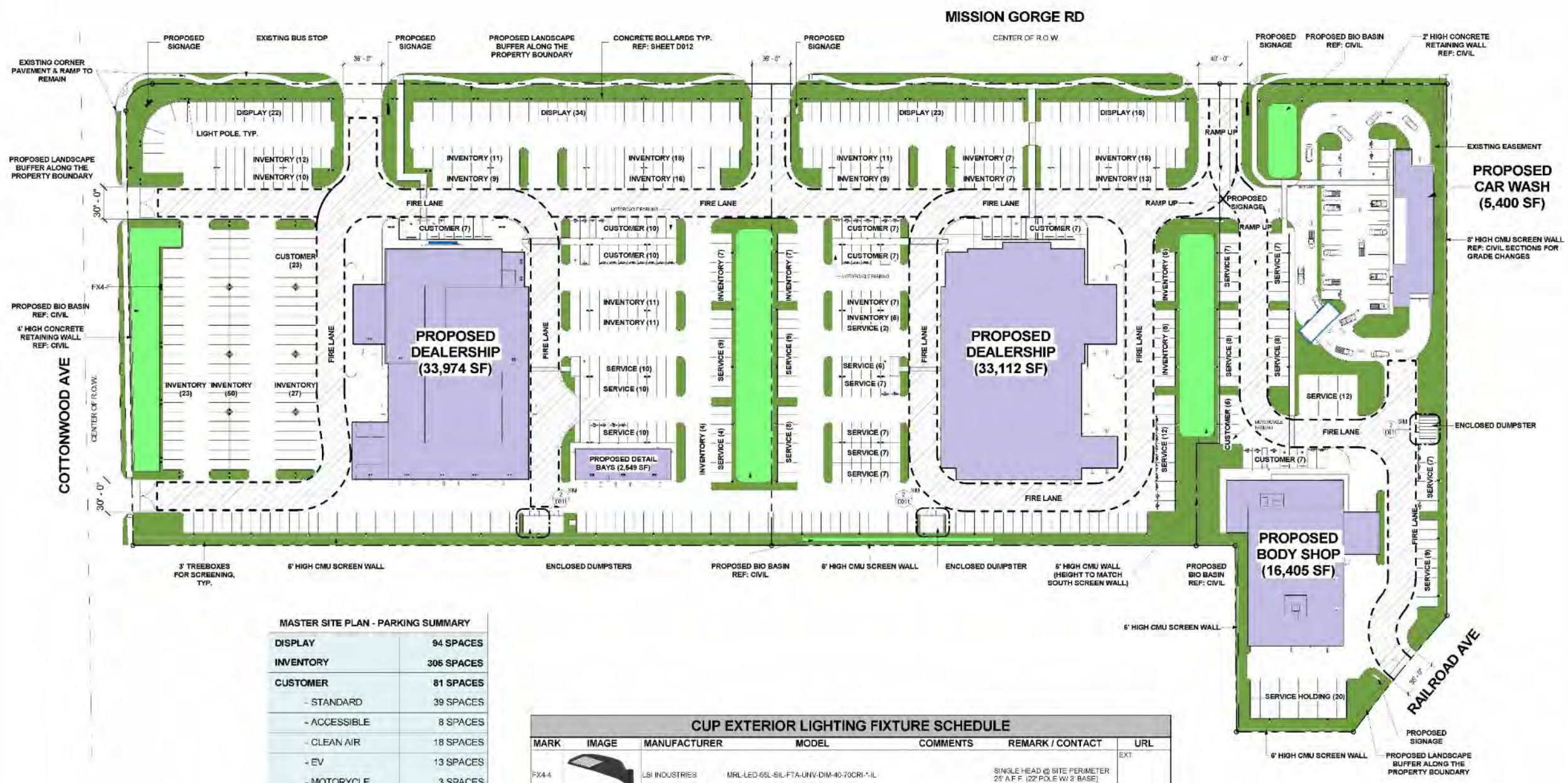
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ISOMETRIC VIEW

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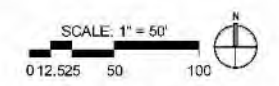
MASTER SITE PLAN - PARKING SUMMARY

DISPLAY	94 SPACES
INVENTORY	306 SPACES
CUSTOMER	81 SPACES
- STANDARD	39 SPACES
- ACCESSIBLE	8 SPACES
- CLEAN AIR	18 SPACES
- EV	13 SPACES
- MOTORCYCLE	3 SPACES
SERVICE / PARTS	186 SPACES
- STANDARD	131 SPACES
- ACCESSIBLE	2 SPACES
- CLEAN AIR	12 SPACES
- EV	9 SPACES
- TANDEM	32 SPACES
VACUUM	22 SPACES
- STANDARD	16 SPACES
- ACCESSIBLE	1 SPACES
- CLEAN AIR	3 SPACES
- EV	2 SPACES
TOTAL PARKING SPACES	688 SPACES

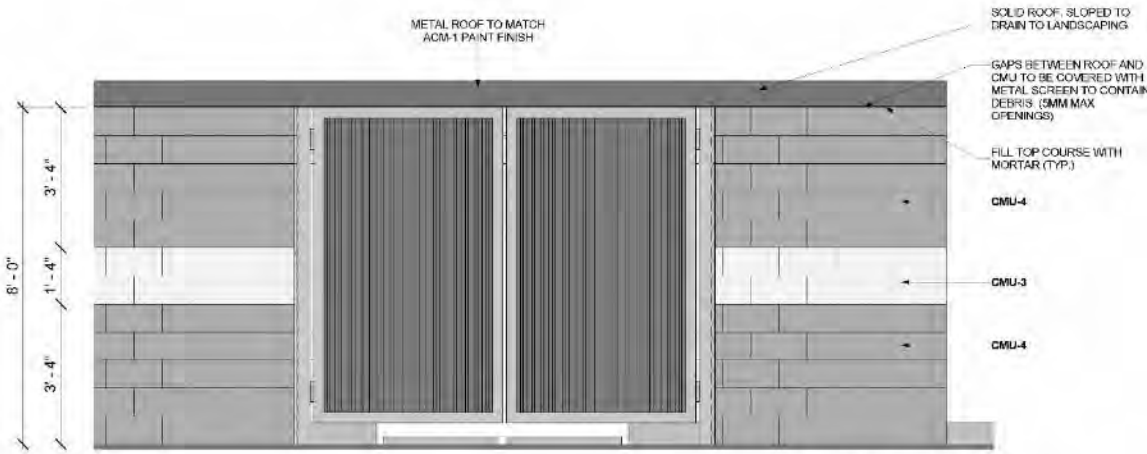
CUP EXTERIOR LIGHTING FIXTURE SCHEDULE

MARK	IMAGE	MANUFACTURER	MODEL	COMMENTS	REMARK / CONTACT	URL
FX4-4		LSI INDUSTRIES	MRL-LED-65L-SIL-FTA-UNV-DIM-40-70CRI-1-IL	SINGLE HEAD @ SITE PERIMETER 25' A.F.F. (22' POLE W/ 3' BASE)		EXT.
FX4-5		LSI INDUSTRIES	MRL-LED-65L-SIL-6W-UNV-DIM-40-70CRI-1-IL	DOUBLE HEAD @ SITE INTERIOR 25' A.F.F. (22' POLE W/ 3' BASE)		EXT.
FX4-F		LSI INDUSTRIES	MRL-LED-50L-SIL-FTA-UNV-DIM-40-70CRI-1-IL	DOUBLE HEAD @ SITE PERIMETER 25' A.F.F. (22' POLE W/ 3' BASE)		EXT.
WP		LSI INDUSTRIES	XWM-3-LED-06L-40	WALL PACK 15' A.F.F.		EXT.

1 MASTER SITE PLAN
1" = 50'-0"



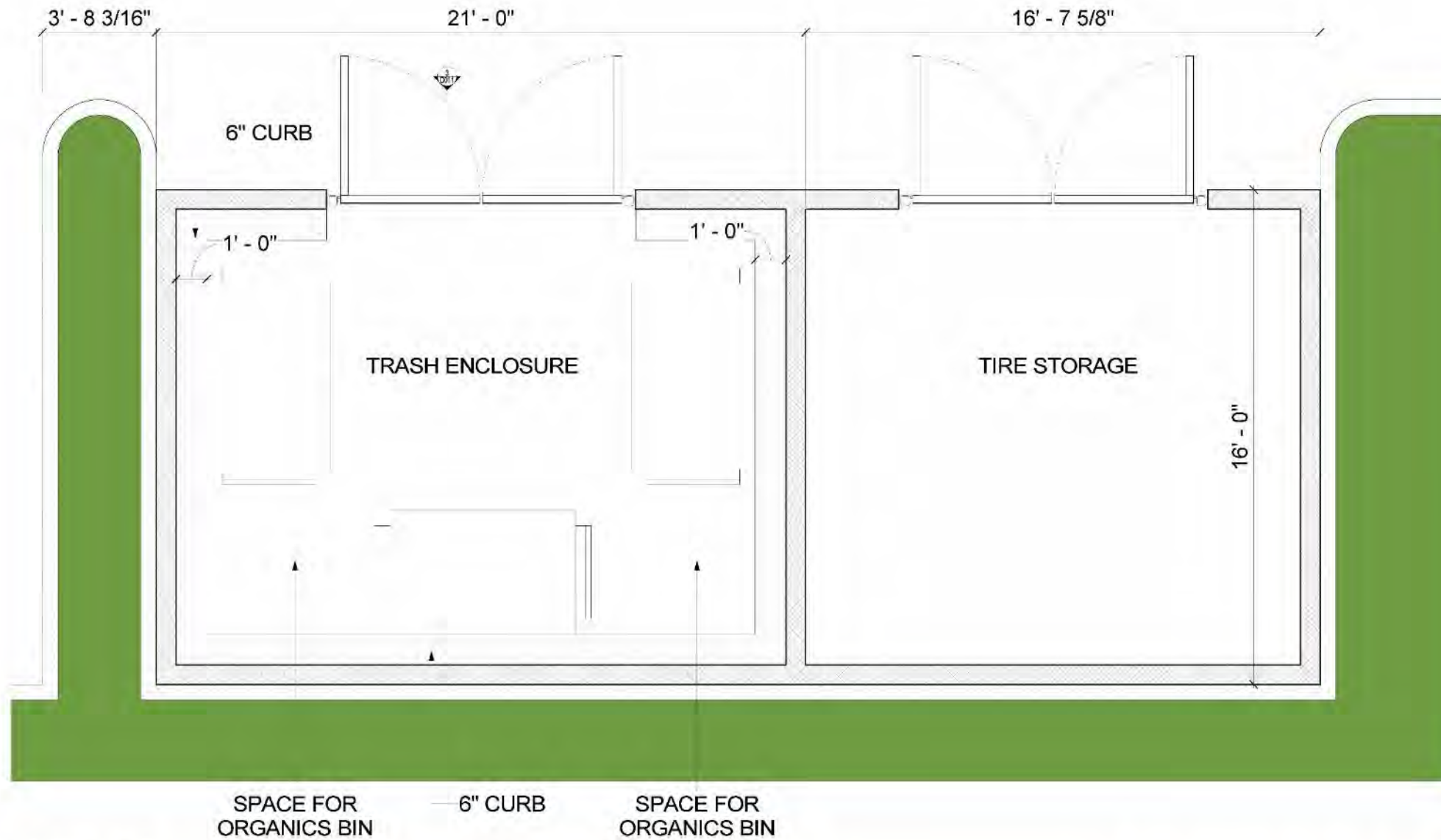
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3 DUMPSTER GATE ELEVATION CUP
1/2" = 1'-0"

NOTE: GAPS ASSOCIATED WITH THE STRUCTURE SHALL BE NO LARGER THAN 1 INCH

CMU SCREEN WALL FINISHES				
NO.	LOCATION	COLOR & MATERIAL SPECIFICATIONS	DESCRIPTIONS	REFERENCE IMAGE
CMU-3	• TRASH ENCLOSURES • SCREEN WALL	NATURAL GREY GROUT TO MATCH	BURNISHED CONCRETE MASONRY UNIT	
CMU-4	• TRASH ENCLOSURES • SCREEN WALL	CHARCOAL GREY	BURNISHED CONCRETE MASONRY UNIT	



2 ENLARGED DUMPSTER PLAN CUP
1/2" = 1'-0"

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TRASH
ENCLOSURE

D011



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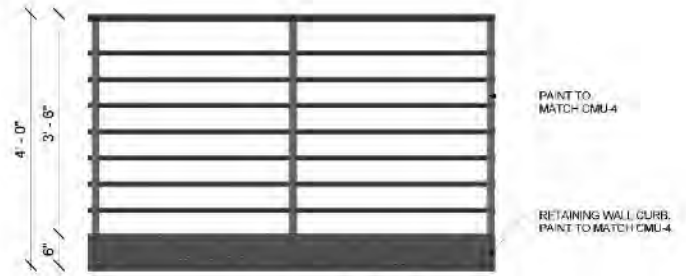
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CMU SCREEN WALL FINISHES

NO.	LOCATION	COLOR & MATERIAL SPECIFICATIONS	DESCRIPTIONS	REFERENCE IMAGE
CMU-3	• TRASH ENCLOSURES • SCREEN WALL	NATURAL GREY GROUT TO MATCH	BURNISHED CONCRETE MASONRY UNIT	
CMU-4	• TRASH ENCLOSURES • SCREEN WALL	CHARCOAL GREY	BURNISHED CONCRETE MASONRY UNIT	



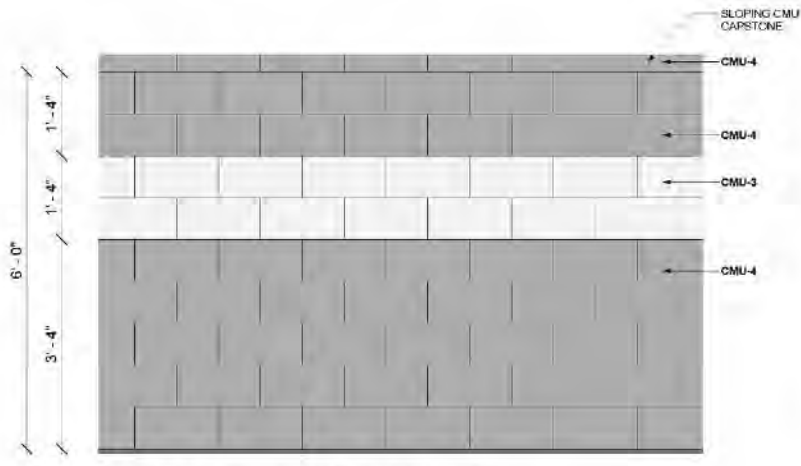
8 BIO BASIN GUARDRAIL
3/4" = 1'-0"



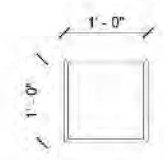
6 2' CONCRETE RETAINING WALL
3/4" = 1'-0"



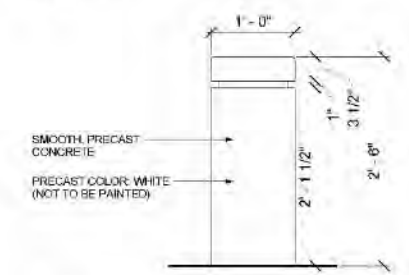
7 4' CONCRETE RETAINING WALL
3/4" = 1'-0"



3 TYP. 6' SCREEN WALL ELEVATION
3/4" = 1'-0"



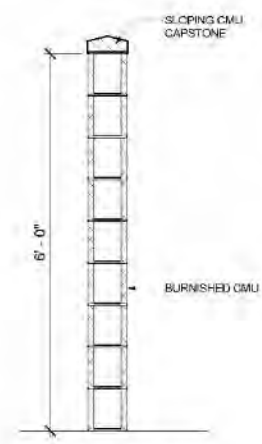
4 CONCRETE BOLLARD - PLAN
1" = 1'-0"



1 CONCRETE BOLLARD - ELEVATION
1" = 1'-0"



5 8' SCREEN WALL TO 2' RETAINING WALL
3/4" = 1'-0"



2 SCREEN WALL SECTION
3/4" = 1'-0"

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SITE DETAILS

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PARKING COUNT REQUIREMENTS

SANTEE, CALIFORNIA MUNICIPAL CODE (SMC)
13.24.040 PARKING REQUIREMENTS

USE CLASS	SQ. FT.	REQUIRED	PROVIDED
DISPLAY / INVENTORY	N/A	N/A	264 SPACES
FIRST FLOOR (DEALERSHIP) MOTOR VEHICLE SALES, AUTOMOTIVE REPAIR, PAINTING, BODY WORK OR SERVICE (1 PER 400 SF)	30,992	78 SPACES	78 SPACES
FIRST FLOOR (DETAIL BAYS) MOTOR VEHICLE SALES, AUTOMOTIVE REPAIR, PAINTING, BODY WORK OR SERVICE (1 PER 400 SF)	2,549	7 SPACES	7 SPACES
SECOND FLOOR (PARTS STORAGE) MOTOR VEHICLE SALES, AUTOMOTIVE REPAIR, PAINTING, BODY WORK OR SERVICE (1 PER 400 SF)	2,962	8 SPACES	8 SPACES
TOTALS	36,503	93 SPACES	357 SPACES

CBC TABLE 11B-208.2 - REQUIRED ACCESSIBLE SPACES

93 REQUIRED SPACES
78 TO 100 = 4 ACCESSIBLE SPACES REQUIRED
ADA PARKING PROVIDED: 4 SPACES (1 VAN ACCESSIBLE)

(SMC) TABLE 13.24.040A - REQUIRED CLEAN AIR SPACES

93 REQUIRED SPACES
78 TO 100 = 8 CLEAN AIR SPACES PROVIDED
CLEAN AIR VEHICLE PARKING PROVIDED: 8 SPOTS

CGBC TABLE 6.106.6.3.3 - REQUIRED EV SPACES

93 REQUIRED SPACES
78 TO 100 = 4 EVCS & 17 EV CAPABLE SPACES REQUIRED
EV PARKING PROVIDED: 7 SPACES (1 VAN ACCESSIBLE, 1 STANDARD ACCESSIBLE) THESE 7 COUNT TOWARDS THE 17 EV CAPABLE SPACES*

PER SMC, MINIMUM LEVEL 2 ELECTRIC VEHICLE CHARGERS MUST BE PROVIDED FOR 5% OF REQUIRED PARKING = 5 SPACES (WE ARE CURRENTLY PROVIDING FOR 7 SPACES)

CGBC 6.106.4.1 - REQUIRED BICYCLE PARKING

SHORT TERM: 25 x (0.05%) = 125 BIKE SPOTS
ONE, TWO CAPACITY BIKE RACK REQUIRED
LONG TERM: NOT REQUIRED

SANTEE MUNICIPAL CODE 13.24 REQUIRED MOTORCYCLE PARKING

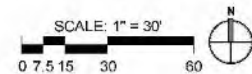
25 OR MORE PARKING SPACES = 1 PARKING AREA (54 SQFT MINIMUM)

*REMAINDER PARKING STALLS ARE: DISPLAY/INVENTORY

PROPOSED BIO BASIN
REF: CIVIL

PROPERTY
LINE

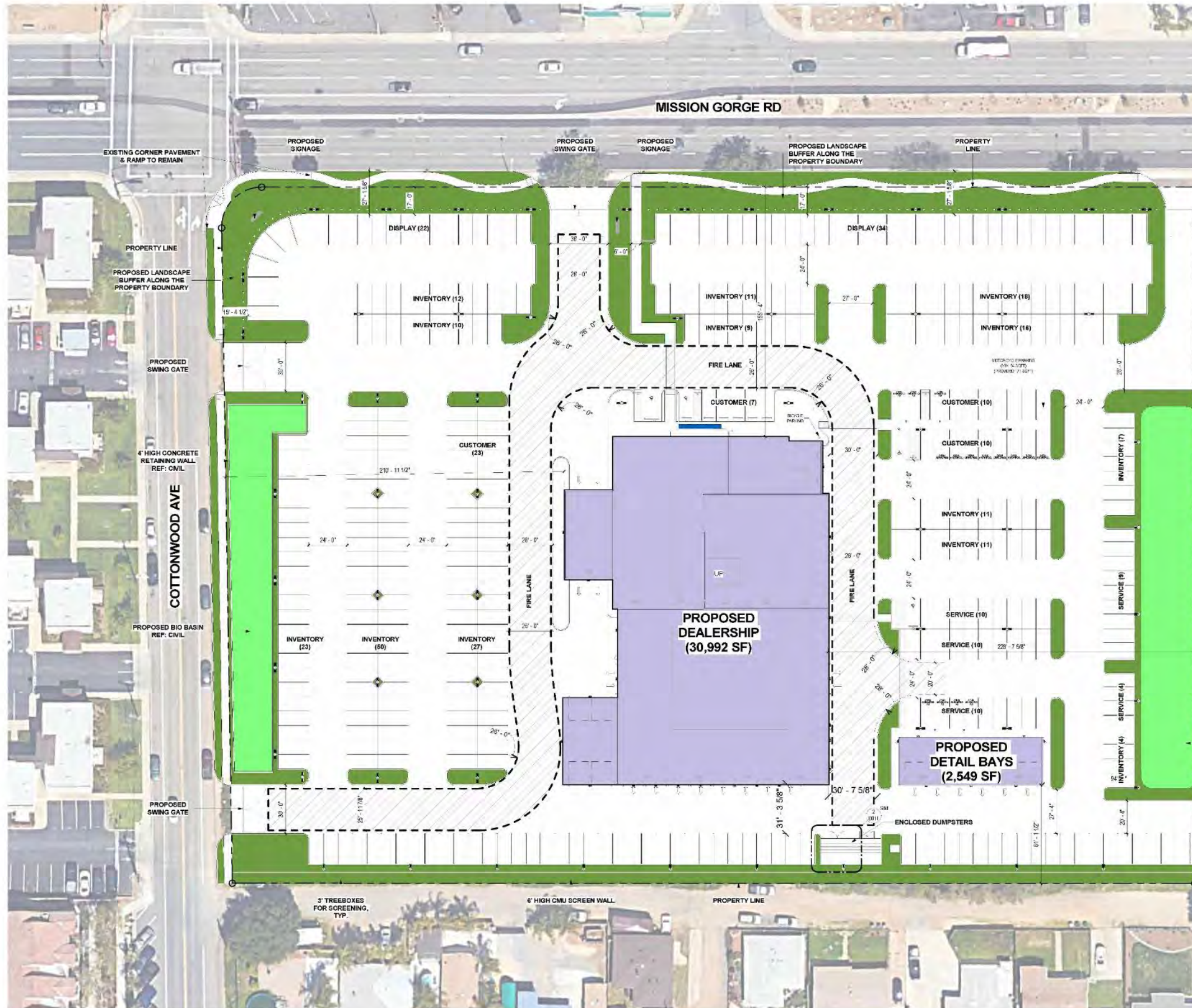
1 PARCEL A - VICINITY SITE PLAN
1" = 30'-0"



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(A) VICINITY SITE
PLAN

D100



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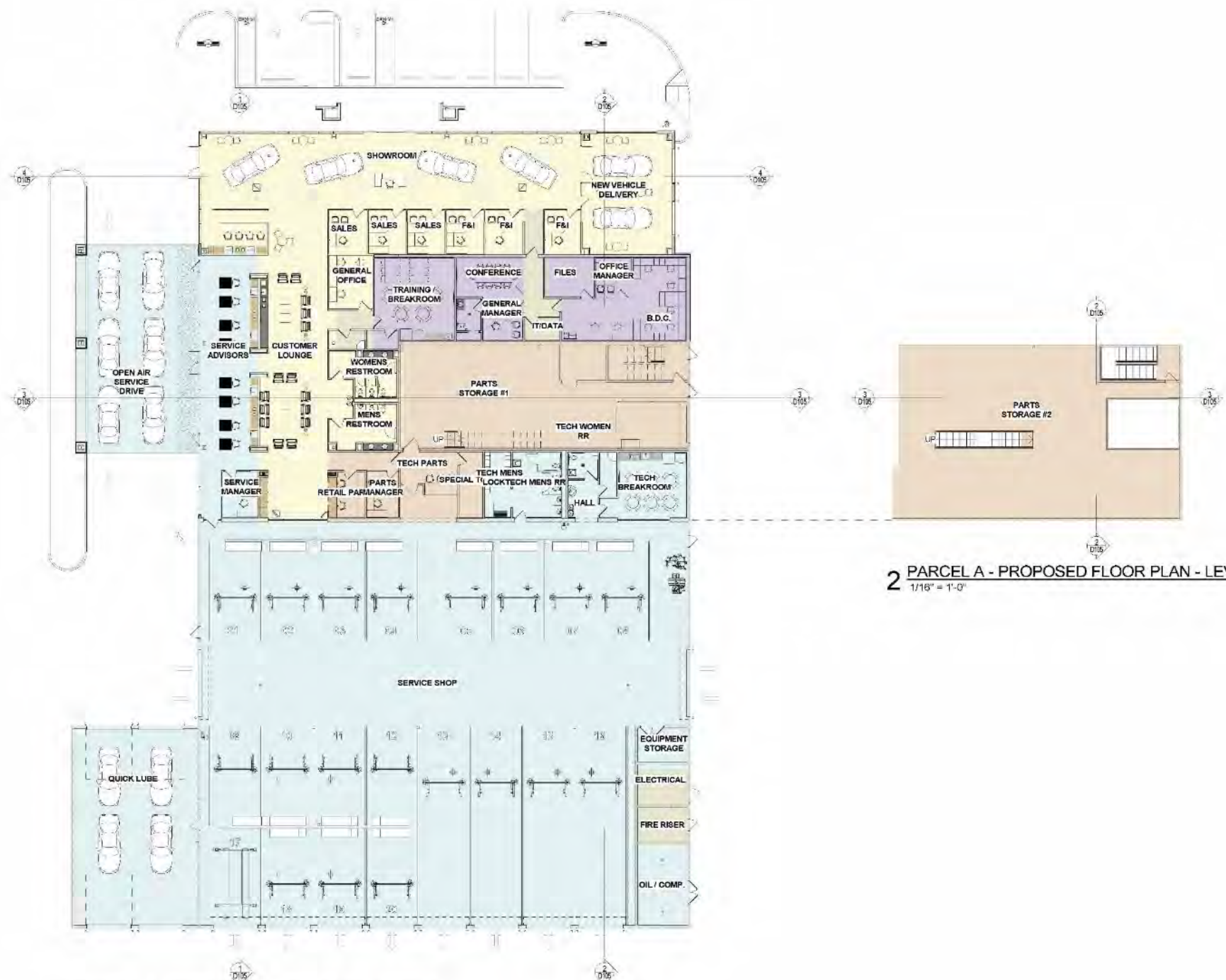


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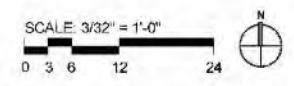
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1 PARCEL A - PROPOSED FLOOR PLAN - LEVEL 1
1/16" = 1'-0"

2 PARCEL A - PROPOSED FLOOR PLAN - LEVEL 2
1/16" = 1'-0"

PROGRAMMING		
PROGRAM TYPE	REQUIRED UNIT / AREA	PROPOSED UNIT / AREA
SALES / SHOWROOM		
SHOWROOM DISPLAY	TBD	4 VEHICLES
SALES / F&I / B.D.C.	TBD	3,091 SF
OPEN SALES	TBD	4 SALES
F&I OFFICES	TBD	3 OFFICES
CUSTOMER LOUNGE	TBD	2,312 SF
N.V.D.	TBD	2 VEHICLES / 849 SF
		6,252 SF
PARTS DEPARTMENT		
PARTS DEPARTMENT	TBD	2,221 SF
PARTS MEZZANINE	TBD	3,129 SF
		5,350 SF
SERVICE DEPARTMENT		
SERVICE SHOP	TBD	16 STALLS / 17,016 SF
QUICK LUBE	TBD	2 BAYS / INCL. ABOVE
HEAVY LINE SERVICE	TBD	4 BAYS / INCL. ABOVE
SERVICE DRIVE CANOPY	TBD	6 VEHICLES / 1,848 SF
SERVICE ADVISORS	TBD	8 ADVISORS / 1,189 SF
		20,063 SF
ADMIN. OFFICE		
GENERAL OFFICE	TBD	1,279 SF
TRAINING / BREAK	TBD	533 SF
		1,812 SF
SUPPORT		
BUILDING SUPPORT	TBD	653 SF
		653 SF
LEVEL 1 TOTAL		TBD 30,991 SF
LEVEL 2 TOTAL		TBD 3,129 SF
BUILDING TOTAL:		TBD 34,120 SF
WASH / DETAIL		
	TBD	6 STALLS / 2,549 SF
GRAND TOTAL:		TBD 36,669 SF



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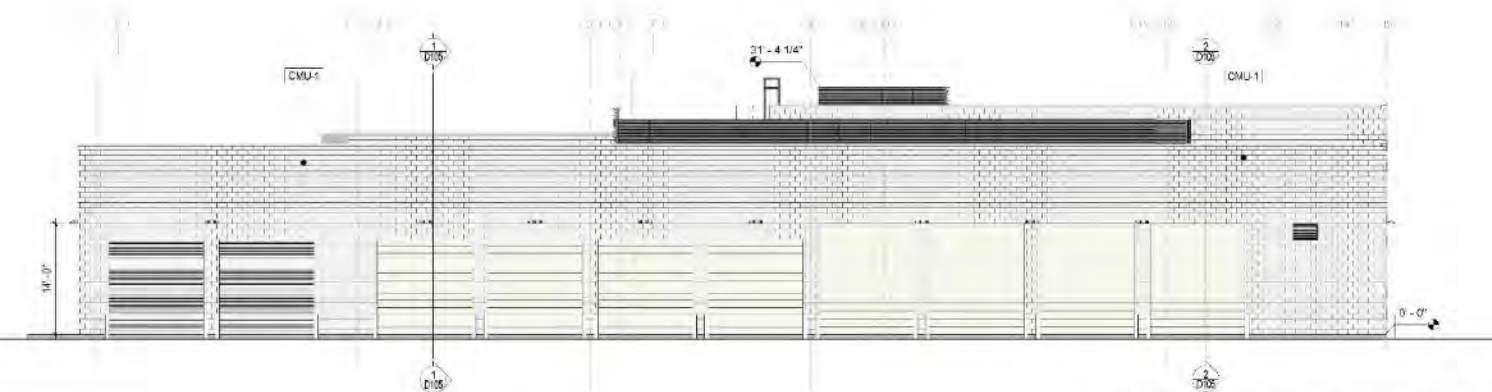
(A) PROPOSED FLOOR PLANS

D102

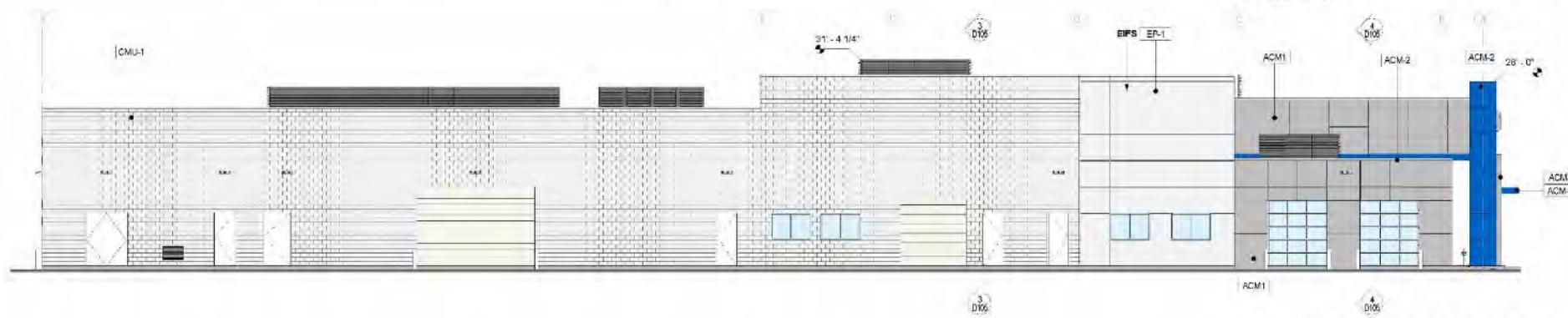
EXTERIOR FINISHES				
NO.	LOCATION	COLOR & MATERIAL SPECIFICATIONS	DESCRIPTIONS	REFERENCE IMAGE
ACM-1	EXTERIOR MAIN SHOWROOM STRUCTURE & SERVICE DRIVE FACADE	AL POLIC TEK BRIGHT ALUMINUM METALLIC	ALUMINUM COMPOSITE MATERIAL	
ACM-2	ENTRY ELEMENT	AL POLIC C/8 BLUE COLOR TO MATCH FINE SS CHEVROLET BLUE	ALUMINUM COMPOSITE MATERIAL	
ACM-3	ENTRY ELEMENT	ALUCORON NATURAL UNFINISHED TO FEVE GLOSS SPS	ALUMINUM COMPOSITE MATERIAL	
EP-1	SERVICE SHOP STRUCTURE AND ANY OTHER BUILDING EXTERIOR EXCEPT MAIN SHOWROOM	BERLAMIN TROCKE CS-41 WHITE UNFINISHED	EXTERIOR GRADE PAINT	
CMU-1	SERVICE SHOP STRUCTURE AND ANY OTHER BUILDING EXTERIOR EXCEPT MAIN SHOWROOM	INTERIAL COLOR TO MATCH EXIST (GLAZING TO MATCH) (IF INTERIAL COLOR CANNOT MATCH EP-1 CMU WALL IS TO BE PAINTED EP-1)	BURNISHED CONCRETE MASONRY UNIT	
GL-1	GLAZING GLASS DOORS	CLEAR GLASS WITH ANODIZED ALUMINUM APPEARANCE FRAMING	GLAZING	



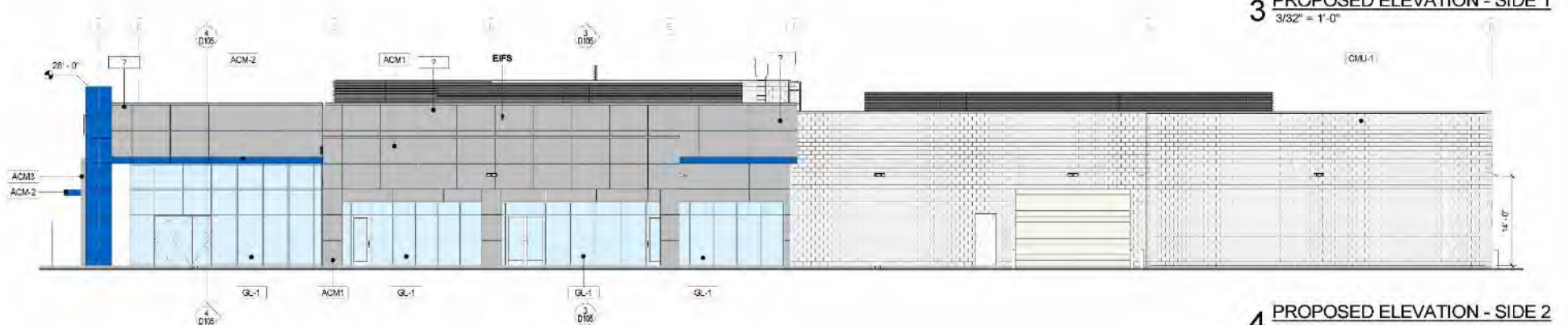
1 PROPOSED ELEVATION - FRONT
3/32" = 1'-0"



2 PROPOSED ELEVATION - REAR
3/32" = 1'-0"



3 PROPOSED ELEVATION - SIDE 1
3/32" = 1'-0"



4 PROPOSED ELEVATION - SIDE 2
3/32" = 1'-0"

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(A) PROPOSED
BUILDING
ELEVATIONS

D104



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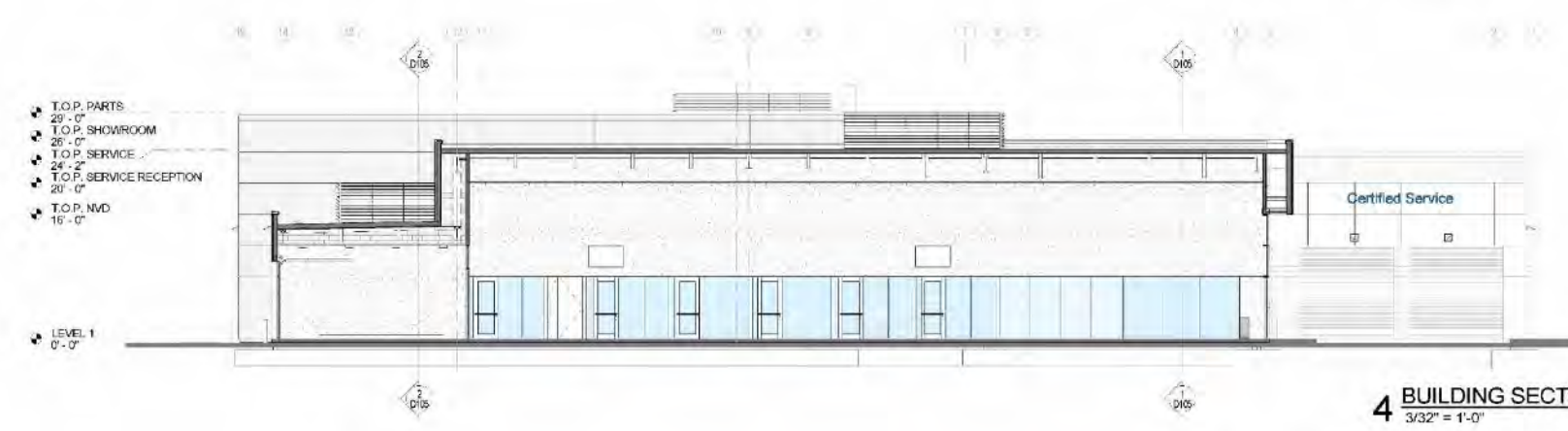
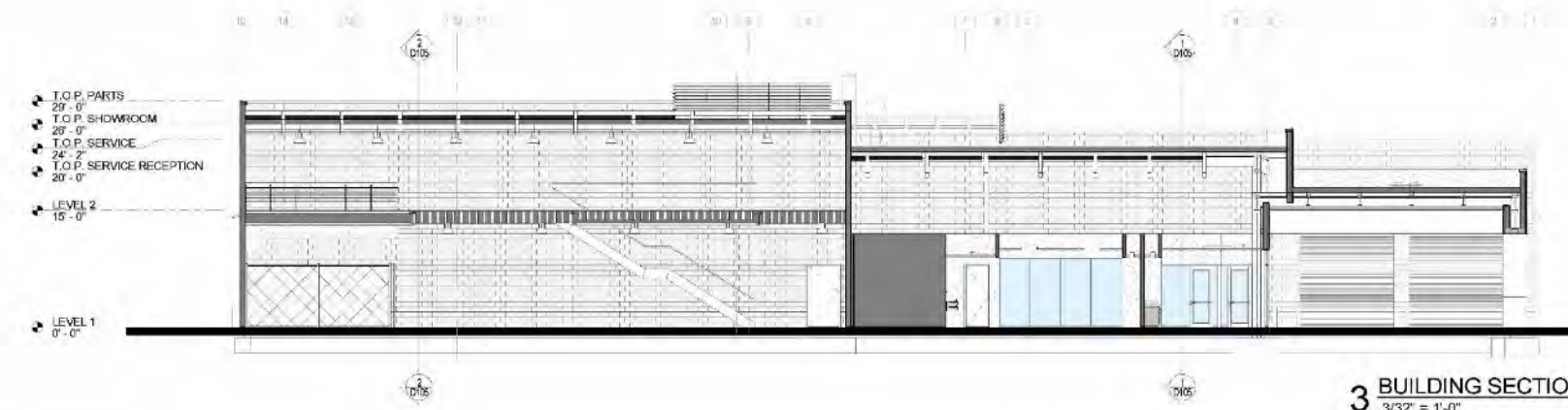
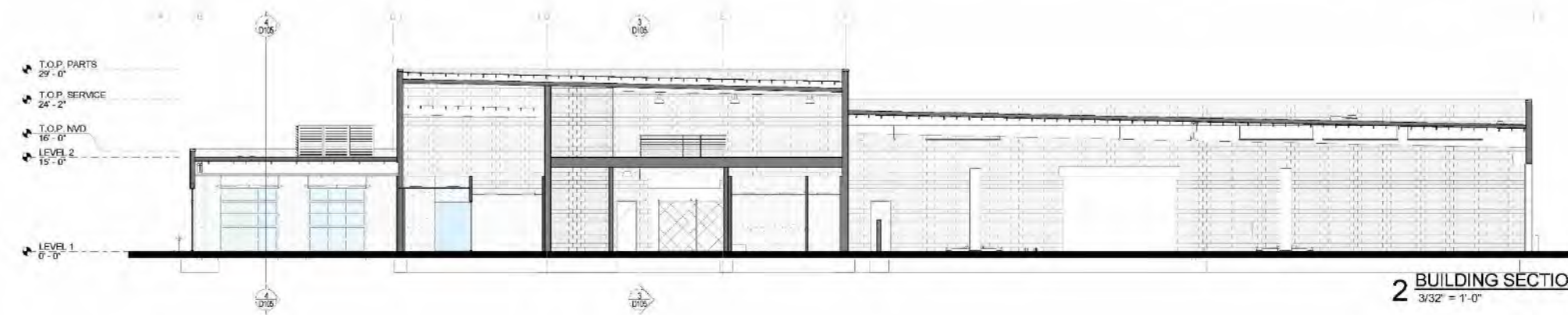
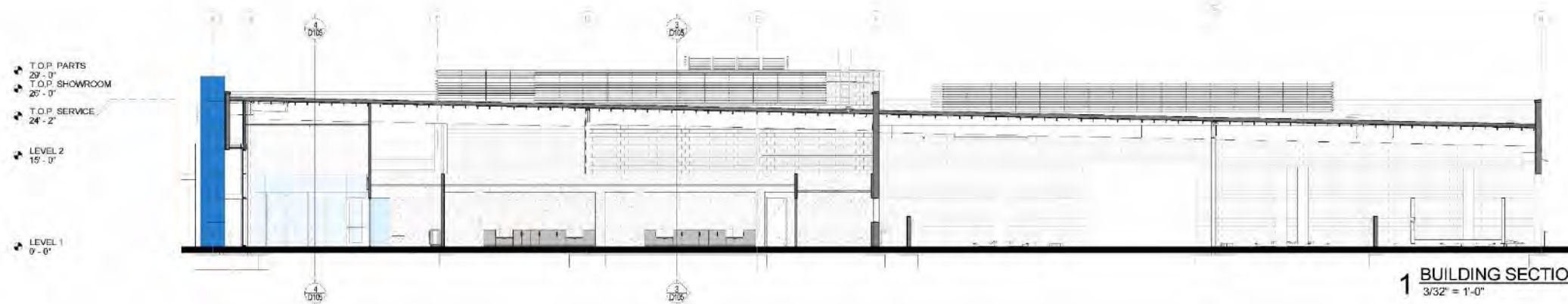
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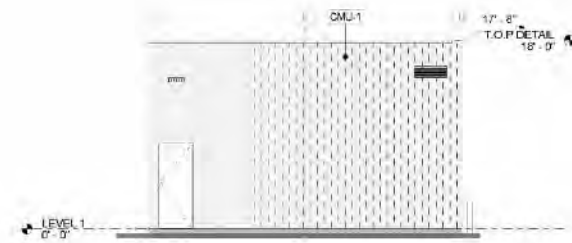
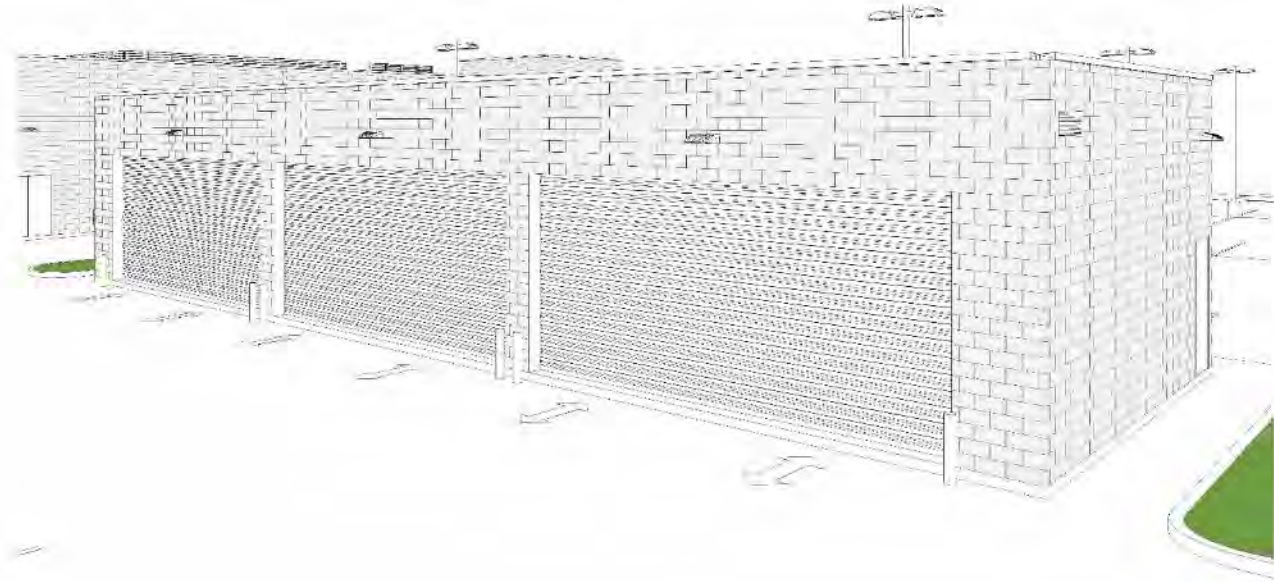
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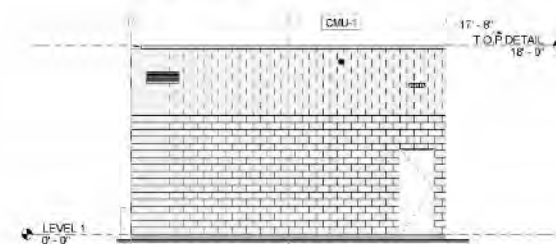
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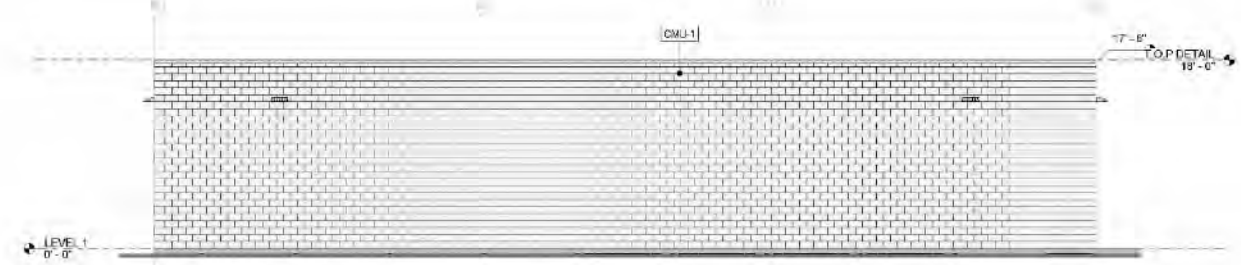
BUILDING SQ.FT	
USE CLASS	SQ. FT.
DETAIL BAYS	2,546
TOTAL	2,546



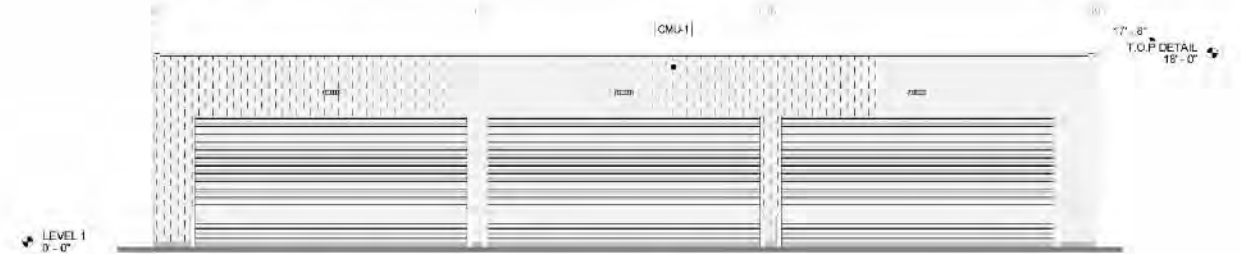
5 PARCEL A DETAIL BAYS WEST ELEVATION
1/8" = 1'-0"



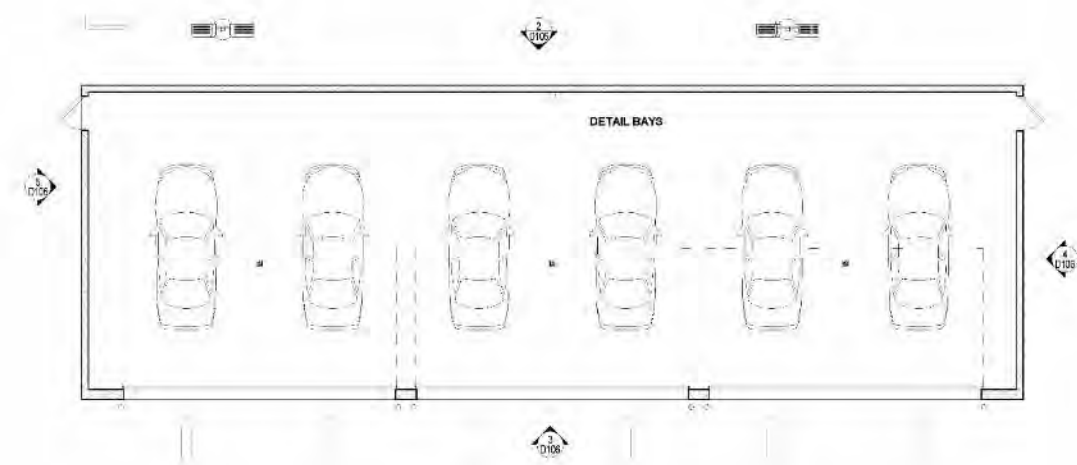
4 PARCEL A DETAIL BAYS EAST ELEVATION
1/8" = 1'-0"



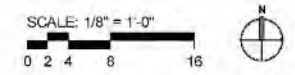
2 PARCEL A DETAIL BAYS NORTH ELEVATION
1/8" = 1'-0"



3 PARCEL A DETAIL BAYS SOUTH ELEVATION
1/8" = 1'-0"



1 PROPOSED PLAN - DETAIL BAYS
1/8" = 1'-0"



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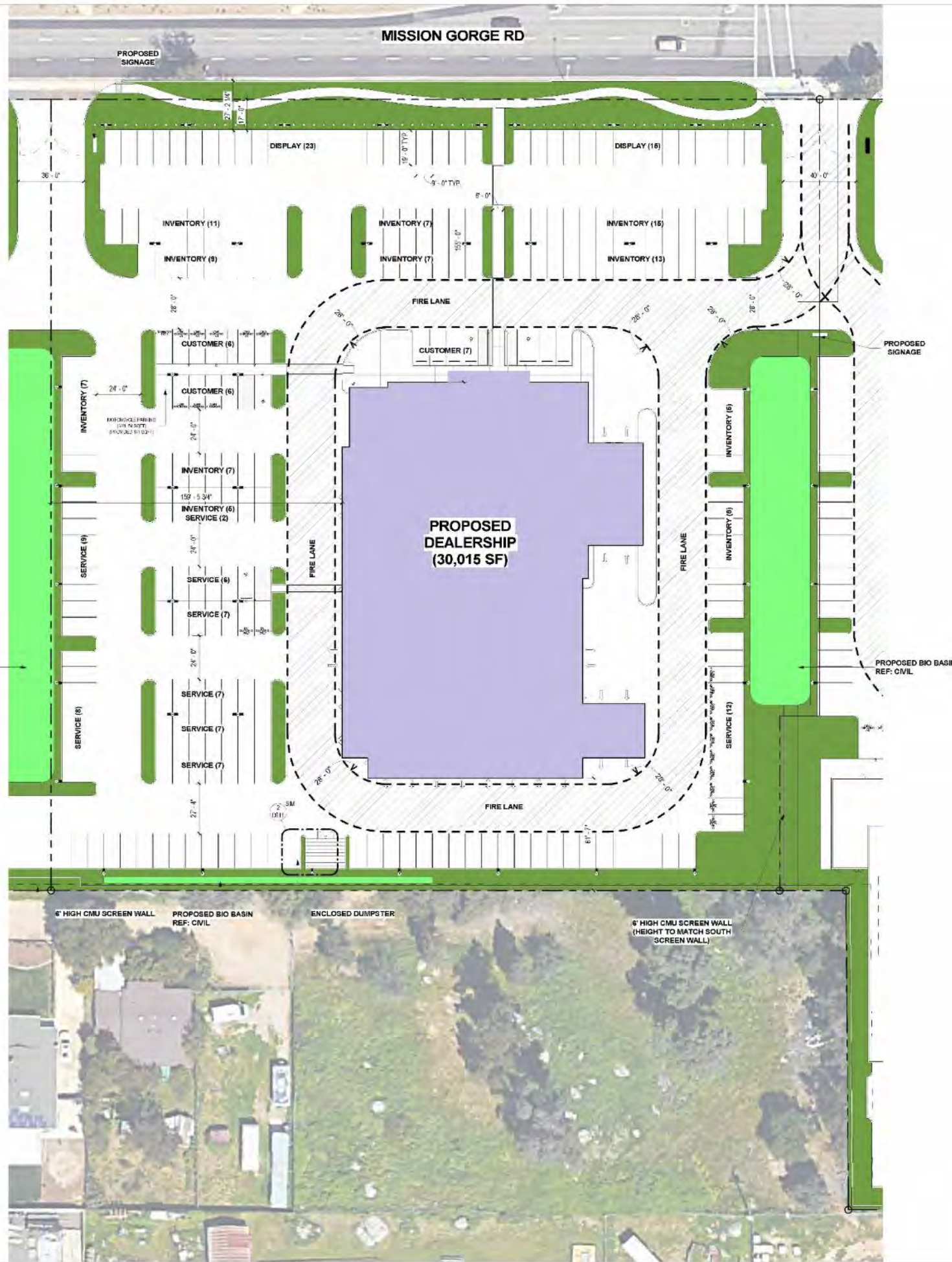
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D106



PARKING COUNT REQUIREMENTS

SANTEE, CALIFORNIA MUNICIPAL CODE
13.24.040 PARKING REQUIREMENTS

USE CLASS	SQ. FT.	REQUIRED	PROVIDED
DISPLAY / INVENTORY	N/A	N/A	134 SPACES
FIRST FLOOR (DEALERSHIP) MOTOR VEHICLE SALES, AUTOMOTIVE REPAIR, PAINTING, BODY WORK OR SERVICE (1 PER 400 SF)	30,015	76 SPACES	76 SPACES
SECOND FLOOR (PARTS STORAGE) MOTOR VEHICLE SALES, AUTOMOTIVE REPAIR, PAINTING, BODY WORK OR SERVICE (1 PER 400 SF)	3,097	8 SPACES	8 SPACES
TOTAL PARKING	33,112	84 SPACES	218 SPACES

CBC TABLE 11B-202.2 - REQUIRED ACCESSIBLE SPACES
84 REQUIRED SPACES
76 TO 100 = 4 ACCESSIBLE SPACES REQUIRED
ADA PARKING PROVIDED: 4 SPACES (1 VAN ACCESSIBLE)

CBC TABLE 6.106.6.2 - REQUIRED CLEAN AIR SPACES
84 REQUIRED SPACES
76 TO 100 = 12 CLEAN AIR SPACES REQUIRED
CLEAN AIR VEHICLE PARKING PROVIDED: 12 SPOTS

CBC TABLE 6.106.6.3.3 - REQUIRED EV SPACES
84 REQUIRED SPACES
76 TO 100 = 9 EV SPACES REQUIRED
EV PARKING PROVIDED: 9 SPOTS (1 VAN ACCESSIBLE, 1 STANDARD ACCESSIBLE)

CBC 6.106.4.1 - REQUIRED BICYCLE PARKING
SHORT TERM: 25 x (0.5%) = 1.25 BIKE SPOTS
ONE TWO CAPACITY BIKE RACK REQUIRED
LONG TERM: NOT REQUIRED

SANTEE (MUNICIPAL CODE 13.24) REQUIRED MOTORCYCLE PARKING
25 OR MORE PARKING SPACES = 1 PARKING AREA (54 SQFT MINIMUM)

*REMAINDER PARKING STALLS ARE DISPLAY/INVENTORY



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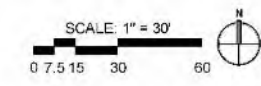
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1 PARCEL B - VICINITY SITE PLAN
1" = 30'-0"



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(B) VICINITY SITE
PLAN

D200



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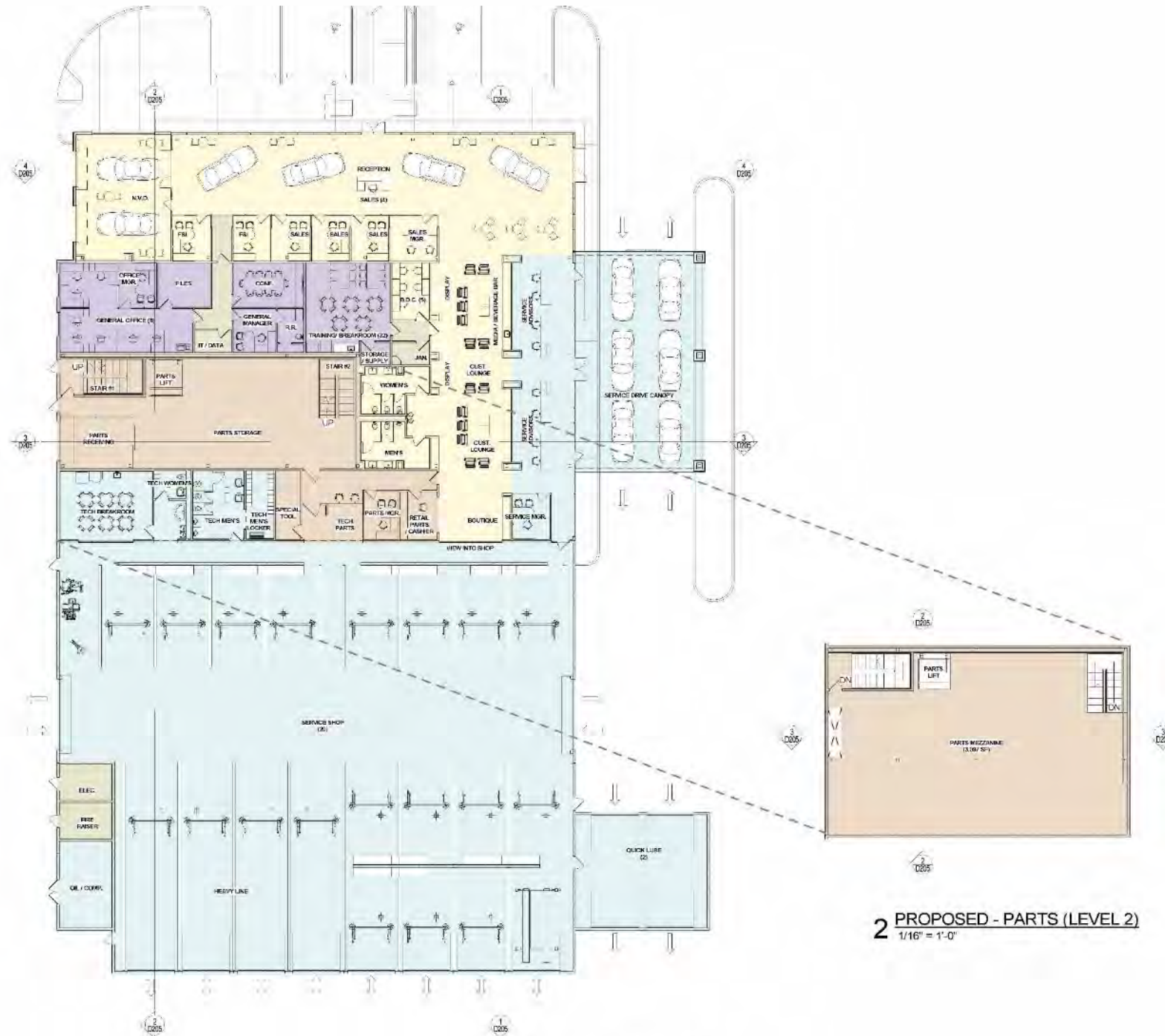
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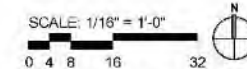
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1 PROPOSED - LEVEL 1
1/16" = 1'-0"

2 PROPOSED - PARTS (LEVEL 2)
1/16" = 1'-0"

PROGRAMMING		
PROGRAM TYPE	REQUIRED UNIT / AREA	PROPOSED UNIT / AREA
SALES / SHOWROOM		
SHOWROOM DISPLAY	TBD	4 VEHICLES
SALES / F&I / B.D.C.	TBD	3,091 SF
OPEN SALES	TBD	4 SALES
F&I OFFICES	TBD	3 OFFICES
CUSTOMER LOUNGE	TBD	2,312 SF
N.V.D.	TBD	2 VEHICLES / 849 SF
		6,252 SF
PARTS DEPARTMENT		
PARTS DEPARTMENT	TBD	2,221 SF
PARTS MEZZANINE	TBD	3,097 SF
		5,318 SF
SERVICE DEPARTMENT		
SERVICE SHOP	TBD	16 STALLS / 16,040 SF
QUICK LUBE	TBD	2 BAYS / INCL. ABOVE
HEAVY LINE SERVICE	TBD	4 BAYS / INCL. ABOVE
SERVICE DRIVE CANOPY	TBD	6 VEHICLES / 1,848 SF
SERVICE ADVISORS	TBD	8 ADVISORS / 1,189 SF
		19,077 SF
ADMIN. OFFICE		
GENERAL OFFICE	TBD	1,279 SF
TRAINING / BREAK	TBD	533 SF
		1,812 SF
SUPPORT		
BUILDING SUPPORT	TBD	653 SF
		653 SF
LEVEL 1 TOTAL	TBD	30,015 SF
LEVEL 2 TOTAL	TBD	3,097 SF
BUILDING TOTAL:	TBD	33,112 SF



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Suite 100
San Diego, California
92121 USA
(858) 398-3800
WWW.HED.DESIGN

02/2022
BD22-CB036-004

(B) PROPOSED
FLOOR PLANS

D202

EXTERIOR FINISHES

NO.	LOCATION	COLOR & MATERIAL SPECIFICATIONS	DESCRIPTIONS	REFERENCE IMAGE
ACM-1	EXTERIOR AT MAIN SHOWROOM STRUCTURE & SERVICE DRIVE FACADE	AL POLIC TBD	ALUMINUM COMPOSITE MATERIAL	
ACM-2	ENTRY ELEMENT	AL POLIC TBD	ALUMINUM COMPOSITE MATERIAL	
ACM-3	ENTRY ELEMENT	AL POLIC TBD	ALUMINUM COMPOSITE MATERIAL	
EIFS-1	SERVICE BUILDING STRUCTURE AND ANY OTHER BUILDING STRUCTURE EXCEPT MAIN SHOWROOM	BENJAMIN MOORE TBD	EXTERIOR GRADE PAINT	
TILT-1	EXTERIOR AT MAIN SHOWROOM STRUCTURE PAINT TO MATCH ACM-1	BENJAMIN MOORE TBD	EXTERIOR GRADE PAINT	
GL-1	CLADDING, GLASS DOORS	CLERESTORY GLASS WITH ANODIZED ALUMINUM APPEARANCE FRAMING	GLAZING	

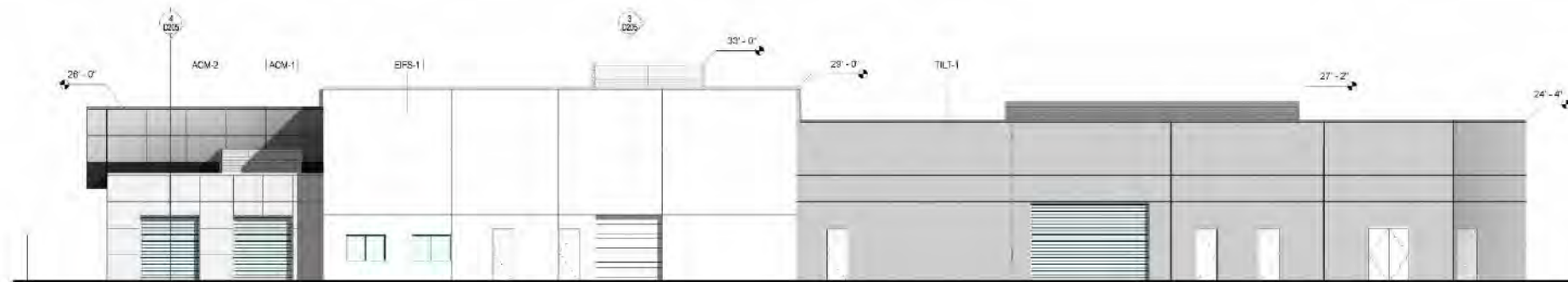
THESE FINISHES ARE TBD UNTIL A BRAND IS SELECTED. THIS SCHEDULE IS A PLACEHOLDER. THEY DO NOT CURRENTLY MATCH FINISHES SHOWN ON PARCELS A & C.



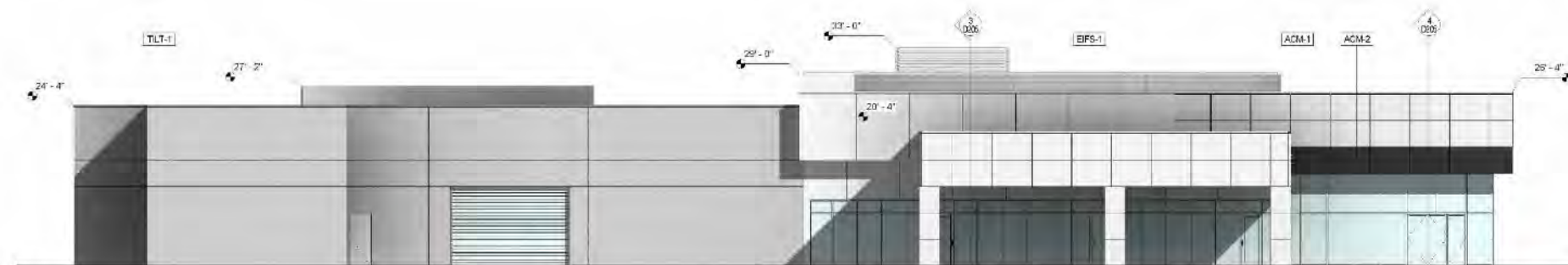
1 PROPOSED ELEVATION - FRONT
3/32" = 1'-0"



2 PROPOSED ELEVATION - REAR
3/32" = 1'-0"



3 PROPOSED ELEVATION - SIDE 1
3/32" = 1'-0"



4 PROPOSED ELEVATION - SIDE 2
3/32" = 1'-0"

**FOR REFERENCE ONLY
NOT FOR CONSTRUCTION**



CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CLP Submitted
06/23/2023 CLP Residential
08/11/2023 CLP Residential

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6022-CB036-004

(B) PROPOSED
BUILDING
ELEVATIONS

D204



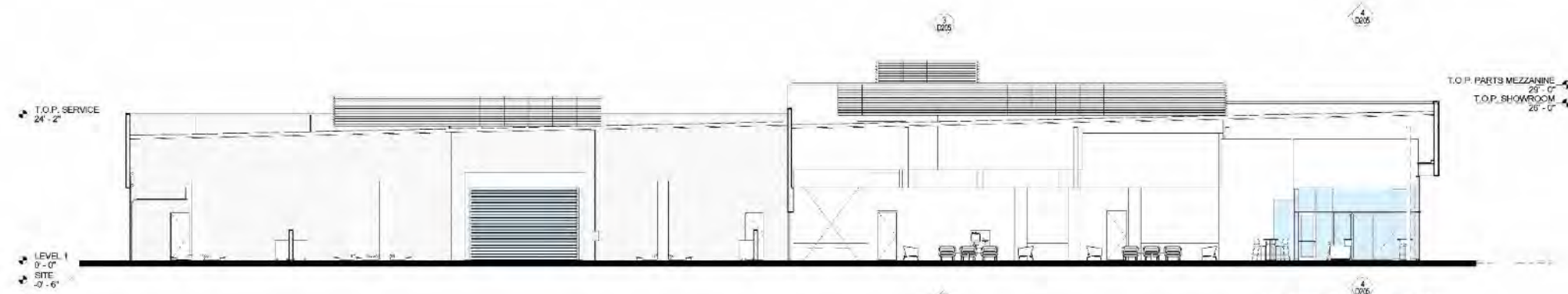
CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

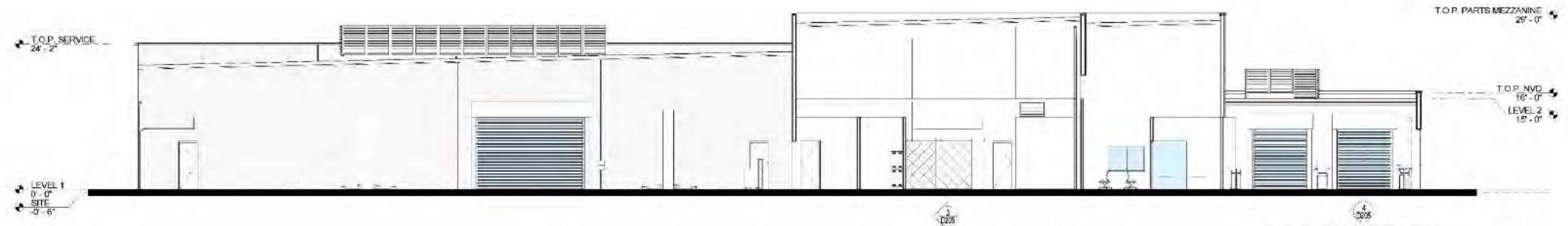
Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

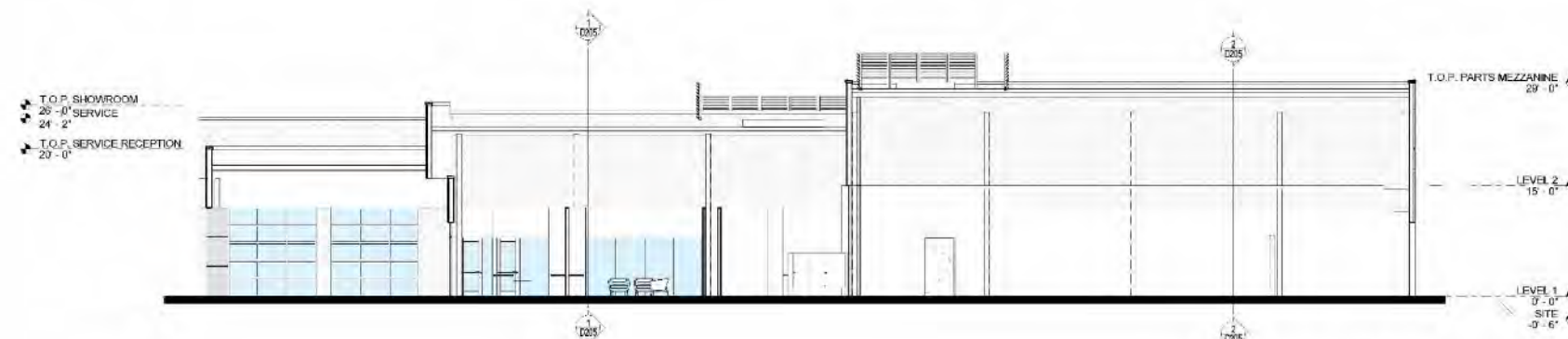
Date Issued For
12/8/2022 CUP Submittal
09/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal



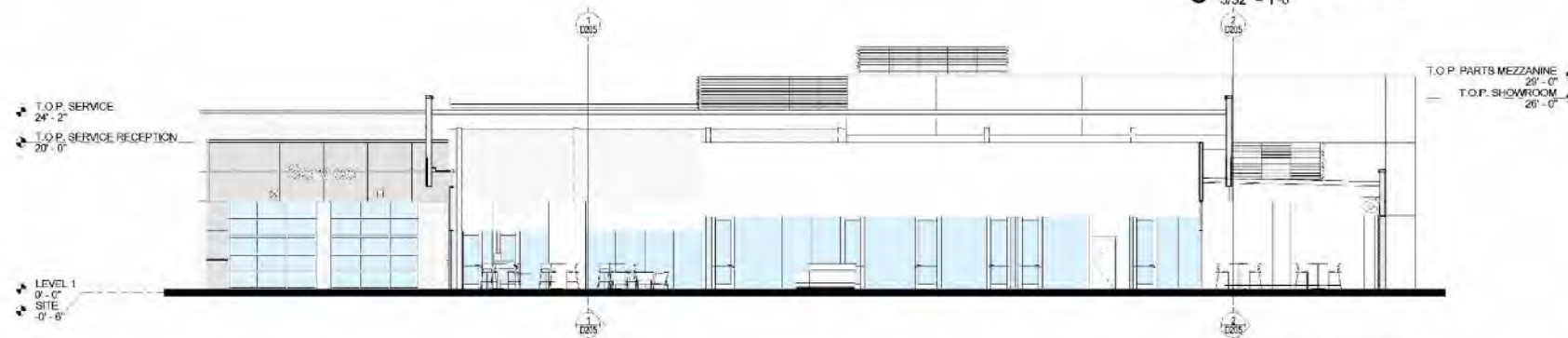
1 BUILDING SECTION
3/32" = 1'-0"



2 BUILDING SECTION
3/32" = 1'-0"



3 BUILDING SECTION
3/32" = 1'-0"



4 BUILDING SECTION
3/32" = 1'-0"

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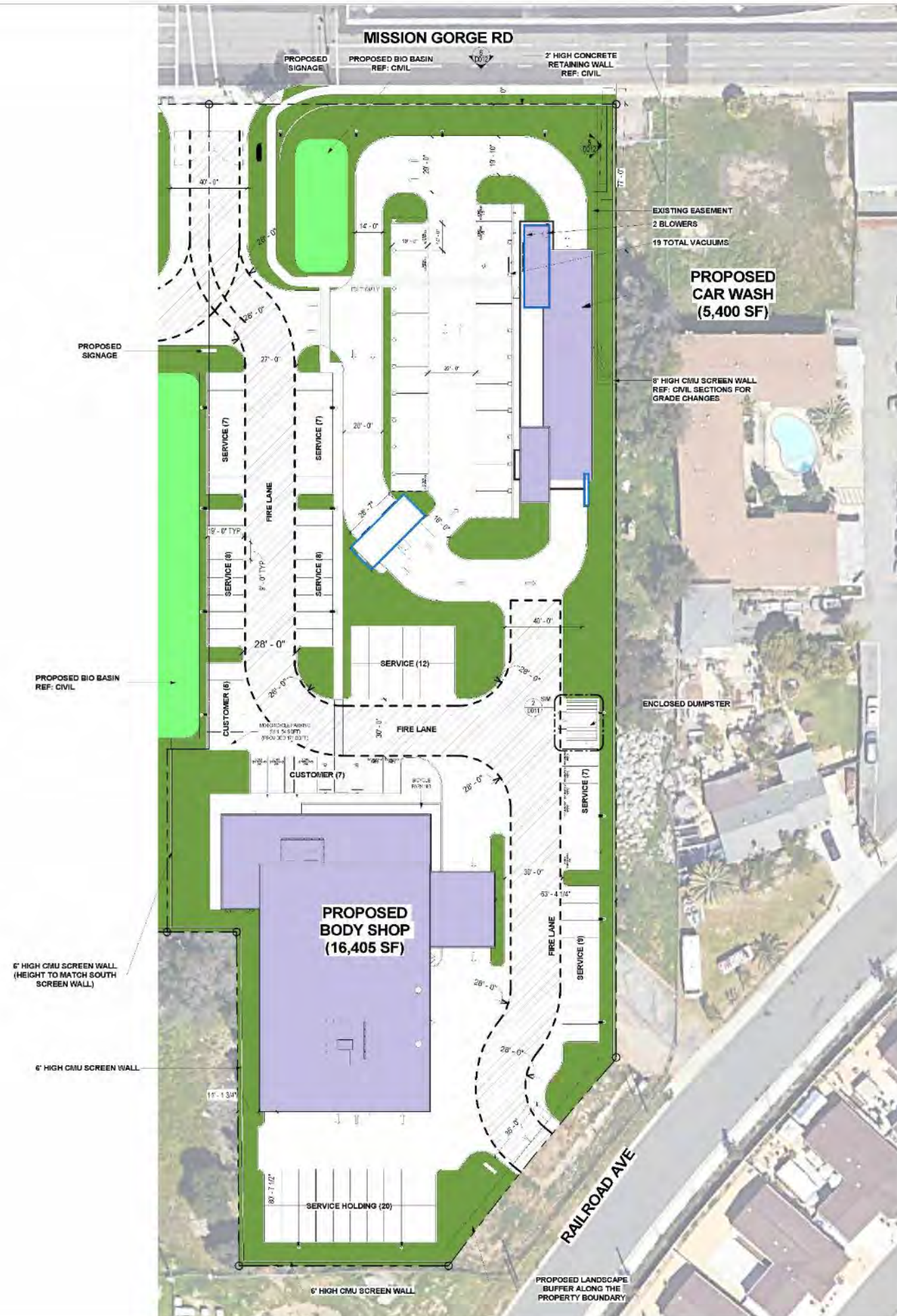
11750 Sorrento Valley Rd
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92121 USA

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B022-C8035-004

(B) PROPOSED
BUILDING
SECTION

D205



PARKING COUNT REQUIREMENTS

SANTEE, CALIFORNIA MUNICIPAL CODE
13.24.040 PARKING REQUIREMENTS

USE CLASS	SQ. FT.	REQUIRED	PROVIDED
AUTOMOBILE WASHING AND CLEANING (16 SPACES)	5,400	16 SPACES	22 SPACES
TOTAL PARKING		16 SPACES	22 SPACES

CGC TABLE 11B-208.2 - REQUIRED ACCESSIBLE SPACES
16 REQUIRED SPACES
1 to 25 = 1 ACCESSIBLE SPACES REQUIRED
ADA PARKING PROVIDED: 1 SPACE (1 VAN ACCESSIBLE)

CGC TABLE 5.105.6.2 - REQUIRED CLEAN AIR SPACES
16 REQUIRED SPACES
10 to 25 = 3 CLEAN AIR SPACES REQUIRED
CLEAN AIR VEHICLE PARKING PROVIDED: 3 SPOTS

CGC TABLE 5.105.6.3.3 - REQUIRED EV SPACES
16 REQUIRED SPACES
10 to 25 = 2 EV SPACES REQUIRED
EV PARKING PROVIDED: 2 SPOTS (1 VAN ACCESSIBLE)

CGC 5.106.4.1 - REQUIRED BICYCLE PARKING
SHORT TERM: 25 x (0.5%) = 1.25 BIKE SPOTS
ONE TWO CAPACITY BIKE RACK REQUIRED
LONG TERM: NOT REQUIRED

*REMAINDER PARKING STALLS ARE DISPLAY/INVENTORY

PARKING COUNT REQUIREMENTS

SANTEE, CALIFORNIA MUNICIPAL CODE
13.24.040 PARKING REQUIREMENTS

USE CLASS	SQ. FT.	REQUIRED	PROVIDED
MOTOR VEHICLE SALES, AUTOMOTIVE REPAIR, PAINTING, BODY WORK OR SERVICE (1 PER 400 SF)	16,405	41 SPACES	90 SPACES
TOTAL PARKING		41 SPACES	90 SPACES

CGC TABLE 11B-208.2 - REQUIRED ACCESSIBLE SPACES
41 REQUIRED SPACES
25 to 50 = 2 ACCESSIBLE SPACES REQUIRED
ADA PARKING PROVIDED: 2 SPACES (1 VAN ACCESSIBLE)

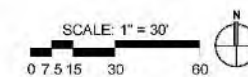
CGC TABLE 5.105.6.2 - REQUIRED CLEAN AIR SPACES
41 REQUIRED SPACES
25 to 50 = 6 CLEAN AIR SPACES REQUIRED
CLEAN AIR VEHICLE PARKING PROVIDED: 6 SPOTS

CGC TABLE 5.105.6.3.3 - REQUIRED EV SPACES
41 REQUIRED SPACES
25 to 50 = 4 EV SPACES REQUIRED
EV PARKING PROVIDED: 4 SPOTS (1 VAN ACCESSIBLE)

CGC 5.106.4.1 - REQUIRED BICYCLE PARKING
SHORT TERM: 25 x (0.5%) = 1.25 BIKE SPOTS
ONE TWO CAPACITY BIKE RACK REQUIRED
LONG TERM: NOT REQUIRED

*REMAINDER PARKING STALLS ARE DISPLAY/INVENTORY

1 **PARCEL C - VICINITY SITE PLAN**
1" = 30'-0"



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NOT FOR CONSTRUCTION**



Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CUP Submittal
09/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal

HED

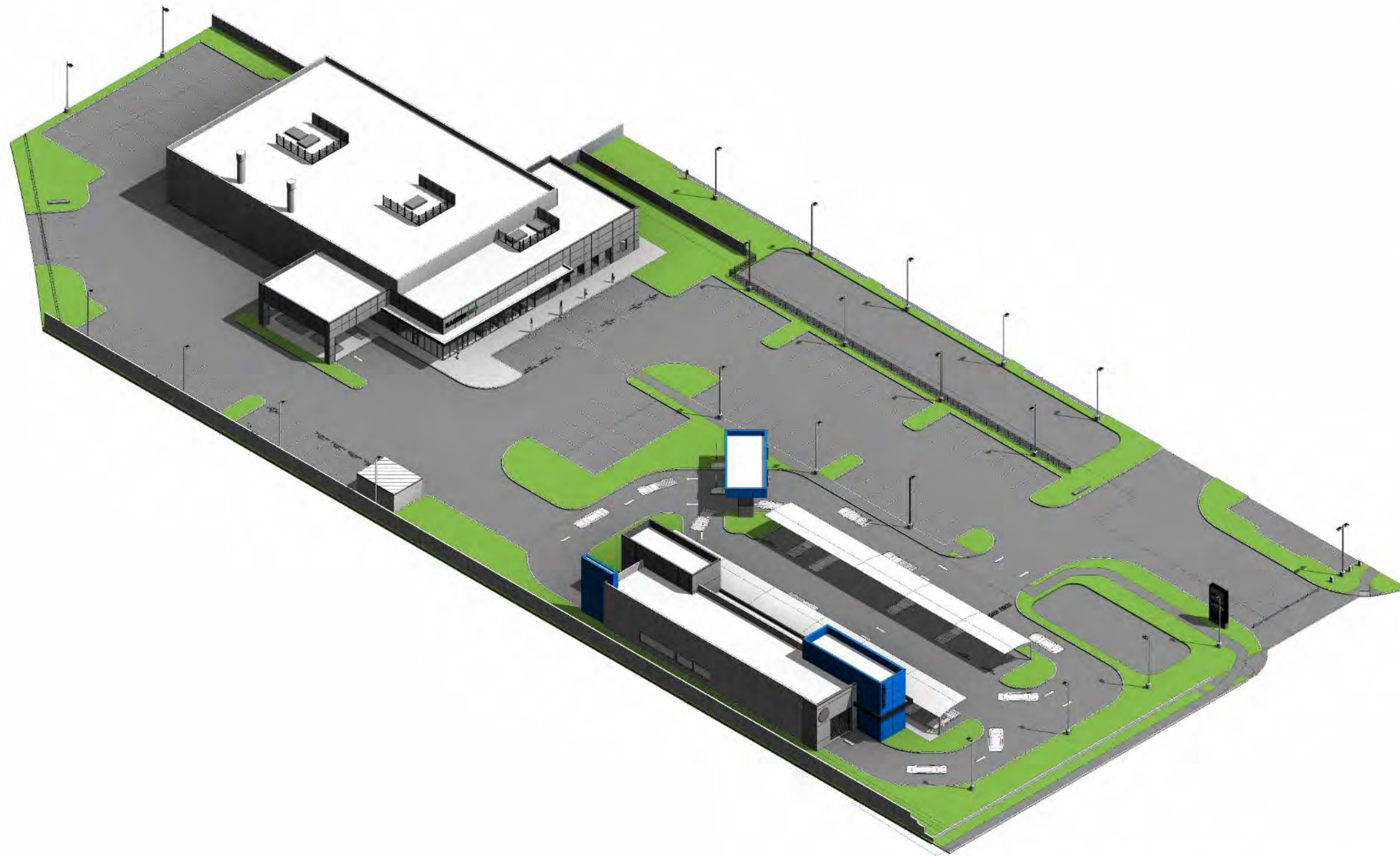
11750 Sorento Valley Rd
Suite 100
San Diego, California
92121 USA

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WWW.HED.DESIGN

02/22
BD22-CB036-004

(C) VICINITY SITE
PLAN

D300



CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date	Issued For
12/16/2022	CUP Submittal
06/23/2023	CUP Re-submittal
08/11/2023	CUP Re-submittal

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(C) ISOMETRIC
VIEW

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D301



Cameron Brothers Company LLC

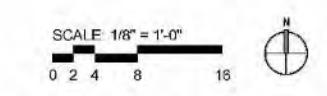
Santee Auto Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CUP Submittal
06/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal



PROGRAMMING		
PROGRAM TYPE	REQUIRED UNIT / AREA	PROPOSED UNIT / AREA
CUSTOMER FOCUS		
RECEPTION & WRITE-UP	TBD	1,413 SF
		1,413 SF
PARTS DEPARTMENT		
PARTS STORAGE	TBD	1,071 SF
		1,071 SF
BODY SHOP		
BODY SHOP	TBD	15 STALLS / 9,873 SF
PAINT BOOTHS	TBD	2 BOOTHS / 1,089 SF
RECEPTION CANOPY	TBD	4 CARS / 1,360 SF
		12,322 SF
ADMIN. OFFICE		
GENERAL OFFICE	TBD	1,414 SF
		1,414 SF
SUPPORT		
BUILDING SUPPORT	TBD	185 SF
		185 SF
BUILDING TOTAL:	TBD	16,405 SF



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



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Suite 100
San Diego, California
92121 USA
(858) 396-3800
WWW.HEDDESIGN

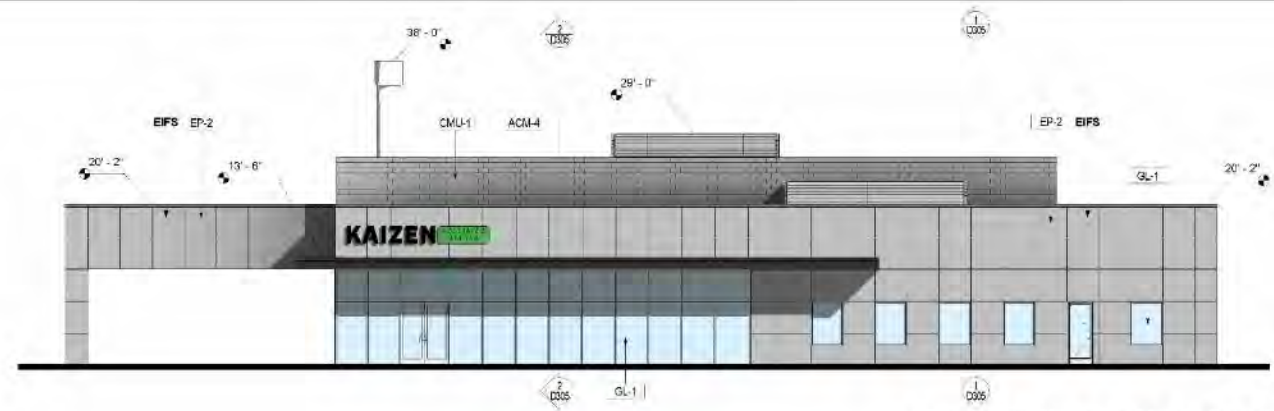
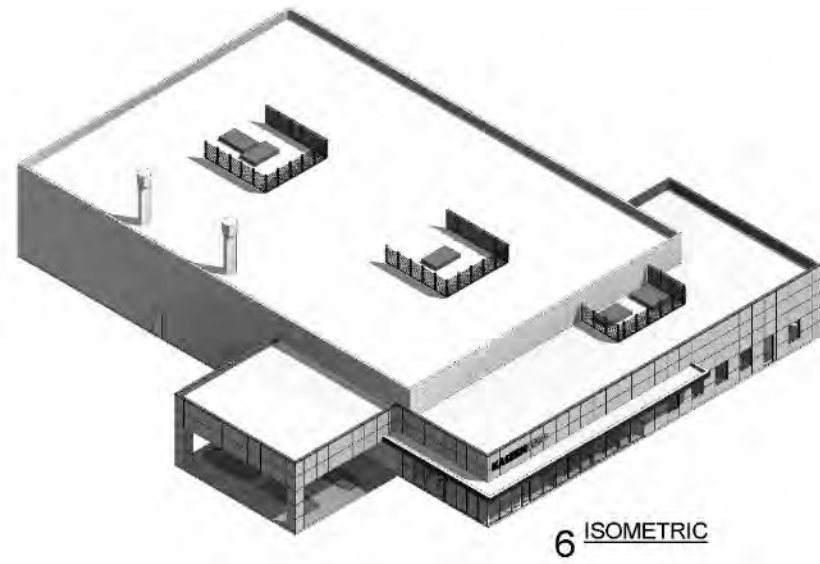
©2022
B022-09005-004

(C) PROPOSED
FLOOR PLAN -
COLLISION

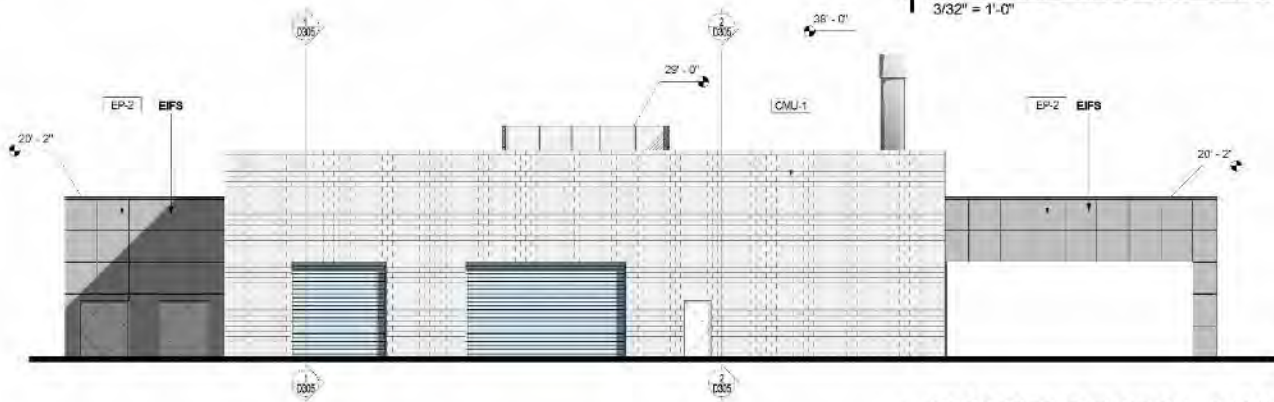
D302

EXTERIOR FINISHES

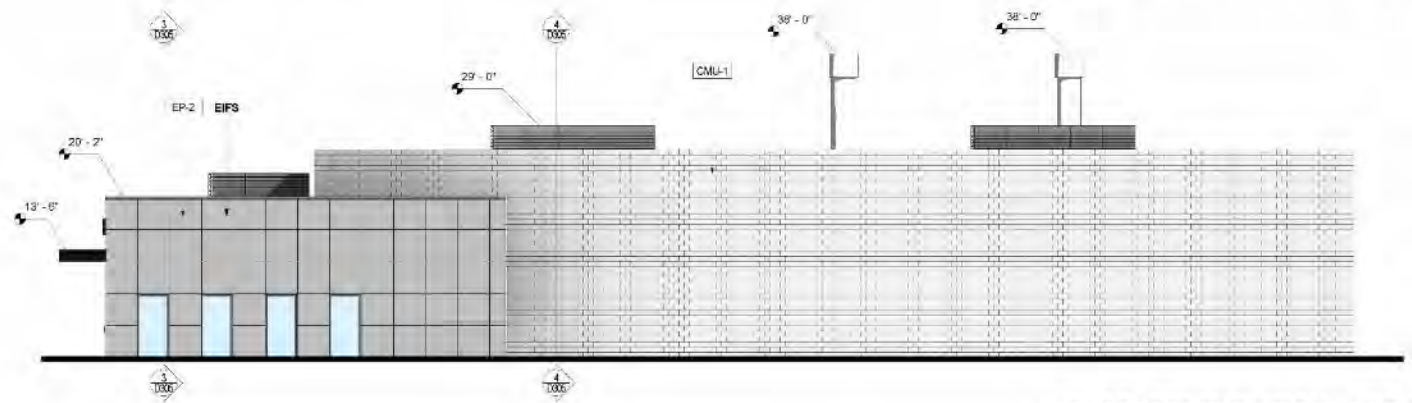
NO.	LOCATION	COLOR & MATERIAL SPECIFICATIONS	DESCRIPTIONS	REFERENCE IMAGE
ACM-1	EXTERIOR AT MAIN SHOWROOM & STRUCTURE & SERVICE DRIVE FACADE	ALPOLIC 158 BRIGHT ALUMINUM METALLIC	ALUMINUM COMPOSITE MATERIAL	
ACM-2	CAR WASH TOWER ELEMENTS	ALPOLIC CMU BLUE COLOR TO MATCH PWS 900 CHEVROLET BLUE	ALUMINUM COMPOSITE MATERIAL	
ACM-4	EXTERIOR AT MAIN SHOWROOM & STRUCTURE & SERVICE DRIVE FACADE	ALPOLIC 888 SLATE BLACK	ALUMINUM COMPOSITE MATERIAL	
EP-1	SERVICE BUILDING STRUCTURE AND ANY OTHER BUILDING EXTERIOR EXCEPT MAIN SHOWROOM	BEHANNI MOORE DC-61 WHITE CHALKED	EXTERIOR GRADE PAINT	
EP-2	SERVICE BUILDING STRUCTURE AND ANY OTHER BUILDING EXTERIOR EXCEPT MAIN SHOWROOM	BEHANNI MOORE 212-49 SILVER HALF DOLLAR	EXTERIOR GRADE PAINT	
CMU-1	SERVICE BUILDING STRUCTURE AND ANY OTHER BUILDING EXTERIOR EXCEPT MAIN SHOWROOM	INTEGRAL COLOR TO MATCH EP-1 (IF INTEGRAL COLOR CANNOT MATCH EP-1, CMU WALLS IS TO BE PAINTED EP-1)	BURNISHED OR FLAT FACED CONCRETE MASONRY UNIT	
GL-1	GLAZING, GLASS DOORS	CLEAR GLASS WITH ANODIZED ALUMINUM APPEARANCE FRAMING	GLAZING	



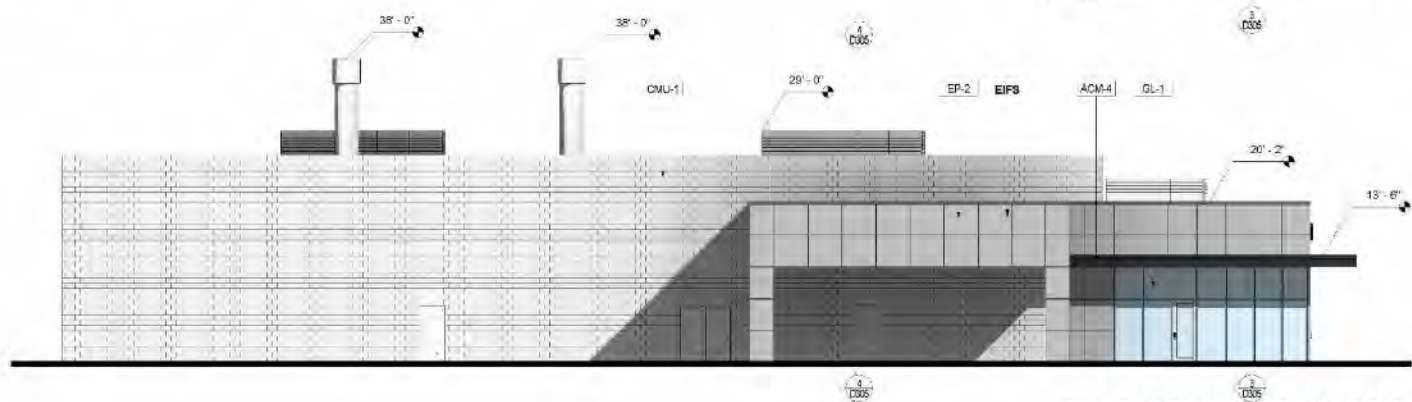
1 PROPOSED ELEVATION - FRONT
3/32" = 1'-0"



2 PROPOSED ELEVATION - BACK
3/32" = 1'-0"



3 PROPOSED ELEVATION - SIDE 1
3/32" = 1'-0"



4 PROPOSED ELEVATION - SIDE 2
3/32" = 1'-0"

**FOR REFERENCE ONLY
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Cameron Brothers Company LLC

Santee Auto Center

10335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CLIP Submittal
06/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal

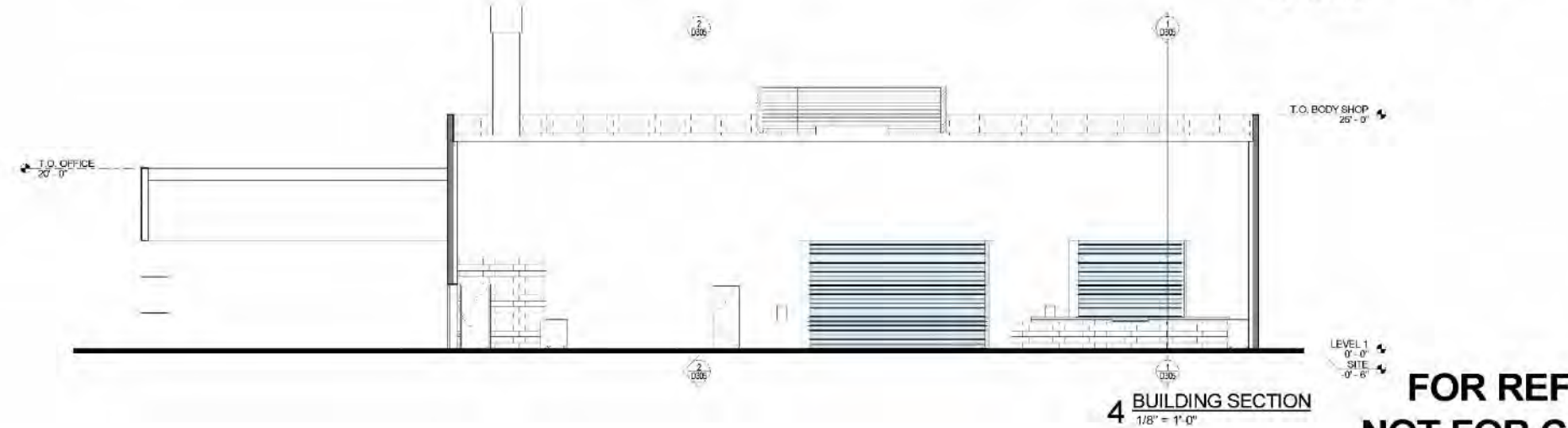
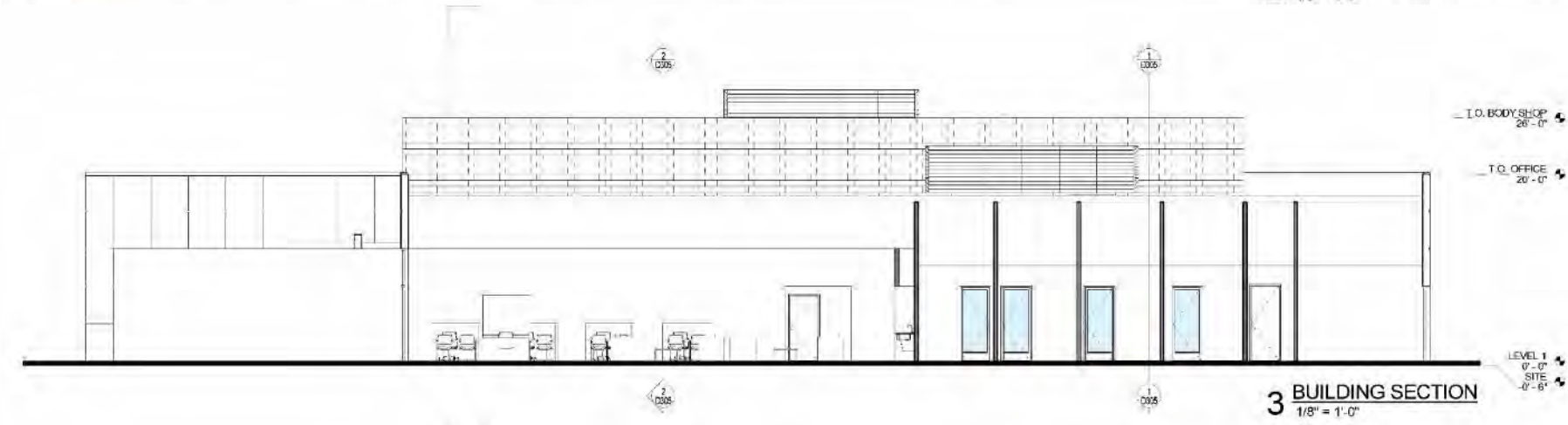
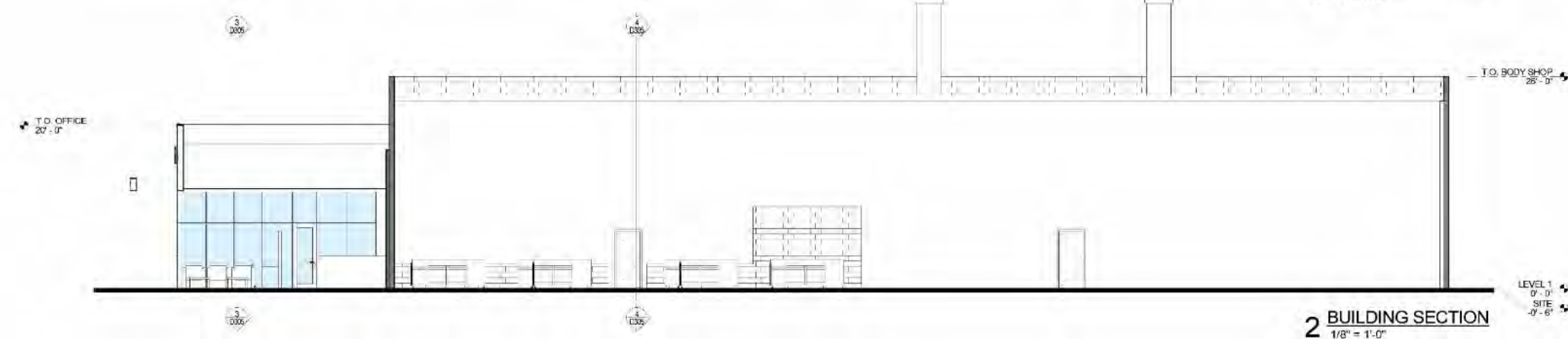
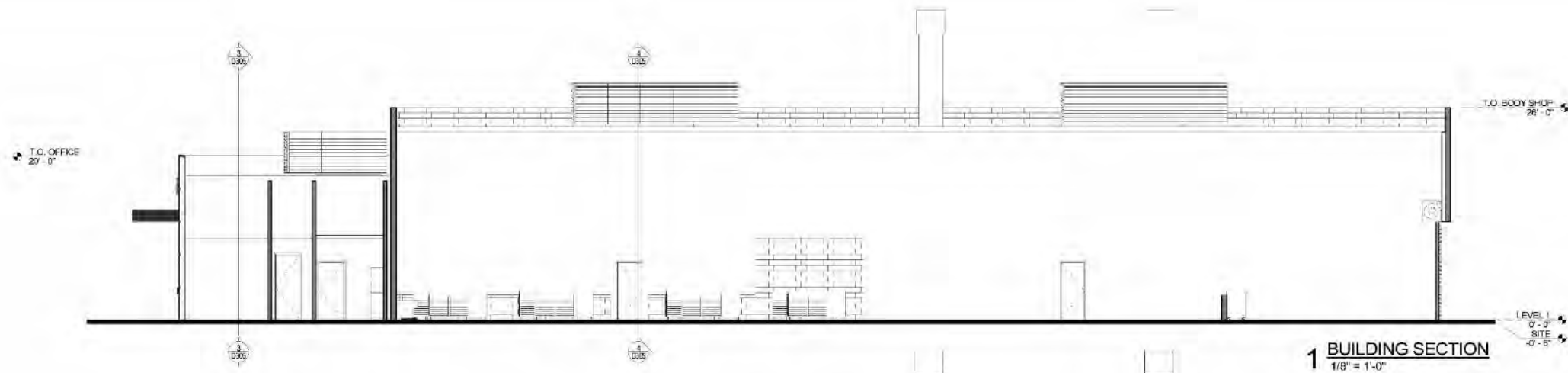
HED

11750 Somers Valley Rd
Suite 100
San Diego, California
92121 USA
(958) 398-3800
WWW.HED.DESIGN

BD22-CB036-004

(C) BUILDING ELEVATIONS - COLLISION

D304



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NOT FOR CONSTRUCTION**



Cameron Brothers
Company LLC

Santee Auto
Center

10335 Mission Gorge Road
Santee, CA 92071

Date	Issued For
12/16/2022	CUP Submittal
06/23/2023	CUP Resubmittal
08/11/2023	CUP Resubmittal



11750 Sorrento Valley Rd
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(C) BUILDING
SECTIONS -
COLLISION

D305



CAMERON
BROTHERS COMPANY LLC

Cameron Brothers
Company LLC

13335 Mission Gorge Road
Santee, CA 92071

Date Issued For
12/16/2022 CUP Submittal
08/23/2023 CUP Resubmittal
08/11/2023 CUP Resubmittal

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BD22-CB036-004

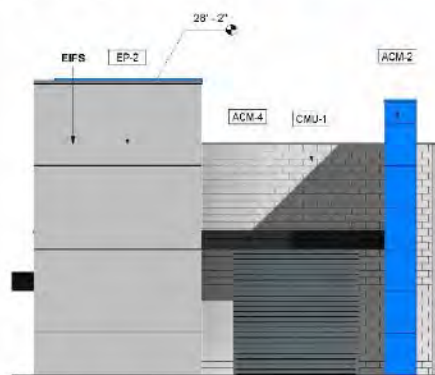
(C) PROPOSED
CAR WASH
FACILITY

D306

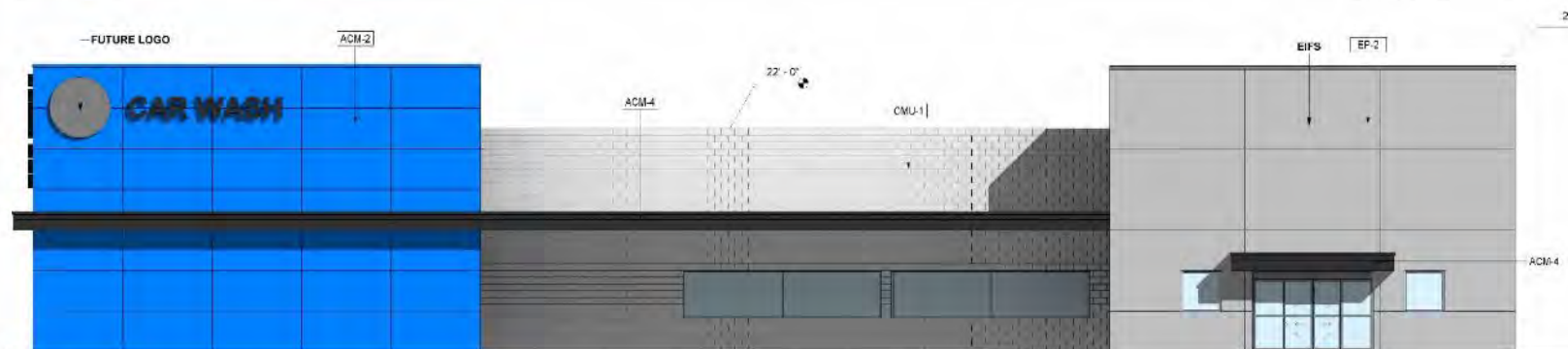
BUILDING SQ. FT	
USE CLASS	SQ. FT.
CAR WASH	5,400
TOTAL	5,400



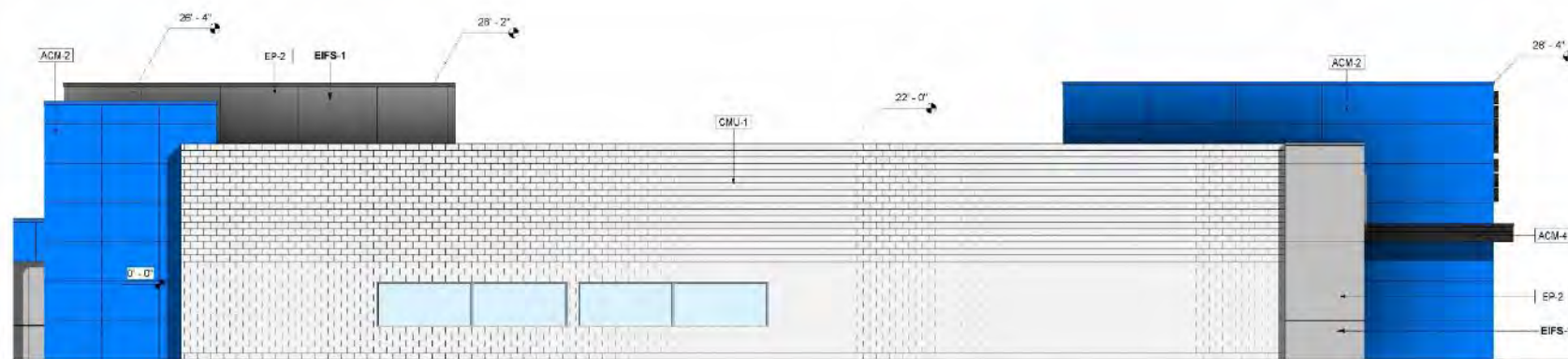
1 PROPOSED ELEVATION - FRONT
1/8" = 1'-0"



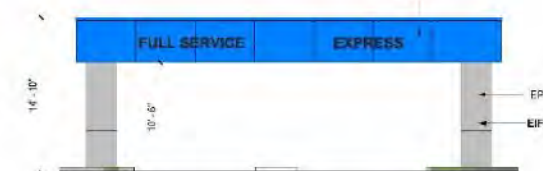
2 PROPOSED ELEVATION - BACK
1/8" = 1'-0"



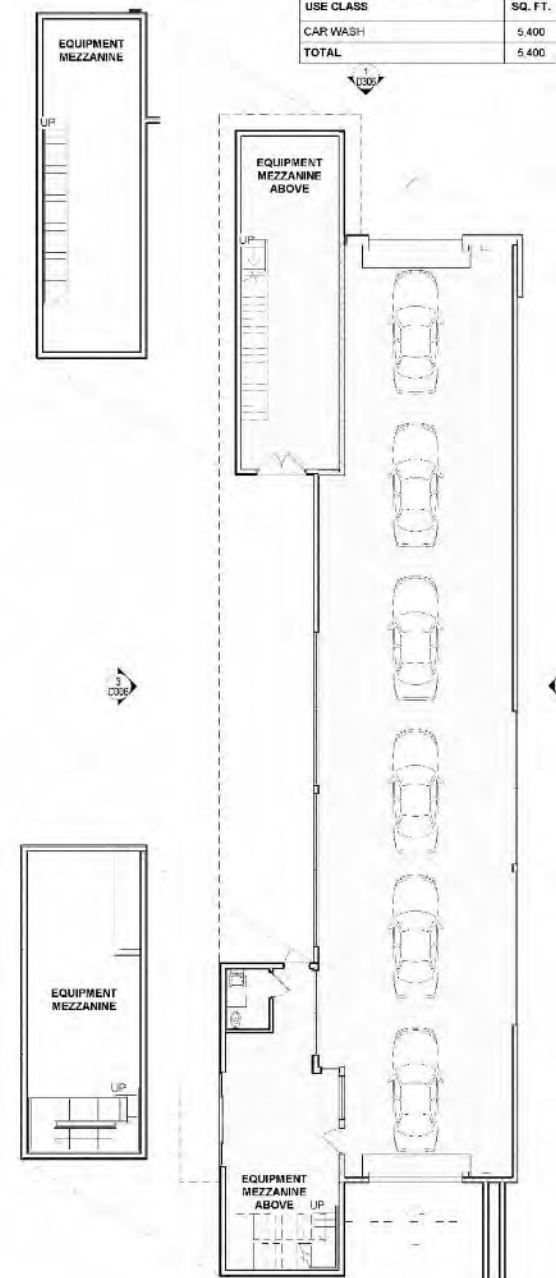
3 PROPOSED ELEVATION - SIDE 1
1/8" = 1'-0"



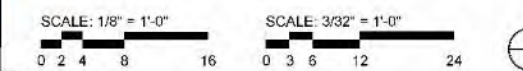
4 PROPOSED ELEVATION - SIDE 2
1/8" = 1'-0"



5 PROPOSED ELEVATION - KIOSK CANOPY
1/8" = 1'-0"



6 PARCEL C - PROPOSED CAR WASH FLOOR PLANS
3/32" = 1'-0"



**FOR REFERENCE ONLY
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RESOLUTION NO. _____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SANTEE,
CALIFORNIA, APPROVING AND ADOPTING A MITIGATED NEGATIVE
DECLARATION (AEIS2022-11) AND A MITIGATION MONITORING AND
REPORTING PROGRAM FOR AN AUTO CENTER LOCATED ON A 13.1-ACRE
SITE AT 10335 MISSION GORGE ROAD, IN THE GENERAL COMMERCIAL
(GC) ZONE.**

**APPLICANT: KEVIN PERRY, (HED DESIGN)
APN: 384-091-01, -13, -14
(RELATED CASE FILES: CONDITIONAL USE PERMIT P2022-10)**

WHEREAS, on August 14, 2023, Kevin Perry from HED Design submitted a complete application for a Conditional Use Permit P2022-10 to develop an auto center on a 13.1-acre site located at 10335 Mission Gorge Road, in the City of Santee, County of San Diego, State of California (“Project”); and

WHEREAS, based on the information contained in the Initial Study, which concluded that the Project would not have significant impacts on the environment with mitigation incorporated, the City determined that a Mitigated Negative Declaration (MND) should be prepared for the Project, and a Draft Initial Study/MND, State Clearinghouse No. 2023080274 was prepared in accordance with CEQA, the State CEQA Guidelines, and the City’s Local CEQA Guidelines; and

WHEREAS, the proposed Project is located within Airport Influence Area 1 of the Gillespie Field Airport Land Use Compatibility Plan (ALUCP). On January 18, 2023, the Federal Aviation Administration determined that the Project, as designed, presented no hazard to air navigation, and the Airport Land Use Commission (ALUC) determined that the Project as conditionally consistent with the ALUCP pursuant to the ALUC determination letter dated August 10, 2023; and

WHEREAS, in compliance with Public Resources Code section 21080.3.1, the City solicited Tribal input on the Project on July 7, 2023, and a request for Tribal consultation was made by the Barona Band of Mission Indians (Barona); and

WHEREAS, as part of this Tribal consultation request, Barona was provided with cultural resource surveys/reports and more information on the proposed Tribal monitoring program for the Project; and

WHEREAS, on July 14, 2023, Barona confirmed the conclusion of the requested Tribal consultation indicating that the proposed Tribal monitoring for the Project was satisfactory; and

WHEREAS, the Planning & Building Department scheduled Conditional Use Permit (P2022-10) and Mitigated Negative Declaration (AEIS2022-11) for a public hearing on October 25, 2023; and

RESOLUTION NO. _____

WHEREAS, pursuant to State CEQA Guidelines section 15072, on August 11, 2023, the Notice of Intent to Adopt the MND was posted by the Clerk for the County of San Diego and published in the East County Californian; and

WHEREAS, pursuant to State CEQA Guidelines section 15073, the Draft MND was circulated for a 30-day public review period from August 11, 2023 through September 11, 2023; and

WHEREAS, the Notice of Intent to Adopt the Draft MND was also submitted to the State Clearinghouse for state agency review and, as required by State CEQA Guidelines section 15073, the state agency review period began on August 11, 2023 and closed on September 11, 2023; and

WHEREAS, during the public comment period, copies of the Draft MND, including any technical appendices, were available for review and inspection at City Hall and on the City's website; and

WHEREAS, one comment letter was received during the public review period, which did not raise any new environmental issues; and

WHEREAS, staff has reviewed all comments and prepared responses to each comment as reflected in the Final MND; and

WHEREAS, the Final MND consists of the Draft MND, comments and responses on the Draft MND, and the Mitigation Monitoring and Reporting Program ("MMRP"); and

WHEREAS, the MND and the MMRP are attached hereto as "Exhibit A" and "Exhibit B" respectively; and

WHEREAS, on October 25, 2023, the City Council held a duly advertised public hearing on P2022-10 and AEIS2022-11; and

WHEREAS, as contained herein, the City Council has endeavored in good faith to set forth the basis for its decision on the Project; and

WHEREAS, all the requirements of the Public Resources Code and the State CEQA Guidelines have been satisfied by the City in connection with the preparation of the MND, which is sufficiently detailed so that all of the potentially significant environmental effects of the Project, as well as feasible mitigation measures, have been adequately evaluated; and

WHEREAS, all of the findings and conclusions made by the City Council pursuant to this Resolution are based upon the oral and written evidence presented to it as a whole and the entirety of the administrative record for the Project, which are incorporated herein by this reference, and not based solely on the information provided in this Resolution; and

RESOLUTION NO. _____

WHEREAS, prior to taking action, the City Council had heard, been presented with, reviewed and considered all of the information and data in the administrative record, including but not limited to the Initial Study, MND, and MMRP; and

WHEREAS, the MND reflects the independent judgment of the City Council and is deemed adequate for purposes of making decisions on the merits of the Project; and

WHEREAS, no comments submitted during the public review period, or made during the public hearing conducted by the City Council, and no additional information submitted to the City require substantial revisions to the MND necessitating recirculation or additional environmental review of the Project under State CEQA Guidelines section 15073.5; and

WHEREAS, all other legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE BE IT RESOLVED by the City Council of the City of Santee, California, as follows:

SECTION 1. RECITALS. The City Council hereby finds that the foregoing recitals are true and correct and are incorporated herein as substantive findings of this Resolution.

SECTION 2. COMPLIANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT. As the decision-making body for the Project, the City Council has reviewed and considered the information contained in the MND, comments received, other documents contained in the administrative record, and all other written and oral evidence presented to the City Council for the Project (collectively, the "Record"). The City Council further finds that the MND and the MMRP have been completed in compliance with CEQA and the State CEQA Guidelines.

SECTION 3. FINDINGS ON ENVIRONMENTAL IMPACTS. Based on the whole record before it, the City Council finds that the Project would have potentially significant impacts but that those impacts can be mitigated to less than significant through mitigation measures outlined in the MND and the MMRP. The City Council finds that the MND contains a complete, objective, and accurate reporting of the environmental impacts associated with the Project and reflects the independent judgment and analysis of the City Council.

No new significant environmental effects have been identified in the Final MND and no changes to the Final MND constitute substantial revisions requiring recirculation under State CEQA Guidelines section 15073.5. All of the mitigation measures contained in the MMRP have been made conditions of Project approval in accordance with State CEQA Guidelines 15074 (d).

SECTION 4. WILDLIFE RESOURCES. Pursuant to Fish and Game Code Section 711.4(c), all Project applicants and public agencies subject to the California Environmental Quality Act shall pay a filing fee for each proposed Project, as specified in subdivision 711.4(d) for any adverse effect on wildlife resources or the habitat upon

RESOLUTION NO. _____

which wildlife depends unless a “no effect” finding is made by the California Department of Fish and Wildlife. This fee is due and payable as a condition precedent to the County Clerk’s filing of a Notice of Determination. The City of Santee hereby notifies the Applicant that in order to comply with State Law, the Applicant shall remit to the City of Santee Department of Development Services, within two (2) working days of the effective date (as defined in Section 8 below) of this approval, a certified check payable to the "County Clerk, County of San Diego" in the amount of \$2,814.00. This fee includes an authorized County administrative fee of \$50. Failure to remit the required fee in full within the time specified above will result in notification to the State that a fee was required but not paid, and could result in State imposed penalties and recovery under the provisions of the Revenue and Taxation Code. In addition, Section 21089 (b) of the Public Resources Code, and Section 711.4 (c) of the Fish and Game Code, provide that no Project shall be operative, vested, or final until the required filing fee is paid.

SECTION 5. ADOPTION OF THE MND. The Final MND, attached hereto as “Exhibit A”, is hereby approved and adopted.

SECTION 6. ADOPTION OF THE MMRP. The MMRP prepared for the Project, attached hereto as “Exhibit B”, is hereby approved and adopted.

SECTION 7. NOTICE OF DETERMINATION. Staff is directed to file a Notice of Determination with the San Diego County Clerk within five (5) working days of approval of the Project and adoption of the Final MND.

SECTION 8. LOCATION AND CUSTODIAN OF RECORDS. The documents and materials associated with the Project and the MND that constitute the record of proceedings on which these findings are based are located at Santee City Hall, 10601 Magnolia Avenue, Building #3, Santee CA 92071. The City Clerk is the custodian of the record of proceedings.

SECTION 9. EFFECTIVE DATE. This Resolution shall become effective immediately upon its adoption.

ADOPTED by the City Council of Santee, California, at a Regular meeting held this 25th day of October 2023, by the following vote:

AYES:

NOES:

ABSENT:

APPROVED:

JOHN W. MINTO, MAYOR

RESOLUTION NO. _____

ATTEST:

ANNETTE ORTIZ, CMC, CITY CLERK

Attachment: Exhibit A - MND
Exhibit B - MMRP

EXHIBIT A



Final Initial Study/Environmental Checklist
Form
for the Santee Auto Center Project
Santee, California

Prepared for
City of Santee
10601 Magnolia Avenue
Santee, CA 92071

Prepared by
RECON Environmental, Inc.
3111 Camino del Rio North, Suite 600
San Diego, CA 92108
P 619.308.9333

RECON Number 9999
October 17~~August 11~~, 2023

DRAFT MITIGATED NEGATIVE DECLARATION

1. Name or description of project:	Applications for a Conditional Use Permit (P2022-10) and Environmental Initial Study (AEIS2022-11) for the construction and operation of an auto center on a 13.1-acre site in the General Commercial Zone. The subject site would be graded into three parcels (Parcel A, B, & C) and developed with new buildings, parking, landscaping, and storm water facilities. Parcel A would include a new auto dealership (33,974 sf) with an accessory auto detail building (2,549 sf). Parcel B would include a new auto dealership (33,112 sf). Parcel C would include a new auto body shop (16,405 sf) with an accessory car wash building (5,400 sf). The project would include six driveways into the site, two on Cottonwood Avenue, three on Mission Gorge Road and one on Railroad Avenue. Proposed public improvements include sidewalks and fully landscaped parkways adjoining the site on Mission Gorge Road and Cottonwood Avenue.
2. Project Location – Identify street address and cross streets or attach a map showing project site (preferably a USGS 15’ or 7 1/2’ topographical map identified by quadrangle name):	Address: 10335 Mission Gorge Road Assessor’s Parcel Numbers: 384-091-01, 384-091-13 and 384-091-14. Nearest cross streets: Cottonwood Avenue and Railroad Avenue. USGS map attached
3. Entity or Person undertaking project:	
A. Entity	
(1) Name:	A.James Moxham, The SchoolYard LLC
(2) Address:	10580 Prospect Avenue, Ste. 200, Santee, CA 92071
B. Other (Private)	
(1) Name:	Kevin Perry, HED Design
(2) Address:	11750 Sorrento Valley Road Ste. 100, San Diego, CA 92121
<p>The Lead Agency, having reviewed the Initial Study of this proposed project, having reviewed the written comments received, and having reviewed the recommendation of the Lead Agency’s Staff, does hereby find and declare that the proposed project will not have a significant effect on the environment. A brief statement of the reasons supporting the Lead Agency’s findings are as follows:</p> <p>The project is compatible with the Santee General Plan in that the proposed commercial project is located within the General Commercial zone. The proposed project meets the development standards including lot size, lot dimension, and setbacks.</p> <p>The Project site is physically suitable and has adequate infrastructure, including roads, water, sewerage, and electricity, to support the type of development. The Project would be developed in accordance with the Sustainable Santee Plan and will not contribute significantly to greenhouse gas emissions, nor frustrate the intent of state policy relative to greenhouse gas emissions.</p> <p>All potentially significant environmental impacts can be mitigated to less than significant levels through implementation of the mitigation measures identified in the Initial Study. Therefore, the Project would not result in significant impacts to the environment.</p>	
<p>The Lead Agency hereby finds that the Mitigated Negative Declaration reflects its independent judgment. A copy of the Initial Study is attached.</p>	

The location and custodian of the documents and any other material which constitute the record of proceedings upon which the Lead Agency based its decision to adopt this Mitigated Negative Declaration are as follows:

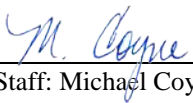
Michael Coyne, Principal Planner
City of Santee
10601 Magnolia Ave.
Santee, CA 92071

This document can also be viewed at:

<https://www.cityofsantee.ca.gov/services/project-environmental-review>

Phone No.: (619) 258-4100 x160

Date Received
for Filing: _____


Staff: Michael Coyne

(Clerk Stamp Here)

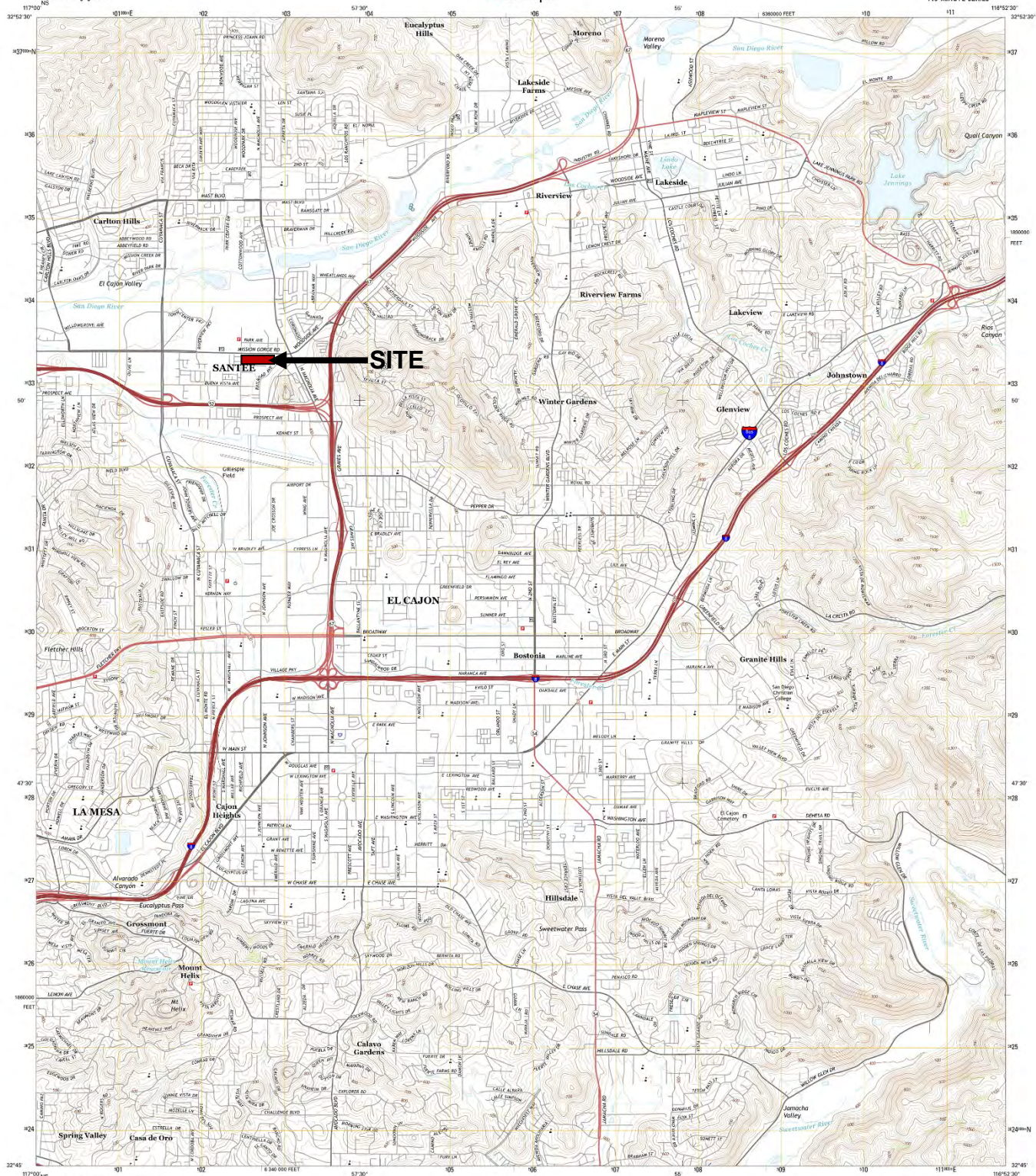
Principal Planner
Title



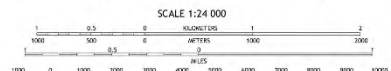
U.S. DEPARTMENT OF THE INTERIOR
U.S. GEOLOGICAL SURVEY



EL CAJON QUADRANGLE
CALIFORNIA-SAN DIEGO CO.
7.5-MINUTE SERIES



Produced by the United States Geological Survey
North American Datum of 1983 (NAD83)
North American Datum of 1983 (NAD83) Projection and
1000 meter grid: Universal Transverse Mercator, Zone 11S
10 000 foot (3048) California Coordinate System of 1985 (CCS85)



CONTOUR INTERVAL, 20 FEET
NORTH AMERICAN VERTICAL DATUM OF 1988
This map was produced in conformance with the
National Geospatial Program of US Topographic, November, 2011.
A metadata file associated with this product is draft version 0.6.18

EL CAJON, CA
2015

LETTERS OF COMMENT AND RESPONSES

The Draft Mitigated Negative Declaration for the Santee Auto Center Project was circulated for public and agency review from August 11, 2023, to September 11, 2023 (State Clearinghouse No. 2023080274). During the 30-day public and agency review period, two comment letters were received from the San Diego County Archaeological Society. The letters can be found on the following pages, with responses to comments provided adjacent to the individual comments in the letter. No substantial revisions to the impact analysis were required as a result of the comments received on the Draft Mitigated Negative Declaration.

Letter	Author	Page Number
A	San Diego County Archaeological Society dated August 21, 2023	RTC-2
B	San Diego County Archaeological Society dated September 10, 2023	RTC-4

Letter A


San Diego County Archaeological Society, Inc.

Environmental Review Committee

27 August 2023

RECEIVED

AUG 31 2023

 Dept. of Development Services
 City of Santee

To: Mr. Michael Coyne, Principal Planner
 Planning & Building Department
 City of Santee
 10601 Magnolia Avenue
 Santee, California 92071

Subject: Draft Mitigated Negative Declaration
 Santee Auto Center

Dear Mr. Coyne:

A-1 I have reviewed the subject DMND on behalf of this committee of the San Diego County Archaeological Society.

Based on the information contained in the DMND and initial study, we have the following comments:

A-2 1. Appendices C, D and E, all cultural resources appendices, have been omitted from the documents posted on the City of Santee's website, thereby significantly limiting the public's ability to judge the adequacy of the studies done to date and the proposed mitigation measures. Normal practice in the region is for the cultural resources consultants to produce two versions of their reports, one with the sensitive/restricted information removed and one containing all the confidential information. The first of these is provided for public review while the latter is retained by the jurisdiction's staff and qualified others. Please obtain and make available to SDCAS the unrestricted version of Appendices C, D and E, so that we may complete our review prior to the close of the public review period on September 11th. You may use my personal email address, jwroyle@att.net, to contact me regarding them.

A-3 2. Given the previous development on the project area, the mitigation measures need to address non-Tribal as well as Tribal resources that may be encountered during monitoring of ground-disturbing activities. This extends through to and including curation.

A-4 Thank you for including SDCAS in the notification of this DMND's availability.

Sincerely,

James W. Royle, Jr., Chairperson
 Environmental Review Committee

P.O. Box 81106 San Diego, CA 92138-1106 (858) 538-0935

A-1 The commenter provides introductory language regarding the content of this comment letter. This comment does not raise an issue related to the adequacy of the analysis contained within the Draft IS/MND.

A-2 Upon receipt of this letter, RECON, the environmental consultant for preparation of the MND sent Appendices C, D, and E to James Royle via email on September 5, 2023. The City of Santee (City) also posted the appendices without confidential information for public review on the City's website at the link below:

<https://www.cityofsanteeca.gov/government/planning-and-building/active-projects-and-environmental-documents-for-public-review>.

A-3 The Draft IS/MND includes mitigation recommended in the Cultural Resources Technical Memorandum prepared by Rincon, now dated August 1, 2023. The mitigation covers both tribal and non-tribal cultural resources.

A-4 The commenter provides conclusive language regarding the content of this comment letter. This comment does not raise an issue related to the adequacy of the analysis contained within the Draft IS/MND.

LETTER

RESPONSE

cc: RECON
Rincon Consultants
SDCAS President
File

P.O. Box 81106 • San Diego, CA 92138-1106 • (619) 538-0935

Letter B



San Diego County Archaeological Society, Inc.

Environmental Review Committee

10 September 2023

RECEIVED

SEP 13 2023

Dept. of Development Services
City of Santee

To: Mr. Michael Coyne, Principal Planner
Planning & Building Department
City of Santee
10601 Magnolia Avenue
Santee, California 92071

Subject: Draft Mitigated Negative Declaration
Santee Auto Center

Dear Mr. Coyne:

- B-1 Thank you, RECON and Rincon Consultants for providing the three cultural resources appendices for the Santee Auto Center Project. Based on those documents, we have the following comments:
- B-2 (1) Regarding the grading that took place between Rincon’s “due diligence” work and the more recent work which it recommended, was that grading properly permitted? Was it done by the previous or present owners? We hope that the “due diligence” work didn’t provide an incentive to destroy the resources, and the City didn’t allow it to happen. The County’s Department of Planning and Development Services has a process it follows in cases where non-permitted development actions damage cultural resources. If applicable and appropriate, the City of Santee could take similar action.
- B-3 (2) Page 6 of the March 28, 2023, Rincon letter report (Appendix E) includes, under Mitigation Measure, the “Preparation of a Cultural Resources Mitigation and Monitoring Program”. The mitigation is the *implementation* of Cultural Resources Mitigation and Monitoring, not the program’s preparation. The details of the monitoring program need to be defined and disclosed to the public. This applies to both historic and Tribal cultural resources, and extends through to the treatment of all recovered cultural material from all phases of the work on the project.
- B-4 (3) The Initial Study, on page 27, also includes the preparation of the monitoring program. Since this is the section which would become legally enforceable upon adoption of the MND, the actual program needs to be included therein. We note that the wording found in CUL-1, CUL-2 and CUL-3 has been corrected to use “shall” rather than the Rincon’s “should”, with the exception of a few portions of CUL-3. They should be corrected, too.

P.O. Box 81106 San Diego, CA 92138-1106 (858) 538-0935

B-1 The commenter provides introductory language regarding the content of this comment letter. This comment does not raise an issue related to the adequacy of the analysis contained within the Draft IS/MND.

B-2 It is noted that the County’s Department of Planning and Development Services has a process it follows in cases where non-permitted development actions damage cultural resources. This is not applicable for the project as project site-related grading activities were a result of exploratory work performed by the property owner. This work did not require permit approval, per the City of Santee (City) Grading Ordinance, Chapter 11.40.105.D, Designated Exceptions, provided the work complies with City’s Grading Ordinance.

The work that occurred and the resulting grade is from the backfill and compaction of restoration efforts that were performed under the supervision of the geotechnical engineer as part of exploratory activities. Please refer to the report titled “Supplemental Geotechnical Investigation” report dated March 9, 2022 posted on the City’s website at the following link:

<https://www.cityofsanteeca.gov/government/planning-and-building/active-projects-and-environmental-documents-for-public-review>

In addition, the 2019 Phase II Cultural Resources Testing and Evaluation Report found that RIN-S-1 was not eligible for listing on the National Register of Historic Places and the California Register of Historical Resources. Further, as stated on page 74 of the Draft IS/MND, the City received one response to the Assembly Bill (AB) 52 consultation letters from the Barona Band of Mission Indians (Barona).

LETTER

RESPONSE


	<p>B-2 (cont.)</p> <p>In an email dated July 7, 2023, Barona requested AB 52 consultation for the project, copies of prior cultural resource surveys or reports and more information on the proposed monitoring program. City staff coordinated with Barona as to the preferred format of the reports (i.e., hard copy, electronic, thumb drive). Three reports were sent to Barona electronically in three separate emails on July 12, 2023. On July 13, 2023, Barona confirmed receipt of all the reports and concluded that the mitigation recommended (starting on page 6 of the Rincon report) "seems adequate for the resources that have been identified." Barona further stated: "However, if anything significant is encountered before or during construction, I definitely want to hear about it." On July 14, 2023, Barona confirmed that the AB 52 consultation was concluded "as long as the mitigation measures are included as conditions of approval and are implemented as the project goes forward." The approved mitigation measures are included in the IS/MND under CUL-1 through CUL-5.</p> <p>B-3</p> <p>The monitoring program will be implemented under CUL-3 using guidelines established in the plan developed under CUL-2. CUL-2 as written includes specific performance standards, outlining what will be included in the plan. The exact measures to be included in the Cultural Resources Mitigation and Monitoring Program will be developed by a qualified archeologist meeting or exceeding the Secretary of Interior's Professional Qualification Standards for archaeology. These plans are not generally prepared until a project and Mitigation Monitoring and Reporting Program are fully approved since these documents establish certain project requirements that will be included in the plan itself and the plan can be properly tailored to on-the-ground conditions.</p>
--	--

LETTER

RESPONSE

B-5 Again, we thank you for promptly providing the copies of Appendices C, D and E, so that we could complete this review within the public review period.

Sincerely,


James W. Royle, Jr., Chairperson
Environmental Review Committee

cc: RECON
Rincon Consultants
SDCAS President
File

B-4 As stated above, the monitoring program will be implemented under CUL-3 using guidelines established in the plan developed under CUL-2. CUL-2 as written outlines what will be included in the plan but does not include specifics as these plans are not generally prepared until a project and Mitigation Monitoring and Reporting Program are fully approved since these documents establish certain project requirements that will be included in the plan itself. Wording in mitigation measure CUL-3 was corrected from "should" to "shall" because the word "shall" is used to show certainty of intention about an action that will happen in the future.

B-5 The commenter provides conclusory language regarding the content of this comment letter. This comment does not raise an issue related to the adequacy of the analysis contained within the Draft IS/MND.

**MITIGATION MONITORING AND REPORTING PROGRAM
INCORPORATED INTO THE PROJECT**

Section 21081.6 of the CEQA Guidelines requires that a MMRP be adopted upon certification of an Environmental Impact Report or adoption of a Mitigated Negative Declaration to ensure that the mitigation measures are implemented. The MMRP specifies the mitigation for the project, when in the process it should be accomplished, and the entity responsible for implementing and/or monitoring the mitigation. Public Resources Code Section 21081.6 requires monitoring of only those impacts identified as significant or potentially significant.

The following MMRP table identifies measures that specifically apply to this project. As shown in the table, each mitigation measure is categorized by impact area and corresponding number, with an accompanying identification of:

- The time of the project during which the measure should be implemented;
- The responsible monitoring party; and
- The action indicating compliance with the mitigation measure.

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
Biological Resources				
<p>BIO-1 – Nesting Birds and Raptors: If vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30), a pre-construction clearance survey for nesting birds and raptors shall be conducted. The survey shall cover all suitable nesting habitats that occur within the proposed project site. The pre-construction survey shall be conducted within three days of the start of work.</p> <p>If any active nests are detected, the area will be flagged and mapped along with a buffer as recommended by the qualified biologist. The buffer area(s) established by the qualified biologist will be avoided until the nesting cycle is complete or it is determined that the nest is no longer active. The qualified biologist shall be a person familiar with bird breeding behavior and capable of identifying the bird species of San Diego County by sight and sound and determining alterations of behavior as a result of human interaction. Buffers will be based on species-appropriate buffers and/or local topography and line of sight, species behavior and tolerance to disturbance, and existing disturbance levels, as determined appropriate by the qualified biologist.</p>	<p>Prior to grading or ground disturbance, if vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30).</p>	<p>Project Applicant</p>	<p>Pre-grading survey for nesting birds and raptors</p>	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
Cultural Resources				
CUL-1: Worker Environmental Awareness Program: Prior to the commencement of project-related ground-disturbing activities, including but not limited to site clearing, grubbing, trenching, and excavation, a qualified archaeologist who meets or exceeds the Secretary of the Interior's Professional Qualifications Standards for archaeology shall provide a Worker Environmental Awareness Program for the general contractor, subcontractors, and construction workers participating in ground-disturbing activity for project construction. The Worker Environmental Awareness Program training shall describe the potential of exposing archaeological resources, types of cultural materials that may be encountered, and directions on the steps that shall be taken if such a find is encountered. This training may be presented alongside other environmental training programs required prior to construction. A Worker Environmental Awareness Program acknowledgment form shall be signed by workers who receive the training.	Prior to grading or ground disturbance	Project Applicant	A qualified archaeologist who meets or exceeds the Secretary of the Interior's Professional Qualifications Standards for archaeology shall provide a Worker Environmental Awareness Program and training for the general contractor, subcontractors, and construction workers participating in ground-disturbing activity for project construction.	
CUL-2: Preparation of a Cultural Resources Mitigation and Monitoring Program: Prior to the start of any ground-disturbing activity for project construction, including but not limited to site clearing, grubbing, trenching, and excavation, a qualified archaeologist who meets or exceeds the Secretary of Interior's Professional Qualifications Standards for archaeology shall be retained to prepare a Cultural Resources Mitigation and Monitoring Program for unanticipated discoveries during project construction. The Cultural Resources Mitigation and Monitoring Program shall be prepared in consultation with Native American tribes who have participated in consultation for the project. The Cultural Resources Mitigation and Monitoring Program shall	Prior to grading or ground disturbance	Project Applicant	A qualified archaeologist who meets or exceeds the Secretary of Interior's Professional Qualifications Standards for archaeology shall be retained to prepare a Cultural Resources Mitigation and Monitoring Program for unanticipated discoveries during project construction.	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>include provisions for archaeological, historic and Native American monitoring of initial ground disturbance related to construction of the project, project construction schedule, procedures to be followed in the event of discovery of archaeological resources including additional artifacts or features associated with RIN-S-1, and protocols for Native American coordination and input, including review of documents. The Cultural Resources Mitigation and Monitoring Program shall outline the role and responsibilities of Native American monitor(s). It shall include communication protocols and opportunity and timelines for review of cultural resources documents related to discoveries that are Native American in origin. The Cultural Resources Mitigation and Monitoring Program shall include provisions for Native American monitoring during testing or data recovery efforts for unknown resources that are Native American in origin, shall any be encountered. Once completed, the Cultural Resources Mitigation and Monitoring Program shall be reviewed and approved by the Project Planner at the City of Santee prior to the start of any ground disturbing activities.</p>				

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>CUL-3: Cultural Resources Construction Monitoring: A qualified archaeologist who meets or exceeds the Secretary of Interior’s Professional Qualifications Standards for archaeology shall be present during ground-disturbing activity for project construction, including but not limited to site clearing, grubbing, trenching, and excavation, for the duration of the project ground disturbance or until the qualified archaeologist determines monitoring is no longer necessary.</p> <p>At the completion of monitoring, the qualified archaeologist should prepare a Cultural Resources Monitoring Report to document the findings during the monitoring effort for the project. The report shall include the monitoring logs completed for the project and shall document any discoveries made during monitoring. The Cultural Resources Monitoring Report shall be submitted to the City of Santee and the South Coastal Information Center.</p>	During ground-disturbing activity	Project Applicant	<p>A qualified archaeologist who meets or exceeds the Secretary of Interior’s Professional Qualifications Standards for archaeology shall be present during ground-disturbing activity for project construction.</p> <p>At the completion of monitoring, the qualified archaeologist should prepare a Cultural Resources Monitoring Report to document the findings during the monitoring effort for the project.</p>	
<p>CUL-4: Native American Construction Monitoring: A minimum of one Native American monitor shall be present during ground-disturbing activity for project ground disturbance, including but not limited to site clearing, grubbing, trenching, and excavation, for the duration of the project ground disturbance or until the qualified archaeologist determines monitoring is no longer necessary. The Native American monitors shall be of Kumeyaay descent with ancestral ties to the San Diego region and at minimum one year of monitoring experience within Kumeyaay ancestral territory.</p>	During ground-disturbing activity	Project Applicant	A minimum of one Native American monitor of Kumeyaay descent shall be present during ground-disturbing activity.	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>CUL-5: California Health and Safety Code Section 7050.5: During project construction, if human remains are found, the California Health and Safety Code Section 7050.5 states no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. Per the Public Resources Code, in the event of an unanticipated discovery of human remains, the County Coroner shall be notified immediately. If the human remains are determined to be prehistoric, the coroner shall notify the Native American Heritage Commission, which will determine and notify a Most Likely Descendant.</p>	During ground-disturbing activity and construction	Project Applicant	If human remains are found, the California Health and Safety Code Section 7050.5 states no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. Per the Public Resources Code, in the event of an unanticipated discovery of human remains, the County Coroner shall be notified immediately. If the human remains are determined to be prehistoric, the coroner shall notify the Native American Heritage Commission, which will determine and notify a Most Likely Descendant.	
Noise				
<p>NOISE-1: Construction Noise Prior to issuance of any grading permit(s) for the project, the project applicant or its contractor(s) shall ensure that:</p> <ul style="list-style-type: none"> • On-site rocks and boulders shall be relocated off-site to the maximum extent feasible. • All on-site rock crushing shall occur at a distance of 165 feet or more from the southern, eastern, and western property lines. • All construction equipment, fixed or mobile, shall be equipped with properly operating and maintained mufflers. • Construction noise reduction methods such as shutting off idling equipment, maximizing the distance between construction equipment staging areas and occupied residential areas, and use of electric air compressors and similar power tools, rather than diesel equipment, shall be used where feasible. 	Prior to issuance of any grading permit(s)	Project Applicant	<p>The project applicant or its contractor(s) shall ensure that:</p> <ul style="list-style-type: none"> • On-site rocks and boulders shall be relocated off-site to the maximum extent feasible. • All on-site rock crushing shall occur at a distance of 165 feet or more from the southern, eastern, and western property lines. • All construction equipment, fixed or mobile, shall be equipped with properly operating and maintained mufflers. • Construction noise reduction methods such as shutting off idling equipment, maximizing the distance between construction equipment staging areas and occupied residential areas, and use of electric air 	

MITIGATION MONITORING AND REPORTING PROGRAM

Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<ul style="list-style-type: none"> • During construction, stationary construction equipment shall be placed such that emitted noise is directed away from or shielded from sensitive noise receivers. • During construction, stockpiling and vehicle staging areas shall be located as far as practical from noise sensitive receptors. • The project shall be in compliance with the City's Noise Abatement and Control Ordinance such that construction shall occur on the weekdays (Monday through Friday) and Saturday between the hours of 7:00 a.m. to 7:00 p.m. and a notice of construction shall be mailed to all owners and occupants within 300 feet of the project site no more than 10 days before the start of construction. Construction hours, allowable workdays and the phone number of the job superintendent shall be clearly posted at all construction entrances to allow surrounding property owners and residents to contact the job superintendent. In the event that the City receives a complaint regarding construction noise, appropriate corrective actions shall be implemented and a report of the action provided to the reporting party. 			<p>compressors and similar power tools, rather than diesel equipment, shall be used where feasible.</p> <ul style="list-style-type: none"> • During construction, stationary construction equipment shall be placed such that emitted noise is directed away from or shielded from sensitive noise receivers. • During construction, stockpiling and vehicle staging areas shall be located as far as practical from noise sensitive receptors. <p>The project shall comply with the City's Noise Abatement and Control Ordinance such that construction shall occur on the weekdays (Monday through Friday) and Saturday between the hours of 7:00 a.m. to 7:00 p.m. and a notice of construction shall be mailed to all owners and occupants within 300 feet of the project site no more than 10 days before the start of construction. Construction hours, allowable workdays and the phone number of the job superintendent shall be clearly posted at all construction entrances to allow surrounding property owners and residents to contact the job superintendent. In the event that the City receives a complaint regarding construction noise, appropriate corrective actions shall be implemented and a report of the action provided to the reporting party.</p>	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>NOISE-2: Operational Noise</p> <p>The project shall incorporate the following measures:</p> <ul style="list-style-type: none"> • Prior to precise grading, a six-foot masonry wall noise barrier shall be constructed along the western, southern, and eastern project boundaries as depicted in Figure 6a. The masonry wall shall be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue. The sound attenuation walls must be solid and free of cracks, gaps, or holes through or below the wall. Any seams or cracks must be filled or caulked. • The manufacturer noise specifications for the car wash blower system selected for the project shall include a silencer package and shall not exceed a sound power level of 106 dB(A) L_{pw}. • The manufacturer noise specifications for the car wash vacuum hoses selected for the project shall not exceed a sound power level of 77.3 dB(A) L_{pw}. • Operation of the car wash shall be prohibited during the hours of 7 p.m. and 7 a.m. 	<ul style="list-style-type: none"> • Prior to precise grading • Prior to approval of grading permit, noise specifications and operation shall be included as notes on the grading plans 	Project Applicant	<ul style="list-style-type: none"> • Prior to precise grading, a six-foot masonry wall noise barrier shall be constructed along the western, southern, and eastern project boundaries as depicted in Figure 6a. The masonry wall shall be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue. The sound attenuation walls must be solid and free of cracks, gaps, or holes through or below the wall. Any seams or cracks must be filled or caulked. • The manufacturer noise specifications for the car wash blower system selected for the project shall include a silencer package and shall not exceed a sound power level of 106 dB(A) L_{pw}. • The manufacturer noise specifications for the car wash vacuum hoses selected for the project shall not exceed a sound power level of 77.3 dB(A) L_{pw}. • Operation of the car wash shall be prohibited during the hours of 7 p.m. and 7 a.m. 	

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- I: Storm Water Quality Management Plan
- J: Noise Analysis

**CITY OF SANTEE
INITIAL STUDY/ENVIRONMENTAL CHECKLIST FORM**

1. Project Title

Santee Auto Center Project

2. Lead Agency Name and Address

City of Santee
10601 Magnolia Avenue
Santee, CA 92071

3. Contact Person and Phone Number

Mr. Michael Coyne
Principal Planner
City of Santee
(619) 258-4100 x160

4. Project Location

The Santee Auto Center Project (project) is located on the southeast corner of the Mission Gorge Road and Cottonwood Avenue intersection in the city of Santee (Assessor's Parcel Number [APN]: 384-091-01, -13, and -14). The project site is accessible via Cottonwood Avenue, Mission Gorge Road, and Railroad Avenue.

5. Project Applicant/Sponsor's Name and Address

Cameron Brothers Construction Co.
10580 Prospect Avenue, Suite 200
Santee, CA 92071

6. General Plan Designation

General Commercial (GC)

7. Zoning

General Commercial (GC)

All reports and documents referenced in this Initial Study are on file with the City of Santee, Department of Development Services, 10601 Magnolia Avenue, Santee, CA 92071. Telephone Number: (619) 258-4100, ext. 167. A digital copy is available from the City website: <http://cityofsanteeca.gov/services/project-environmental-review>.

8. Project Description

The project site is located the southeast corner of Mission Gorge Road and Cottonwood Avenue intersection on approximately 13.1 acres (APN 384-091-01, -13, and -14), located in the city of Santee, California, north of State Route 52 (SR-52). Access to the project site would be provided via two driveways on Cottonwood Avenue, three driveways on Mission Gorge Road, and one driveway on Railroad Avenue. Figure 1 shows the project's regional location. Figure 2 shows the project's specific location on U.S. Geological Survey map. Figure 3 shows an aerial photograph of the project site and vicinity. Figure 4 depicts the proposed site plan, which includes construction of two auto dealerships with sales and service buildings, a detail bay, a self-service car wash, and a body shop.

On Parcel A (APN 384-091-14), the project would construct a 33,974-square-foot auto dealership consisting of auto sales and service and a 2,549-square-foot detail bay which can hold six vehicles. The two-level, 33,974-square-foot, auto sales and service building would consist of a sales and showrooms area, parts department, service department, and administrative office. Level one would total 30,992 square feet and level two would solely consist of parts storage and total 2,549 square feet. Parcel A would have approximately 100 employees and hours of operation would be 7 a.m. to 9 p.m. Monday through Sunday. Parcel A would include 358 parking spaces which exceeds the required 93 spaces specified in 13.24.040 of the Santee Municipal Code. Of the 358 parking spaces, four spaces would be Americans with Disabilities Act (ADA) accessible, 12 spaces would be clean air vehicle spaces, and nine spaces would be electric vehicle (EV) spaces. The parking spaces would be allocated for car display (56 spaces), car inventory (209 spaces), cars for service (43 spaces) and customer parking (50 spaces). One, two capacity bicycle rack would also be provided. Motorcycle parking would also be provided per Santee Municipal Code Section 13.24. Parcel A would provide 93 spaces for employee and customer parking, which meets the required spaces specified in 13.24.040 of the Santee Municipal Code.

On Parcel B (APN 384-091-01), the project would construct a second auto dealership consisting of a 33,112-square-foot auto sales and service building. Similar to Parcel A, the two-level 33,112-square-foot auto sales and service building would consist of a sales and showrooms area, parts department, service department, and administrative office. Level one would total 30,015 square feet and level two would solely consist of parts and total 3,097 square feet. Parcel B would have approximately 100 employees and hours of operation would be 7 a.m. to 9 p.m. Monday through Sunday. Parcel B would also include 218 parking spaces, which exceeds the required 84 spaces specified in 13.24.040 of the Santee Municipal Code. Of the 218 parking spaces, four spaces would be ADA accessible, 12 spaces would be clean air vehicle spaces, and nine spaces would be EV spaces. The parking spaces would be allocated for car display (38 spaces), car inventory (94 spaces), cars for service (65 spaces) and customer parking (21 spaces). One, two capacity bicycle rack would also be provided. Motorcycle parking would also be provided per Santee Municipal Code Section 13.24. Parcel B would provide 84 spaces for employee and customer parking, which meets the required spaces specified in 13.24.040 of the Santee Municipal Code.

On Parcel C (APN 384-091-13), the project would construct a 5,400-square-foot self-service car wash, and a 16,405-square-foot body shop. The 16,405-square-foot body shop would consist of a customer reception area, parts department, body shop, paint booths, reception canopy and general office. The proposed carwash on Parcel C would have approximately four employees and hours of operation would be 7 a.m. to 7 p.m. Monday through Saturday. The proposed body shop on Parcel C would

have approximately 23 employees and hours of operation would be 8 a.m. to 5:30 p.m. Monday through Friday. The project proposes 112 parking spaces within Parcel C, which exceeds the required 57 spaces specified in 13.24.040 of the Santee Municipal Code. Of the 112 parking spaces, three spaces would be ADA accessible, nine spaces would be clean air vehicle spaces, and six would be EV spaces. The parking spaces would be allocated for cars for service (78 spaces) and customer parking and manual car drying (34 spaces). Two, two capacity bicycle racks would also be provided.

Screening and Landscaping

Parking lot screening would be constructed in accordance with Santee Municipal Code 13.24.030.A.8. The project would construct a six-foot masonry wall noise barrier shall be constructed along the western, southern, and eastern project boundaries. The masonry wall shall be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue. The project would construct a four-foot-high concrete retaining wall along the west side of the project site. A six-foot-high screen wall would be constructed along the southern edge of the property, and the project would contain three-foot tree boxes to screen the project from the adjacent residential homes to the south. The project would also include a landscape buffer along the property boundary with a meandering, non-contiguous sidewalk along Mission Gorge Road. Parcel A would provide 91 trees, Parcel B would provide 84 trees, and Parcel C would provide 57 trees.

Site Access

A total of six access driveways are proposed for the project. This includes three access driveways on Mission Gorge Road, two access driveways on Cottonwood Avenue, and one on Railroad Avenue, as described below:

- A right-in/right-out only driveway located the just east of Cottonwood Avenue on Mission Gorge Road
- A right-in/right-out only driveway located east of Project Driveway #1 on Mission Gorge Road
- A full access driveway forming the fourth (south) leg of the Mission Gorge Road/Edgemoor Drive intersection.
- A full access driveway located on Cottonwood Avenue, just south of Mission Gorge Road
- A full access driveway located Cottonwood Avenue, just south of Project Driveway #4
- A full access driveway located on Railroad Avenue, south of Mission Gorge Road

Security Lighting and Cameras

The project site would be well lit to provide convenience and security at any time of day. All project lighting would be implemented consistent with the City Municipal Code Section 13.08.070 Development Review Criteria. Under Section 13.08.070, light fixtures for walks, parking areas, driveways, and other facilities shall be provided in sufficient number and at proper locations to provide illumination and clear visibility to all outdoor areas, with minimal shadows or light leaving the property. The lighting shall be stationary, directed away from adjacent properties and shielded

so that no light or glare is transmitted or reflected in such concentrated quantities or intensities as to be detrimental to the surrounding area.

9. Project Site Existing Conditions and Surrounding Land Use(s)

The 13.1-acre project site is the location of the original Santee School (formerly, Cowles School constructed in 1891). The school was demolished and reconstructed during the 1960s and demolished again in 2007. The project site includes several bedrock outcrops, two existing parking lots, several foundations, and the remnants of baseball fields. Land uses surrounding the project site include commercial uses to the north, multi-family residential uses to the east, single-family residences to the south, and multi-family residential uses to the west.

10. Other Required Agency Approvals or Permits Required

General Construction Permit (San Diego Regional Water Quality Control Board)

11. Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code Section 21080.3.1? If so, is there a plan for consultation that includes, for example, the determination of significance of impacts to tribal cultural resources, procedures regarding confidentiality, etc.?

On July 3, 2023, the City prepared and sent Assembly Bill (AB) 52 notification letters to the four tribal contacts that formally requested notification of projects in the City via certified mail. The City received evidence that three of the four tribes received the notification. No evidence was provided to the City that the fourth tribal organization received the notice. Of the three tribal organizations that received the notice, one was returned to the City with a "return to sender" notification.

Under California Public Resources Code, Section 21080.3.1(b), the tribes had 30 days from the receipt of the notification letters to request consultation under AB 52. Within the 30-day response period, the City received one response to the AB 52 consultation letters from the Barona Band of Mission Indians (Barona).

In an email dated July 7, 2023, Barona requested AB 52 consultation for the project, copies of prior cultural resource surveys or reports and more information on the proposed monitoring program. City staff coordinated with Barona as to the preferred format of the reports (i.e., hard copy, electronic, thumb drive). Three reports were sent to Barona electronically in three separate emails on July 12, 2023. On July 13, 2023, Barona confirmed receipt of all the reports and concluded that the mitigation recommended (starting on page 6 of the Rincon report) "seems adequate for the resources that have been identified." Barona further stated: "However, if anything significant is encountered before or during construction, I definitely want to hear about it." On July 14, 2023, Barona confirmed that the AB 52 consultation was concluded "as long as the mitigation measures are included as conditions of approval and are implemented as the project goes forward." As of the publication of this MND, the AB 52 consultation has concluded.

12. Statement of Environmental Findings

An Initial Study was prepared by the City to evaluate the potential effects of the project on the environment. As Lead Agency under the California Environmental Quality Act (CEQA) and based on

the finding contained in the attached Initial Study, the City has determined that the project would not have a significant effect upon the environment with implementation of the proposed mitigation measures.

The City also finds that the Initial Study reflects the City’s independent judgement.

The location and custodian of the documents and any other materials which constitute the record of proceedings upon which the City bases its determination to adopt this Mitigated Negative Declaration are as follows: City of Santee, Department of Development Services, 10601 Magnolia Avenue, Santee, California.

13. Summary of Environmental Factors Potentially Affected

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” as indicated by the checklist on the following pages.

- | | | |
|--|---|--|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture and Forestry Resources | <input type="checkbox"/> Air Quality |
| <input checked="" type="checkbox"/> Biological Resources | <input checked="" type="checkbox"/> Cultural Resources | <input type="checkbox"/> Energy |
| <input type="checkbox"/> Geology/Soils | <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials |
| <input type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Land Use/Planning | <input type="checkbox"/> Mineral Resources |
| <input checked="" type="checkbox"/> Noise | <input type="checkbox"/> Population/Housing | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Recreation | <input type="checkbox"/> Transportation | <input checked="" type="checkbox"/> Tribal Cultural Resources |
| <input type="checkbox"/> Utilities/Service Systems | <input type="checkbox"/> Wildfire | <input checked="" type="checkbox"/> Mandatory Findings of Significance |

14. Determination

I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.	
I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. A MITIGATED NEGATIVE DECLARATION will be prepared.	X
I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.	
I find that the proposed project MAY have a significant effect(s) on the environment, but at least one effect (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a "potentially significant impact" or "potentially significant unless mitigated." An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.	
I find that although the proposed project could have a significant effect on the environment, there WILL NOT be a significant effect in this case because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION , including revisions or mitigation measures that are imposed upon the proposed project, and nothing further is required	



Signature

August 11, 2023

Date

Michael Coyne

Printed Name and Title

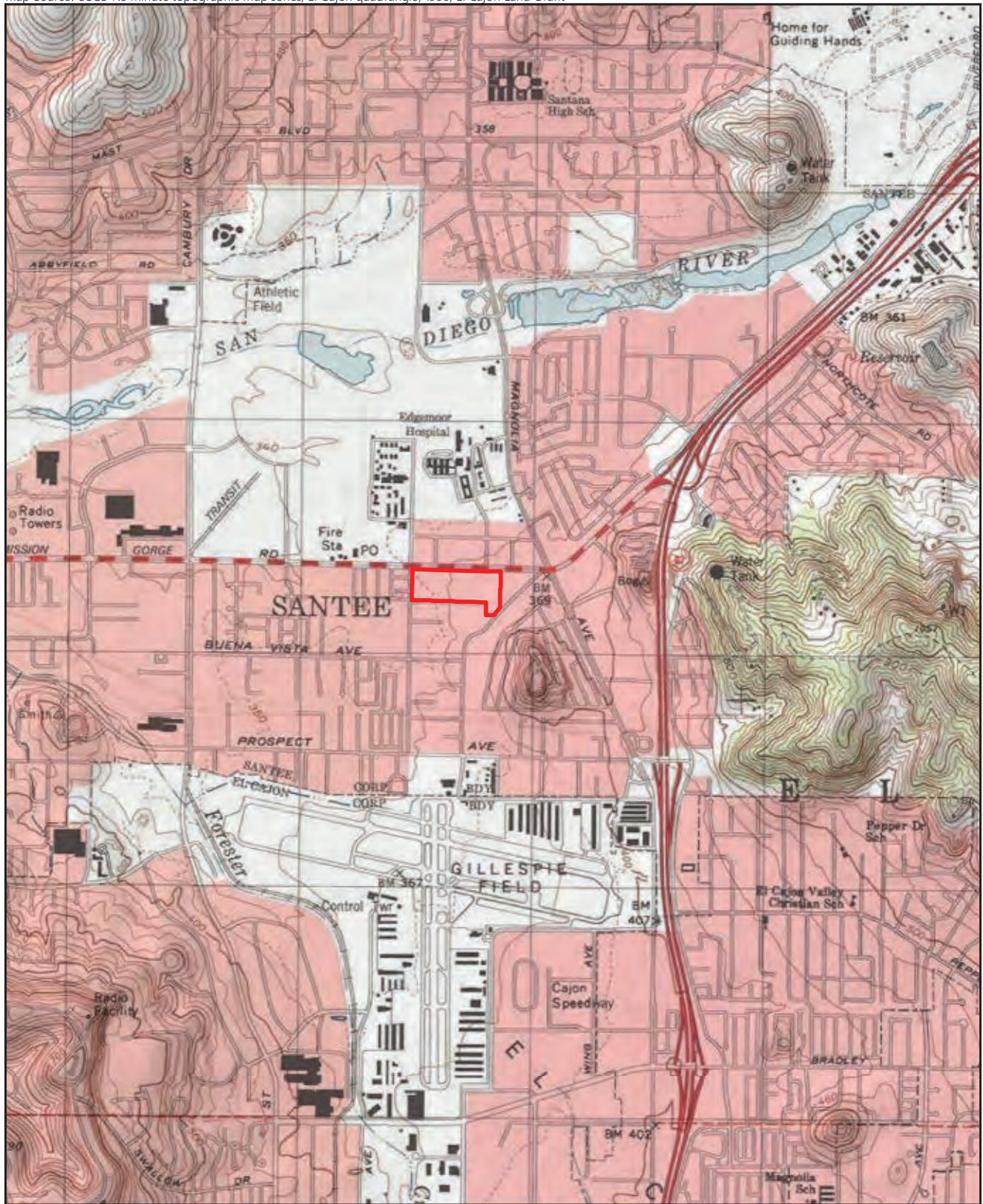
City of Santee

For



 Project Location

FIGURE 1
Regional Location



 Project Boundary

FIGURE 2
Project Location on USGS Map



 Project Boundary

FIGURE 3
Project Location on Aerial Photograph

15. Environmental Checklist Form

15.1 Aesthetics

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially damage scenic resources, including but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from a publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Project Plans; City of Santee General Plan (Conservation, Community Enhancement, and Circulation Elements); Santee Municipal Code.

a. No Impact. The City General Plan identifies existing visual resources, including the San Diego River and other waterway corridors, undeveloped hillsides and ridgelines, the Santee Town Center, Santee Lakes and Mission Trails Regional Parks, and the San Diego Trolley. The project site is not located adjacent to any of these visual resources, nor are there views of any of these sites from the property. The project site is located within an urbanized environment along the Mission Gorge Road corridor and is surrounded by commercial and residential uses. Additionally, the project site is not designated as open space, nor does it possess views of any areas designated as open space. Therefore, the project would not have a substantial adverse effect on a scenic vista. No impact would occur.

b. Less Than Significant Impact. There are no designated state scenic highways within the city of Santee. The segment of SR-52 that is designated as a state scenic highway (Santo Road to Mast Boulevard) is located in the city of San Diego, approximately 2.5 miles to the west, and is not visible from the project site. The project site does not possess any scenic resources such as trees. There are

large boulders and bedrock located in the southeast portion of the project site. However, removal of the boulders and bedrock would not be considered significant because they are not listed as a scenic resource within the City General Plan. As described in Section 15.5.a below, there are no historic resources located on the project site. Therefore, the project would not substantially damage any scenic resources within a state scenic highway, and impacts would be less than significant.

c. Less Than Significant Impact. The project site is located within an urban environment consisting of commercial and residential uses located along the southern frontage of Mission Gorge Road. The project site is vacant and currently is undeveloped. The site was originally developed as an elementary school in the 1920s and remained as a school site until being demolished in 2007. The project would be consistent with the existing visual character because it would construct a commercial use within an area that currently consists of a mix of commercial and residential uses. The project has also been designed with and will comply with applicable zoning regulations pertaining to scenic quality and would include landscaping to enhance the visual quality of the project site. Therefore, the project would not substantially degrade the existing visual character or quality of the site and its surroundings, and impacts would be less than significant.

D. Less Than Significant Impact. Project construction would be limited to the City's allowable construction hours of 7:00 a.m. and 7:00 p.m. and is not anticipated to require lighting. In the event that construction lighting is required, it would be properly shielded to avoid spillover effects. Once constructed, the project would not include large uninterrupted expanses of glass or any other highly reflective material that could generate glare during the daytime.

The project would include outdoor lighting typical of commercial uses. The project would utilize light-emitting diode (LED) shielded lighting on the buildings to provide both security and path of travel lighting. Light spillover, trespass, and potential glare from project lighting are regulated by Section 13.30.030(B) of the Santee Municipal Code. The code requires that all lights and illuminated signs must be designed and adjusted to reflect light away from any road or street, away from any adjoining premises, and shall be shielded or directed to not cause glare on adjacent properties or motorists. Project lighting would be designed consistent with the requirements of Section 13.30.030(B) of the Santee Municipal Code. Light associated with additional vehicle trips generated by the project would be similar in character to what is currently generated by vehicles traveling along the existing roadway network after dark. Therefore, the project would not create a new source of substantial light or glare that would adversely affect day or nighttime views in the area, and impacts would be less than significant.

15.2 Agriculture Resources

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act Contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 12220[g]), timberland (as defined by Public Resources Code Section 4526), or timberland zoned Timberland Production (as defined by Government Code Section 51104[g])?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: City of Santee General Plan–Land Use Element; City of Santee Zoning Ordinance; California Department of Conservation–Farmland Mapping and Monitoring Program, 2016

a. No Impact. The project site and surrounding properties are not identified as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance. The Farmland Mapping and Monitoring Program classifies the project site and surrounding properties as “Urban and Built Up Land” (California Department of Conservation 2016). No impact would occur.

b. No Impact. The project site and surrounding properties are not zoned for agricultural uses and are not subject to a Williamson Act contract. No impact would occur.

c. No Impact. The project site does not contain any forest or timberland as defined by Public Resources Code Section 12220(g), Public Resources Code Section 4526, or Government Code Section 51104(g) and is not zoned as forest or timberland. No impact would occur.

d. No Impact. The project site does not contain any forest or timberland as defined by Public Resources Code Section 12220(g), Public Resources Code Section 4526, or Government Code Section 51104(g). No impact would occur.

e. No Impact. Land uses surrounding the project site include commercial uses to the north, multi-family residential uses to the east, single-family residences to the south, and multi-family residential uses to the west. There are no agricultural uses or forestlands on-site or in the vicinity of the project site. Therefore, the project would not result in conversion of farmland or forest land. No impact would occur.

15.3 Air Quality

Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Result in other emissions such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Project Description, City of Santee General Plan–Land Use Element; Air Quality Model Results (California Emissions Estimator Model [CalEEMod] Output Files) prepared by RECON Environmental, Inc. (Appendix A); Linscott, Law, and Greenspan Engineers Traffic Impact Analysis (Appendix B); San Diego Air Pollution Control District (SDAPCD) Rules 20.1, 20.2, 20.3, 67.0.1, 67.20.1; Office of Environmental Health Hazard Assessment (OEHHA) Air Toxics Hot Spots Program Guidance Manual for the Preparation of Risk Assessments (OEHHA 2015).

A. Less than Significant impact. Following the California Clean Air Act, California was divided geographically into 15 air basins for managing the state air resources on a regional basis. Areas within each air basin are considered to share the same air masses and, therefore, have similar ambient air quality. The project site is located within the San Diego Air Basin (SDAB). Stationary sources of air emissions within each air basin are regulated by regional air quality districts, of which the project is located within the jurisdiction of the SDAPCD.

Air districts are tasked with regulating emissions such that air quality in the basin does not exceed national or California ambient air quality standards (NAAQS and CAAQS); where NAAQS and CAAQS represent the maximum levels of background pollution considered safe, with an adequate margin of safety, to protect the public health and welfare. NAAQS and CAAQS have been established for six common pollutants of concern known as criteria pollutants, which include ozone (O₃), carbon monoxide (CO), sulfur dioxide (SO₂), nitrogen dioxide (NO₂), lead (Pb), and respirable particulate matter (particulate matter less than 10 microns [PM₁₀] and less than 2.5 microns [PM_{2.5}]).

The SDAB is currently classified as a federal and state non-attainment area for ozone, and as a state non-attainment area for PM₁₀, and PM_{2.5}. The SDAPCD prepared an air quality plan, the 2016 Regional Air Quality Strategy (RAQS), to identify feasible emission control measures intended to progress toward attaining NAAQS and CAAQS for ozone. Reducing ozone concentrations is achieved by reducing the precursors to the photochemical formation of ozone (volatile organic compounds [VOC] and oxides of nitrogen [NO_x]).

The growth forecasting for the RAQS is based in part on the land uses established by local general plans. Thus, if a project is consistent with land use designated in the local general plan, it can normally be considered consistent with the RAQS. Projects that propose a different land use than is identified in the local general plan may also be considered consistent with the RAQS if the proposed land use is less intensive than the current land use designation. For projects that propose a land use that is more intensive than the current zoning designation, detailed analysis is required to assess conformance with the RAQS.

The project site is currently designated and zoned as General Commercial (GC). The project would be consistent with the existing land use and zoning designations for the project site, and therefore would be consistent with the growth assumptions of the General Plan. Additionally, as discussed in Section 15.3.b below, project emissions would not exceed the project-level significance thresholds. Therefore, the project would not result in an increase in emissions that are not already accounted for in the RAQS, and impacts would be less than significant.

b. Less than Significant Impact. As discussed in Section 15.3.a above, NAAQS and CAAQS have been established for six criteria pollutants (ozone, CO, SO₂, NO₂, lead, and particulate matter). The City has not adopted air quality significance thresholds for these pollutants, and the SDAPCD does not provide specific numeric thresholds for determining the significance of air quality impacts under the CEQA Guidelines. However, the SDAPCD does specify air quality impact analysis "trigger" levels for criteria pollutant emissions associated with new or modified stationary sources (SDAPCD Rules 20.1, 20.2, and 20.3). The SDAPCD does not consider these trigger levels to represent adverse air quality impacts; rather, if these trigger levels are exceeded by stationary sources associated with a project, the SDAPCD requires an air quality analysis to determine if a significant air quality impact would occur. This analysis uses SDAPCD trigger levels shown in Table 1 as air quality impact screening levels.

Table 1 Air Quality Impact Analysis Trigger Levels			
Pollutant	Emission Rate (pounds per hour)	Emission Rate (pounds per day)	Emission Rate (tons per year)
NO _x	25	250	40
SO _x	25	250	40
CO	100	550	100
PM ₁₀	--	100	15
Lead	--	3.2	0.6
ROG ¹	--	250	--
PM _{2.5}	--	67	10

SOURCE: SDAPCD, Rules 20.1, 20.2, 20.3 (SDAPCD 2016).
¹ The reactive organic gases (ROG) threshold is based on federal General Conformity de minimis levels for ozone precursors.

The project would result in short-term emissions from construction and long-term emissions associated with project operation. Construction and operational emissions associated with the project were modeled using CalEEMod version 2022.1 (see Appendix A), which incorporates current air emission data. Planning methods, protocol, modeling methodology, and assumptions are summarized below.

Construction Emissions

Construction-related activities are temporary, short-term sources of air emissions. Sources of construction-related air emissions include the following:

- Fugitive dust from grading and rock breaking activities;
- Construction equipment exhaust;
- Construction-related trips by workers, delivery trucks, and material-hauling trucks; and
- Construction-related power consumption.

Construction-related pollutants result from dust raised during demolition and grading, emissions from construction vehicles, and chemicals used during construction. Fugitive dust emissions vary greatly during construction and are dependent on the amount and type of activity, silt content of the soil, and the weather. Vehicles moving over paved and unpaved surfaces, demolition, excavation, earth movement, grading, and wind erosion from exposed surfaces are all sources of fugitive dust. Construction operations are subject to the requirements established in Regulation 4, Rules 52, 54, and 55 of the SDAPCD’s rules and regulations.

Heavy-duty construction equipment is usually diesel powered. In general, emissions from diesel-powered equipment contain more NO_x, SO_x, and particulate matter than gasoline-powered engines. However, diesel-powered engines generally produce less CO and less ROG than do gasoline-powered engines. Standard construction equipment includes tractors/loaders/backhoes, rubber-tired dozers, excavators, graders, cranes, forklifts, rollers, paving equipment, generator sets, welders, cement and mortar mixers, and air compressors. Rock breaking and crushing activities would also be required. It is anticipated that these activities would last approximately two months. First, the larger boulders would be drilled with a rock drill and chemicals would be used to break

them down to manageable sizes. Then, an excavator with a mounted 10,000-pound hydraulic hammer/breaker would break those rock pieces down to two-foot diameter or less fragments. These smaller rocks would then be hauled off-site or crushed on-site.

Primary inputs are the numbers of each piece of equipment and the length of each construction stage. Specific construction phasing and equipment parameters are not available at this time. However, CalEEMod can estimate the required construction equipment when project-specific information is unavailable. The estimates are based on surveys (performed by the South Coast Air Quality Management District and the Sacramento Metropolitan Air Quality Management District) of typical construction projects, which provide a basis for scaling equipment needs and schedule with a project’s size. Air emission estimates in CalEEMod are based on the duration of construction phases; construction equipment type, quantity, and usage; grading area; season; and ambient temperature, among other parameters. Construction is anticipated to begin in 2024 and last for one year. Construction emissions for most phases were calculated using CalEEMod default phasing and equipment for the given land use, project site size, and building size. To account for rock drilling and crushing, a rock drill, crushing/processing equipment, and an additional excavator were added to the grading phase. Project grading would include the export of 4,500 cubic yards of soil.

Table 2 shows the total projected construction maximum daily emission levels for each criteria pollutant. The CalEEMod output files for construction emissions for the project are contained in Appendix A.

Table 2 Summary of Maximum Build-out Construction Emissions (pounds per day)						
	ROG	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
Site Preparation	4	36	34	<1	9	5
Grading	38	39	83	<1	6	3
Building Construction/ Architectural Coatings	15	13	16	<1	1	1
Paving	3	8	11	<1	1	<1
Maximum Daily Emissions	38	39	83	<1	9	5
<i>Significance Threshold</i>	<i>250</i>	<i>250</i>	<i>550</i>	<i>250</i>	<i>100</i>	<i>67</i>
Source: Appendix A						

Standard dust control measures would be implemented as a part of project construction in accordance with mandatory SDAPCD rules and regulations. Fugitive dust emissions were calculated using CalEEMod default values with implementation of SDAPCD dust control measures.

To assess the significance of the air quality emissions resulting from construction of the project, construction emissions were compared to the significance thresholds. As shown, maximum daily construction emissions associated with the project are projected to be less than the applicable thresholds for all criteria pollutants. These thresholds are designed to provide limits below which project emissions would not significantly change regional air quality. In addition, the project applicant would implement standard construction measures in order to comply with mandatory SDAPCD rules and regulations (Rules 50, 51, 52, 54, and 55) for controlling emissions from fugitive dust and fumes:

- Water the grading areas a minimum of twice daily to minimize fugitive dust.
- Provide sufficient erosion control to prevent washout of silty material onto public roads.
- Cover haul trucks or maintain at least 12 inches of freeboard to reduce blow-off during hauling.
- Periodically sweep up dirt and debris spilled onto paved surfaces to reduce re-suspension of particulate matter caused by vehicle movement. Clean approach routes to construction sites of construction-related dirt.

Further, all construction equipment is subject to the California Air Resources Board (CARB) In-Use Off-Road Diesel-Fueled Fleets Regulation. This regulation, which applies to all off-road diesel vehicles 25 horsepower or greater, limits unnecessary idling to five minutes, requires all construction fleets to be labeled and report to CARB, bans Tier 0 equipment and phases out Tier 1 and 2 equipment (thereby replacing fleets with cleaner equipment), and requires that fleets comply with Best Available Control Technology requirements.

Therefore, as project construction emissions would be well below these limits and the project would implement standard construction measures in order to comply with SDAPCD rules and regulations and CARB's In-Use Off-Road Diesel-Fueled Fleets Regulation, construction emissions would not result in regional emissions that would exceed the NAAQS or CAAQS or contribute to existing violations. Therefore, construction of the project would not result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment, and impacts would be less than significant.

Operational Emissions

Operation of the project would result in long-term emissions from mobile, energy, and area sources. Mobile emissions were calculated using Institute of Transportation Engineers (ITE) 11th Edition trip generation rates. The two car dealerships and body shop portion of the project would generate 2,436 daily trips and the car wash would generate 900 daily trips for a total of 3,336 daily trips. CalEEMod default trip lengths and vehicle emission factors for the soonest operational year of 2025 were modeled.

Energy sources include emissions from the combustion of natural gas used for water heating, and area sources include emissions from the use of landscaping equipment, consumer products (aerosols, cleansers, etc.), and architectural coatings (e.g., building and parking lot paint). These energy and area sources were calculated based on default CalEEMod regional use factors.

ROG/VOC emission would also be generated by the paint surface coating activities associated with the spray booth. Although CalEEMod is not able to estimate ROG/VOC emissions associated specifically with the spray booth, it is not anticipated that the spray booth would generate significant ROG/VOCs due to the overall small size of the facility. In addition, the project would be required to adhere to the SDAPCD Rule 67.20.1: Motor Vehicle and Mobile Equipment Coating Operations. Based on the SDAPCD 2020 Industry Wide Emissions Inventory Report (SDAPCD 2020), the Caliber Collusion auto body repair shops similar to the one proposed emit up to 0.73 tons per year of ROG/VOC which equates to approximately 4 pounds per day. When combined with the emissions shown in Table 3 below, emissions would be well less than the significance threshold. With adherence to SDAPCD Rule 67.20.1, emissions due to the spray booth would be less than significant.

Table 3 provides a summary of the total operational emissions generated by the project. CalEEMod output files for operation of the project are contained in Appendix A.

Table 3 Summary of Maximum Build-out Operational Emissions (pounds per day)						
Emission Source	ROG	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
Area Sources	3	<1	4	<1	<1	<1
Energy Sources	<1	1	1	<1	<1	<1
Mobile Sources	14	11	100	<1	8	2
Total	17	12	104	<1	8	2
<i>Significance Threshold</i>	<i>250</i>	<i>250</i>	<i>550</i>	<i>250</i>	<i>100</i>	<i>67</i>
Source: Appendix A Note: Totals may vary due to independent rounding.						

As shown in Table 3, operation of the project would not generate regional emissions that would exceed the NAAQS or CAAQS or contribute to existing violations. Therefore, operation of the project would not result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment, and impacts would be less than significant.

c. Less than Significant Impact. A sensitive receptor is a person in the population who is more susceptible to health effects due to exposure to an air contaminant than is the population at large. Examples of sensitive receptor locations in the community include residences, schools, playgrounds, childcare centers, churches, athletic facilities, retirement homes, and long-term health care facilities. Land uses surrounding the project site include commercial uses to the north, multi-family residential uses to the east, single-family residences to the south, and multi-family residential uses to the west.

Diesel Particulate Matter–Construction

Construction of the project would result in short-term diesel exhaust emissions from on-site heavy-duty equipment. Construction of the project would result in the generation of diesel exhaust diesel particulate matter (DPM) emissions from the use of off-road diesel equipment required for site grading and excavation, paving, and other construction activities and on-road diesel equipment used to bring materials to and from the project site. Not all construction worker vehicles would be diesel-fueled and most DPM emissions associated with material delivery trucks and construction worker vehicles would occur off-site.

Generation of DPM from construction projects typically occurs in a single area for a short period. According to the OEHHA, health risk assessments, which determine the exposure of sensitive receptors to toxic emissions, should be based on a 30-year exposure period; however, such assessments should be limited to the period/duration of activities associated with the project (OEHHA 2015). Thus, if the duration of proposed construction activities near any specific sensitive receptor were a year, the exposure would be three percent of the total exposure period used for health risk calculation.

For purposes of analyzing construction-related toxic air contaminant emissions and their impact on sensitive receptors, the maximum annual PM₁₀ emissions from equipment exhaust were used to develop an average daily emission rate. The exhaust emissions were calculated by CalEEMod, and the maximum annual DPM concentration was calculated using AERSCREEN. AERSCREEN calculates a worst-case maximum 1-hour concentration at a specific distance and specific angle from the source. The maximum 1-hour concentration is then converted to an annual concentration using a 0.08 conversion factor (U.S. Environmental Protection Agency [U.S. EPA] 1992).

Once the dispersed concentrations of diesel particulates are estimated in the surrounding air, they are used to evaluate estimated exposure to people. Exposure is evaluated by calculating the dose in milligrams per kilogram body weight per day (mg/kg/d). For residential exposure, the breathing rates are determined for specific age groups, so inhalation dose (Dose-air) is calculated for each of these age groups: third trimester of pregnancy, 0<2, 2<9, 2<16, 16<30 and 16–70 years. The equation for dose through inhalation (Dose-air) is as follows:

$$\text{Dose-air} = (C_{\text{air}} \times \text{DBR} \times A \times \text{EF} \times 10^{-6});$$

Where:

- Dose-air = Chronic daily intake, mg/kg/d
- C_{air} = Ground-level concentration of toxic air contaminants to which the receptor is exposed, micrograms/cubic meter
- DBR = Daily breathing rate, normalized to body weight (liters per kilogram body weight per day (Office of Environmental Health Hazard Assessment [OEHHA] 2015)
- A = Inhalation absorption factor (OEHHA recommended factor of 1)
- EF = Exposure frequency, days/year (OEHHA recommended factor of 0.96 for resident and 0.68 for workers)

Cancer risk is calculated by multiplying the daily inhalation or oral dose, by a cancer potency factor, the age sensitivity factor, the frequency of time spent at home and the exposure duration divided by averaging time, to yield the excess cancer risk. The excess cancer risk is calculated separately for each age grouping and then summed to yield cancer risk for any given location. The worst-case cancer risk is calculated as follows:

$$\text{Excess Cancer Risk} = \text{Dose-air} \times \text{CPF} \times \text{ASF} \times \text{ED/AT} \times \text{FAH};$$

Where:

Dose-air	=	Chronic daily intake, mg/kg body weight per day
CPF	=	Cancer potency factor (mg/kg/d)
ASF	=	Age sensitivity factor
ED	=	Exposure duration (years)
AT	=	Averaging time for lifetime cancer risk (years)
FAH	=	Fraction of time at home

Non-cancer risks are defined as chronic or acute. With respect to DPM only chronic risks are calculated and are determined by the hazard index. To calculate hazard index, DPM concentration is divided by its chronic Reference Exposure Levels. Where the total equals or exceeds one, a health hazard is presumed to exist.

In this analysis, non-carcinogenic impacts are evaluated for chronic exposure inhalation exposure. Estimates of health impacts from non-carcinogenic concentrations are expressed as a hazard quotient (HQ) for individual substances, such as diesel particulate. An HQ of one or less indicates that adverse health effects are not expected to result from exposure to emissions of that substance. Reference Exposure Levels are defined as the concentration at which no adverse health effects are anticipated. Generally, the inhalation pathway is the largest contributor to the total dose. The HQ is calculated with the following equation:

$$\text{HQ} = \text{Ground-Level Concentration } (\mu\text{g}/\text{m}^3) / \text{Reference Exposure Level } (\mu\text{g}/\text{m}^3)$$

It should also be noted that all construction equipment is subject to the CARB In-Use Off-Road Diesel-Fueled Fleets Regulation. This regulation, which applies to all off-road diesel vehicles 25 horsepower or greater, limits unnecessary idling to five minutes, requires all construction fleets to be labeled and reported to CARB, bans Tier 0 equipment and phases out Tier 1 and 2 equipment (thereby replacing fleets with cleaner equipment), and requires that fleets comply with Best Available Control Technology requirements.

Based on the CalEEMod calculations for project construction, the project would result in on-site maximum annual emissions of 0.1055 ton of PM₁₀ exhaust. This maximum annual emissions rate was modeled over the entire 12-month construction period, and therefore is a conservative assessment. Based on AERSCREEN modeling results, the maximum 1-hour ground-level DPM concentration from construction activities would be 0.02617 micrograms per cubic meter ($\mu\text{g}/\text{m}^3$). This was converted to an annual average concentration of 0.00209 $\mu\text{g}/\text{m}^3$ using a conversion factor of 0.08 (U.S. EPA 1992). The resulting annual concentration was used in the equations discussed above. Using this methodology, it was calculated that the excess cancer risk would be 0.38 in a million. AERSCREEN and cancer risk calculations are provided in Appendix A. DPM generated by project construction is

not expected to create conditions where the probability is greater than 10 in 1 million of contracting cancer. Additionally, the HQ would be 0.0004, which is less than one. Therefore, the project would not expose sensitive receptors to substantial pollutant concentrations associated with diesel particulate matter during construction that could result in excess cancer risks, and impacts would be less than significant.

Carbon Monoxide Hot Spots

Localized CO concentration is a direct function of motor vehicle activity at signalized intersections (e.g., idling time and traffic flow conditions), particularly during peak commute hours and meteorological conditions. The SDAB is a CO maintenance area under the federal Clean Air Act. This means that SDAB was previously a non-attainment area and is currently implementing a 10-year plan for continuing to meet and maintain air quality standards.

Due to increased requirements for cleaner vehicles, equipment, and fuels, CO levels in the state have dropped substantially. All air basins are attainment or maintenance areas for CO. Therefore, more recent screening procedures based on more current methodologies have been developed. The Sacramento Metropolitan Air Quality Management District developed a screening threshold in 2011, which states that any project involving an intersection experiencing 31,600 vehicles per hour or more will require detailed analysis. In addition, the Bay Area Air Quality Management District developed a screening threshold in 2010 which states that any project involving an intersection experiencing 44,000 vehicles per hour would require detailed analysis. Based on the intersection turning volumes provided in the traffic analysis prepared for the project (see Appendix B), intersection traffic volumes would be significantly less than 31,600 vehicles per hour. Consequently, the project is not anticipated to result in a CO hot spot. Therefore, project operation would not expose sensitive receptors to substantial pollutant concentrations, and impacts would be less than significant.

D. Less than Significant Impact. During construction, the use of fuels, including diesel, would generate some nuisance odors. However, these odors generated during construction would be temporary, intermittent, disperse quickly, and would not affect a substantial number of people. The project would not be a significant operational source of objectionable odors. The project would construct an auto service shop on Parcels A and B, and an auto body shop on Parcel C that would include coating operations; thus, the project could be subject to odor complaints. However, all operational activities would occur within the proposed building inside an enclosed and ventilated painting booth. Appropriate ventilation systems would be installed in accordance with state and SDAPCD rules and regulations. The car dealerships and car wash portions of the project would not be a source of objectionable odors. Therefore, the project would not generate odors adversely affecting a substantial number of people, and impacts would be less than significant.

15.4 Biological Resources

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Have substantial adverse effects, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife (CDFW) or U.S. Fish and Wildlife Service (USFWS)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations or by the CDFW or USFWS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: City of Santee Draft Multiple Species Conservation Program Subarea Plan; City of Santee, General Plan, Conservation Element

a. Less than Significant with Mitigation. The project site consists entirely of Urban/Developed Land as delineated on Figure 6-3 of the General Plan Conservation Element. Existing land cover on the project site includes non-vegetated pervious areas and is surrounded by development on all sides. In addition, the site was originally developed as an elementary school (Santee Elementary School) in the 1920s and was used as such until 2007. Subsequent grading to remove rock material has also occurred. Suitable nesting trees are located on the project site. If vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30), mitigation measure BIO-1 would require a pre-construction clearance survey for nesting birds and raptors. Implementation of mitigation measure BIO-1 would reduce potential impacts to nesting birds and raptors to a level less than significant.

b. No Impact. As described in 15.4(a), the project site consists entirely of Urban/Developed Land that does not qualify as riparian habitat. Therefore, there is no riparian habitat located on the project site. No impact would occur.

c. No Impact. No drainages, wetlands, or waters are located within the project site. Therefore, there are no state or federally protected wetlands located on the project site. No impact would occur.

d. No Impact. The project site consists of Urban/Developed Land, is surrounded by development on all sides, and does not connect separate isolated areas of habitat. In addition, the project site does not function as a wildlife corridor, nor are there any wildlife corridors adjacent to the project site within the surrounding urban environment. No impact would occur.

e. No Impact. The City's Urban Forestry Ordinance "sets forth tree-related policies, regulations, and generally accepted standards for planting, trimming, and removing trees on public property and public rights-of-way" (Ord. 561 § 3, 2019). The ordinance identifies native tree species such as coast live oak (*Quercus agrifolia*), canyon live oak (*Quercus chrysolepis*), Englemann oak (*Quercus engelmannii*), and western sycamore (*Platanus racemosa*) as "protected trees." However, there are no native trees located on the project site that would require protection under the City's Urban Forestry Ordinance. Therefore, the project would not conflict with any local policies or ordinances protecting biological resources, such as tree preservation policy or ordinance. No impact would occur.

f. No Impact. The City does not have an adopted Habitat Conservation Plan. The project site is classified as Urban/Developed and is not located within the Draft Preserve of the City's Draft Multiple Species Conservation Program Subarea Plan. The project site is not proposed for conservation and is not adjacent to any preserve areas. The project would not conflict with any local policies or ordinances protecting biological resources. Therefore, the project would not conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan. No impact would occur.

BIO-1 – Nesting Birds and Raptors

If vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30), a pre-construction clearance survey for nesting birds and raptors shall be conducted. The survey shall cover all suitable nesting habitats that occur within the proposed project site. The pre-construction survey shall be conducted within three days of the start of work.

If any active nests are detected, the area will be flagged and mapped along with a buffer as recommended by the qualified biologist. The buffer area(s) established by the qualified biologist will be avoided until the nesting cycle is complete or it is determined that the nest is no longer active. The qualified biologist shall be a person familiar with bird breeding behavior and capable of identifying the bird species of San Diego County by sight and sound and determining alterations of behavior as a result of human interaction. Buffers will be based on species-appropriate buffers and/or local topography and line of sight, species behavior and tolerance to disturbance, and existing disturbance levels, as determined appropriate by the qualified biologist.

15.5 Cultural Resources

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Cause a substantial adverse change in the significance of an historical resource pursuant to §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Disturb human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: City of Santee General Plan-Conservation Element; Cultural Resources Due Diligence Study prepared by Rincon Consultants, Inc. (2019, Appendix C); Phase II Cultural Resources Testing and Evaluation Report prepared by Rincon Consultants, Inc. (2019, Appendix D); Cultural Resources Technical Memorandum prepared by Rincon Consultants, Inc. (2023, Appendix E).

a and b. Less than Significant with Mitigation. The term “historic resources” applies to any such resource that is at least 50 years old and is listed or determined eligible for listing in the California Register of Historical Resources. In April and May 2019, Rincon Consultants completed pedestrian surveys of the project site and identified several prehistoric bedrock milling features (BMFs) and the archaeological remains of the historic period Santee Elementary School. The multi-component archaeological resource was given the temporary site designation RIN-S-1. RIN-S-1 includes the majority of the project site. Based on the positive findings of the survey, Rincon Consultants recommended that the archaeological remains be formally recorded and that a Phase II study be conducted to evaluate the significance of RIN-S-1 for listing on the California Register of Historical Resources (CRHR) and National Register of Historic Places (NRHP).

The Phase II evaluation program included field, laboratory, and archival studies. Field work was conducted by Rincon personnel between July 15 and 19, 2019. The field work effort consisted of site documentation and mapping and the excavation of 28 shovel test pits and 1 test unit. A Native American monitor from Red Tail Environmental was present during the archaeological test

excavations. Site documentation resulted in the recordation of 18 archaeological features, including 11 prehistoric BMFs and the remnants of 7 historic period buildings and structures. Test excavations revealed a low-density, subsurface deposit of prehistoric artifacts was associated with the BMFs. A larger, low-density, subsurface deposit of historic and modern artifacts was also identified extending across much of the project site. Examination of the prehistoric cultural deposits found extensive subsurface disturbance with prehistoric cultural constituents intermixed with historic and modern refuse.

Following completion of the field work, artifact analyses were conducted of the flaked and ground stone artifacts, faunal remains, and ceramics. Special studies, including obsidian X-ray fluorescence and hydration analyses, were also undertaken to acquire lithic sourcing and chronometric data, respectively. Finally, archival research was completed to obtain information on the history and development of Santee Elementary School.

Based on the results of the Phase II study, RIN-S-1 was recommended as ineligible for listing on the NRHP and CRHR. The prehistoric and historic components of the site were not associated with events that have made a significant contribution to broad patterns of prehistory or history (NRHP Criterion A/CRHR Criterion 1), nor are they associated with the lives of persons significant to the past (NRHP Criterion B/CRHR Criterion 2). They do not embody the distinctive characteristics of a type, period, or method of construction (NRHP Criterion C/CRHR Criterion 3). Finally, test excavations indicated that the prehistoric subsurface cultural deposits are not extensive and exhibit a high level of disturbance. Data recovery efforts on these have little potential to yield additional data pertinent to addressing research questions. In addition, further study of the historic period remains at the site is not expected to yield any new information important on either the construction or use of the Santee Elementary School. Therefore, the prehistoric or historic components of the site were recommended as ineligible for listing under NRHP Criterion D/CRHR Criterion 4.

In 2023, Rincon prepared a Cultural Resources Technical Memorandum (Appendix E) to document the current conditions of RIN-S-1 and the project site. Rincon archaeologists, as well as Annabel Flores from the Jamul Indian Village of the Kumeyaay Nation (Jamul) conducted a pedestrian survey of the project site on March 10, 2023, to ascertain the current conditions of the project site and archaeological site RIN-S-1. Rincon found that the multicomponent archaeological site RIN-S-1 was almost entirely displaced by boulder removal that occurred on the project site subsequent to the resource's 2019 recordation. All the historic-period features that were likely associated with Santee Elementary School have been removed.

Although RIN-S-1 was deemed ineligible for inclusion in the NRHP and CRHR, there is still potential for additional buried archaeological deposits that could be encountered during project related ground disturbance. Due to the overall sensitivity of the project area, the project would require completion of a Worker's Environmental Awareness Program prior to the start of ground disturbance for the project and that archaeological and Native American monitoring take place during initial project related ground disturbance. Implementation of mitigation measures CUL-1 through CUL-4 would reduce impacts to historic and unique archaeological resources to a level less than significant.

c. Less than Significant Impact. There are no dedicated cemeteries or recorded burials within the project footprint or surrounding vicinity and no human remains are known to be present within the project site. However, the discovery of human remains is always a possibility during

ground-disturbing activities. Implementation of mitigation measure CUL-5 requires the project to adhere to California Health and Safety Code Section 7050.5. Project implementation of mitigation measure CUL-5 would reduce impacts related to the discovery of human remains to a less than significant level.

CUL-1: Worker Environmental Awareness Program

Prior to the commencement of project-related ground-disturbing activities, including but not limited to site clearing, grubbing, trenching, and excavation, a qualified archaeologist who meets or exceeds the Secretary of the Interior's Professional Qualifications Standards for archaeology shall provide a Worker Environmental Awareness Program for the general contractor, subcontractors, and construction workers participating in ground-disturbing activity for project construction. The Worker Environmental Awareness Program training shall describe the potential of exposing archaeological resources, types of cultural materials that may be encountered, and directions on the steps that shall be taken if such a find is encountered. This training may be presented alongside other environmental training programs required prior to construction. A Worker Environmental Awareness Program acknowledgment form shall be signed by workers who receive the training.

CUL-2: Preparation of a Cultural Resources Mitigation and Monitoring Program

Prior to the start of any ground-disturbing activity for project construction, including but not limited to site clearing, grubbing, trenching, and excavation, a qualified archaeologist who meets or exceeds the Secretary of Interior's Professional Qualifications Standards for archaeology shall be retained to prepare a Cultural Resources Mitigation and Monitoring Program for unanticipated discoveries during project construction. The Cultural Resources Mitigation and Monitoring Program shall be prepared in consultation with Native American tribes who have participated in consultation for the project. The Cultural Resources Mitigation and Monitoring Program shall include provisions for archaeological, historic, and Native American monitoring of initial ground disturbance related to construction of the project, project construction schedule, procedures to be followed in the event of discovery of archaeological resources including additional artifacts or features associated with RIN-S-1, and protocols for Native American coordination and input, including review of documents. The Cultural Resources Mitigation and Monitoring Program shall outline the role and responsibilities of Native American monitor(s). It shall include communication protocols and opportunity and timelines for review of cultural resources documents related to discoveries that are Native American in origin. The Cultural Resources Mitigation and Monitoring Program shall include provisions for Native American monitoring during testing or data recovery efforts for unknown resources that are Native American in origin, shall any be encountered. Once completed, the Cultural Resources Mitigation and Monitoring Program shall be reviewed and approved by the Project Planner at the City of Santee prior to the start of any ground disturbing activities.

CUL-3: Cultural Resources Construction Monitoring

A qualified archaeologist who meets or exceeds the Secretary of Interior's Professional Qualifications Standards for archaeology shall be present during ground-disturbing activity for project construction, including but not limited to site clearing, grubbing, trenching, and excavation, for the duration of the project ground disturbance or until the qualified archaeologist determines monitoring is no longer necessary.

At the completion of monitoring, the qualified archaeologist ~~should~~shall prepare a Cultural Resources Monitoring Report to document the findings during the monitoring effort for the project. The report ~~should~~shall include the monitoring logs completed for the project and ~~should~~shall document any discoveries made during monitoring. The Cultural Resources Monitoring Report ~~should~~shall be submitted to the City of Santee and the South Coastal Information Center.

CUL-4: Native American Construction Monitoring

A minimum of one Native American monitor shall be present during ground-disturbing activity for project ground disturbance, including but not limited to site clearing, grubbing, trenching, and excavation, for the duration of the project ground disturbance or until the qualified archaeologist determines monitoring is no longer necessary. The Native American monitors shall be of Kumeyaay descent with ancestral ties to the San Diego region and at minimum one year of monitoring experience within Kumeyaay ancestral territory.

CUL-5: California Health and Safety Code Section 7050.5

During project construction, if human remains are found, the California Health and Safety Code Section 7050.5 states no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. Per the Public Resources Code, in the event of an unanticipated discovery of human remains, the County Coroner shall be notified immediately. If the human remains are determined to be prehistoric, the coroner shall notify the Native American Heritage Commission, which will determine and notify a Most Likely Descendant.

15.6 Energy

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Result in potentially significant environmental impacts due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Project Description, California Green Building Standards Code (CALGreen) and the California Energy Code (Title 24, Part 6 of the California Code of Regulations).

a. Less Than Significant Impact.

Energy use associated with a project typically includes fuel (gasoline and diesel), electricity, and natural gas, and sources include the following:

- Construction-related vehicle and equipment energy use.
- Transportation energy use from people traveling to and from the project area during operation.
- Building and facility energy use of the project during operation.

Construction-Related Energy Use

Energy use during construction would occur within two general categories: fuel use from vehicles used by workers commuting to and from the construction site, and fuel use by vehicles and other equipment to conduct construction activities. Fuel consumption associated with construction worker commute would be similar of any other typical commute in San Diego County and would not result in a wasteful, inefficient, or unnecessary consumption of gasoline or diesel fuel. Consistent with state requirements, all construction equipment would meet CARB Tier 3 In-Use Off-Road Diesel Engine Standards. Engines are required to meet certain emission standards, and groups of standards are referred to as Tiers. A Tier 0 engine is unregulated with no emission controls, and each progression of standard level (i.e., Tier 1, Tier 2, Tier 3, etc.) generates lower emissions, use less energy, and are more advanced technologically than the previous tier. CARB's Tier 3 In-Use Off-Road Diesel Engine Standards requires that construction equipment fleets become cleaner and use less energy over time. There are no known conditions in the project area that would require nonstandard equipment or construction practices that would increase fuel-energy consumption above typical equipment fuel consumption rates. Additionally, construction activities would be temporary and short-term (twelve months) and would adhere to all construction BMPs. Therefore, project construction would not result in the wasteful, inefficient, or unnecessary consumption of energy resources, and impacts would be less than significant.

Operation-Related Energy Use

During operation, energy use would be associated with transportation-related fuel use (gasoline, diesel fuel, and electric vehicles), and building-related energy use (electricity and natural gas).

Transportation-Related Energy Use

Buildout of the project and vehicle trips associated with the project would result in transportation energy use. Trips by individuals traveling to and from the project site would result from the use of passenger vehicles and work trucks. Vehicles would be mostly powered by gasoline, with some fueled by diesel or electricity. The two car dealerships and body shop would generate 2,436 daily trips and the 5,400-square-foot car wash would generate 900 daily trips for a total of 3,336 daily trips. Based on CalEEMod default trip lengths, the project would generate 3,298,000 vehicle miles travelled (VMT) annually.

Periodic deliveries would occur to bring new cars and other goods deliveries to the project site. Vehicle deliveries would occur after construction activities are completed and would be similar or less than the number of delivery trips required to deliver building materials to the site. However,

these trips would be minimal and would not result in an exceedance of daily emission thresholds or result in excessive energy consumption.

Project fuel consumption would decline over time beyond the initial operational year of the project as a result of continued implementation of increased federal and state vehicle efficiency standards. There is no component of the project that would result in unusually high vehicle fuel use during operation. Therefore, operation of the project would not create a land use pattern that would result in wasteful, inefficient, or unnecessary use of energy, and impacts would be less than significant.

Non-Transportation-Related Energy Use

Non-transportation energy use would be associated with electricity and natural gas. The Renewables Portfolio Standard (RPS) promotes diversification of the state's electricity supply and decreased reliance on fossil fuel energy sources. Renewable energy includes (but is not limited to) wind, solar, geothermal, small hydroelectric, biomass, anaerobic digestion, and landfill gas. Originally adopted in 2002 with a goal to achieve a 20 percent renewable energy mix by 2020 (referred to as the "Initial RPS"), the goal has been accelerated and increased by Executive Orders (EOs) S-14-08 and S-21-09 to a goal of 33 percent by 2020. In April 2011, Senate Bill (SB) 2 (1X) codified California's 33 percent RPS goal. SB 350 (2015) increased California's renewable energy mix goal to 50 percent by the year 2030. SB 100 (2018) further increased the standard set by SB 350 establishing the RPS goal of 44 percent by the end of 2024, 52 percent by the end of 2027, and 60 percent by 2030. Once operational, the project would be served by San Diego Gas & Electric (SDG&E). Based on the most recent annual report, SDG&E has already procured 39 percent (California Public Utilities Commission 2021) renewable energy and is on track to procure 60 percent by 2030 as outlined in SDG&E's 2019 RPS Procurement Plan.

The California Code of Regulations, Title 24, is referred to as the California Building Code (CBC). It consists of a compilation of several distinct standards and codes related to building construction, including plumbing, electrical, interior acoustics, energy efficiency, handicap accessibility, and so on. Of particular relevance to greenhouse gas (GHG) reductions are the CBC's energy efficiency and green building standards as outlined below.

Title 24, Part 11 of the California Code of Regulations is CALGreen. Beginning in 2011, CALGreen instituted mandatory minimum environmental performance standards for all ground-up new construction of commercial and low-rise residential buildings, state-owned buildings, schools, and hospitals. It also includes voluntary tiers (I and II) with stricter environmental performance standards for these same categories of residential and non-residential buildings. Local jurisdictions must enforce the minimum mandatory requirements and may adopt CALGreen with amendments for stricter requirements.

The project would, at a minimum, be required to comply with the mandatory measures included in the current 2022 Energy Code (California Code of Regulations, Title 24, Part 6) and the 2022 CALGreen standards, which went into effect January 1, 2023. The mandatory standards require the following:

- EV charging for new construction
- outdoor water use requirements as outlined in local water efficient landscaping ordinances or current Model Water Efficient Landscape Ordinance standards, whichever is more stringent;
- requirements for water conserving plumbing fixtures and fittings;
- 65 percent construction/demolition waste diverted from landfills;
- inspections of energy systems to ensure optimal working efficiency; and
- low-pollutant emitting exterior and interior finish materials such as paints, carpets, vinyl flooring, and particle boards.

Similar to the compliance reporting procedure for demonstrating Energy Code compliance in new buildings and major renovations, compliance with the CALGreen operational water reduction requirements must be demonstrated through completion of water use reporting forms for new low-rise residential and non-residential buildings. The water use compliance form must demonstrate a 20 percent reduction in indoor water use by either showing a 20 percent reduction in the overall baseline water use as identified in CALGreen or a reduced per-plumbing-fixture water use rate.

Electricity and natural gas service to the project site is provided by SDG&E. Once operational, the project would use electricity and natural gas to run various appliances and equipment, including space and water heaters, air conditioners, ventilation equipment, lights, and numerous other devices. Generally, electricity use is higher in the warmer months due to increased air conditioning needs, and natural gas use is highest when the weather is colder as a result of high heating demand. As a part of the air quality modeling prepared for the project (see Appendix A), CalEEMod was used to estimate the total operational electricity and natural gas consumption associated with the project. Table 4 summarizes the anticipated operational energy and natural gas use.

Table 4 Operational Electricity and Natural Gas Use	
	Total Use
Electricity	881,954 kWh/Year
Natural Gas	1,043,460 BTU/Year
kWh = kilowatt hour; BTU = British thermal units	

Buildout of the project would result in an increase of operational electricity and natural gas usage when compared to the existing condition. The project would be required to meet the mandatory energy requirements of 2022 CALGreen and the California Energy Code (Title 24, Part 6 of the California Code of Regulations) and would benefit from the efficiencies associated with these regulations as they relate to building heating, ventilating, and air conditioning mechanical systems, water-heating systems, and lighting. Additionally, the project would implement all applicable GHG reduction measures related to energy efficiency and clean energy as required by the City's

Sustainable Santee Plan (see Section 15.8). These measures include increasing energy efficiency through CALGreen mandatory and voluntary requirements, decreasing energy demand through reducing the urban heat island effect, and installing a solar photovoltaic system. Therefore, there are no project features that would support the use of excessive amounts of energy or would create unnecessary energy waste, or conflict with any adopted plan for renewable energy efficiency, and impacts would be less than significant.

b. Less Than Significant Impact. The applicable state plans that address renewable energy and energy efficiency are CALGreen, the California Energy Code, and RPS, and the applicable local plan is the Sustainable Santee Plan. As discussed in Section 15.6.a, the project would be required to meet the mandatory energy requirements of 2022 CALGreen and the 2022 California Energy Code. The project would not conflict with or obstruct implementation of CALGreen and the California Energy Code, or with SDG&E's implementation of RPS. Additionally, as detailed in Section 15.8 below, the project would be consistent with the Sustainable Santee Plan. Therefore, the project would not conflict with or obstruct a state or local plan for renewable energy or energy efficiency, and impacts would be less than significant.

15.7 Geology and Soils

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:				
(i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Source(s): Geotechnical Investigation prepared by GEOCON Incorporated, 2023 (Appendix F).

a(i). Less than Significant Impact. The project site is not located within a State of California Alquist-Priolo fault zone and there are no known faults that traverse the project site. Consequently, the risk from fault rupture is low. Therefore, the project would not expose people or structures to rupture of a known earthquake fault, and impacts would be less than significant.

a(ii). Less than Significant Impact. The project site is located in the seismically active southern California region, and therefore could be affected by seismic activity. However, the project would adhere to the City’s grading guidelines and seismic design parameters of the 2022 CBC. These site preparation activities would remove any soils that would be seismically unstable. The project would also adhere to all other geotechnical recommendations provided in the Geologic Investigation (see Appendix F) related to seismic safety. Therefore, the project would not expose people or structures to strong seismic shaking, and impacts would be less than significant.

a(iii). Less than Significant Impact. Liquefaction typically occurs when a site is in a zone with seismic activity, on-site soils are cohesionless or silt/clay with low plasticity, groundwater is encountered, and soil relative densities are less than about 70 percent. If these four criteria are met, a seismic event could result in a rapid pore-water pressure increase from the earthquake-generated ground accelerations. Seismically induced settlement may occur whether the potential for liquefaction exists or not. Due to the lack of a near surface groundwater table and the dense nature of the proposed

fill and underlying older alluvium and granitic rock, the potential for liquefaction and seismically induced settlement occurring at the site is considered negligible. Therefore, the project would not expose people or structures to seismic-related ground failure, including liquefaction, and impacts would be less than significant.

a(iv). No Impact. The project site and surrounding area are relatively flat and do not possess any slopes that could generate a landslide. Therefore, the project would not expose people or structures to adverse effects related to landslides. No impact would occur.

b. Less than Significant Impact. Prior to construction, the project applicant shall prepare a site-specific stormwater pollution prevention plan (SWPPP) consistent with the State Water Resources Control Board (SWRCB) Construction General Permit as a condition of approval. The SWPPP shall describe BMPs to be used during construction to prevent discharge of sediment and other pollutants in storm water runoff from the project site. Typical construction BMPs include silt fencing, fiber rolls, and sweeping. Specific BMPs would be determined by the project contractor and engineer based on site-specific conditions. As part of the project, the contractor will monitor the construction BMPs, including conducting routine inspections of disturbed areas to ensure that the BMPs remain intact and effective. Adherence to these BMPs would ensure that the project would not result in substantial soil erosion or loss of topsoil, and impacts would be less than significant.

c. Less than Significant Impact. Site preparation activities would remove any soils that would be seismically unstable. As a condition of project approval, the project would also adhere to all other geotechnical design recommendations provided in the Geotechnical Investigation (see Appendix F) related to seismic safety, as well as the seismic design parameters of the 2022 CBC. Therefore, the project would not be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and impacts would be less than significant.

d. Less than Significant Impact. As stated in the Geotechnical Investigation, the soil encountered in the field investigation was found to have "very low" to "low" expansion potential. Site preparation activities would remove any soils that would be seismically unstable. In addition, the project would adhere to all other geotechnical design recommendations provided in the Geotechnical Investigation (see Appendix F) related to seismic safety, as well as the seismic design parameters of the 2022 CBC. Therefore, the project would not be located on expansive soil, and impacts would be less than significant.

e. No Impact. The project would connect to the Padre Dam Municipal Water District (PDMWD) sewer system and would not utilize a septic tank or alternative wastewater disposal system. No impact would occur.

f. Less than Significant Impact. The project site is located within the Coastal Plain Region of the Peninsular Range Province. The Geotechnical Investigation determined that the project site is underlain by undocumented fill, topsoil, young alluvium, older alluvium, and granite rock. Review of the County of San Diego, Guidelines for Determining Significance, Paleontological Resources determined that these soils types have not been assigned moderate or high paleontological sensitivity rating. Consequently, it is unlikely that paleontological resources would be located beneath the project site. Therefore, the project would not directly or indirectly destroy a unique paleontological resource or site or unique geologic feature, and impacts would be less than significant.

15.8 Greenhouse Gas Emissions

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Climate Change Scoping Plan (CARB 2008); CARB 2017 Scoping Plan; CARB 2022 Scoping Plan Update; Sustainable Santee Plan Project Consistency Checklist (Appendix G); and Sustainable Santee Plan (LSA 2019).

a. Less than Significant Impact.

The City adopted the Sustainable Santee Plan on January 8, 2020, which provides guidance for the reduction of GHG emissions within the city. The Sustainable Santee Plan provides policy direction and identifies actions the City and community will take to reduce GHG emissions consistent with State goals and targets. State GHG emissions reduction targets proposed and/or codified by EO S-3-05, Assembly Bill (AB) 32, EO B-30-15, and SB 32 include achieving 1990 emission levels by 2020 (which the state has achieved); 40 percent below 1990 levels by 2030; and 80 percent below 1990 levels by 2050. The Sustainable Santee Plan would also work to achieve a per-capita GHG emission level by 2030 in conformance with SB 32 and the CARB 2017 Scoping Plan.

The Sustainable Santee Plan Project Consistency Checklist (Checklist) is intended to be a tool for development projects to demonstrate consistency with the Sustainable Santee Plan, which is a qualified GHG emissions reduction plan in accordance with CEQA Guidelines Section 15183.5. The Checklist has been developed as part of the Sustainable Santee Plan implementation and monitoring process and supports the achievement of individual GHG reduction measures as well as the City’s overall GHG reduction goals. Additionally, the Checklist supports the City’s sustainability goals and policies that encourage sustainable development and aim to conserve and reduce the consumption of resources, such as energy and water, among others. Projects that meet the requirements of the Checklist are considered consistent with the Sustainable Santee Plan and would have a less than significant contribution to cumulative GHG impacts (i.e., the project’s incremental contribution to cumulative GHG effects is not cumulatively considerable), pursuant to CEQA Guidelines Sections 15064(h)(3), 15130(d), and 15183(b).

The project-specific Checklist is included in Appendix G. The project would be consistent with the existing General Commercial (GC) General Plan and land use zoning designations, and therefore

would be consistent with the land use assumptions used in the Sustainable Santee Plan. As demonstrated in the Checklist, the project would implement all applicable GHG reduction measures related to energy efficiency, solid waste, and clean energy required by the City's Sustainable Santee Plan. Specifically, the project would be consistent with the following goals:

- Increase Energy Efficiency (Goal 4): The project would implement all feasible and applicable CALGreen Tier 2 Building Standards. The CALGreen Tier 2 measures that would be implemented by the project are related to planning and design, energy efficiency, water efficiency and conservation, material conservation and resource efficiency, and environmental quality. Refer to the Checklist in Appendix G.
- Decrease Energy Demand through Reducing Urban Heat Island Effect (Goal 5): To achieve this goal, projects are required to utilize tree planting for shade and energy efficiency, and to use light-reflecting surfaces. The project would include landscaping along project frontages and throughout the project site to provide shade. Additionally, the project would reduce energy demand by constructing cool roofs.
- Decrease GHG Emissions through a Reduction in VMT (Goal 6): Access to the project site would be provided via two driveways on Cottonwood Avenue, three driveways on Mission Gorge Road, and one driveway on Railroad Avenue. Existing bus stops are located immediately adjacent to the project site along Mission Gorge Road. The bus stops are served by the San Diego Metropolitan Transit System (MTS) bus route 833 that runs along Magnolia Avenue and Mission Gorge Road to the Santee Town Center, which is served by the Sycuan Green Line Trolley. The project would improve pedestrian connectivity by improving the sidewalk and providing landscaping along the northern project boundary. The project would provide a total of 688 parking spaces. Additionally, Mission Gorge Road is identified as a Reduced Speed Class 2 bike lane in the Santee Bicycle Master Plan. The project would include four, two-capacity bicycle racks.
- Electric Vehicles (Goal 7): The project would include 24 EV spaces in accordance with CALGreen.
- Solid Waste (Goal 9): The project would reduce waste at landfills by providing on-site recycling storage per CALGreen and the Santee Municipal Code.
- Clean Energy (Goal 10): To achieve this goal, projects are required to install photovoltaic solar systems. The project would include rooftop solar panels. In addition, the project would include 33 clean air vehicle parking spaces.

Based on the project's consistency with the City's Sustainable Santee Plan demonstrated in the Checklist, the project's contribution of GHGs to cumulative statewide emissions would be less than cumulatively considerable. Therefore, the project would not generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment, and impacts would be less than significant.

b. Less than Significant Impact.

As described in Section 15.8.a above, the project would be consistent with the existing General Plan and land use zoning designations, and therefore would be consistent with the land use assumptions used in the Sustainable Santee Plan. As demonstrated in the Checklist, the project would implement all applicable GHG reduction measures related to energy efficiency, solid waste, and clean energy required by the City’s Sustainable Santee Plan.

AB 1279, the California Climate Crisis Act, codified the carbon neutrality target as 85 percent below 1990 levels by 2045. The 2022 Scoping Plan lays out a path to achieve targets for carbon neutrality and reduce anthropogenic GHG emissions by 85 percent below 1990 levels no later than 2045, as directed by AB 1279. Appendix D of the 2022 Scoping Plan includes local actions that jurisdictions may take to reduce GHG emissions in line with AB 1279 goals. The three key priority areas identified in the 2022 Scoping Plan are (1) transportation electrification, (2) VMT reduction, and (3) building decarbonization. The project would support transportation electrification by installing EV parking spaces in accordance with CALGreen. Additionally, the project would have less than significant VMT impacts (see Section 15.17.b). Projects located within a half-mile radius of an existing major transit stop or an existing stop along a high-quality transit corridor may be presumed to have a less-than-significant impact per the City’s VMT screening criteria. Bus routes serving the immediate project area include MTS Routes 832, 833, and 834. The bus stop closest to the project site is less than 300 feet on Mission Gorge Road along the project frontage. In addition, a portion of the project is located within the Transit Priority Area. The project would therefore reduce VMT. Lastly, the project would implement all feasible and applicable CALGreen Tier 2 Building Standards. The CALGreen Tier 2 measures that would be implemented by the project are related to planning and design, energy efficiency, water efficiency and conservation, material conservation and resource efficiency, and environmental quality.

The project would be consistent with the Sustainable Santee Plan and would support the three key priority areas identified in the 2022 Scoping Plan. Therefore, the project would not conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of GHGs, and impacts would be less than significant.

15.9 Hazards and Hazardous Materials

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Create a significant hazard to the public or the environment through routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Project Description, City of Santee General Plan–Safety Element; California Department of Toxic Substances Control–EnviroStor Database; State Water Resources Control Board–Geotracker Database (2022); Gillespie Field Airport Land Use Compatibility Plan (ALUCP; Airport Land Use Commission [ALUC] 2010); Santee Municipal Code (Chapter 15.20.040); California Department of Forestry and Fire Protection (<https://egis.fire.ca.gov/FHSZ/>)

a. Less than Significant Impact. The project site is bound by commercial uses to the north, multi-family residential uses to the east, single-family residences to the south, and multi-family residential uses to the west. The project would not place housing near any hazardous materials facilities. The routine use, transport, or disposal of hazardous materials is primarily associated with industrial uses, which require such materials for manufacturing operations or produce hazardous wastes as by-products of production applications. The project proposes the construction of a 33,974-square-foot auto sales and service building, a 2,549-square-foot detail bay, a second 33,112-square-foot auto sales and service building, a 5,400-square-foot self-service car wash, and a 16,405-square-foot body shop. The project does not propose or facilitate any activity involving significant use, routine transport, or disposal of hazardous substances.

Construction of the project would require the use and transport of hazardous materials such as asphalt, paints, chemicals to break down the larger boulders, and other solvents. The use and handling of these materials would follow all applicable federal, state, and local regulations, including California Occupational Safety and Health Administration, California Department of Transportation (Caltrans), and the California Department of Environmental Health Hazardous Materials Division. All hazardous materials are required to be utilized and transported in accordance with their labeling pursuant to federal and state law. Routine construction practices include good housekeeping measures to prevent/contain/clean-up spills and contamination from fuels, solvents, concrete wastes, and other waste materials.

The operation of the project may involve the use of paints, other solvents, and cleaners. Cleaning solutions would be used for daily operation and paints would be used in the body shop and for routine maintenance and re-coating of structures. The remnants of these and other products are disposed of as household hazardous waste that includes lead-acid battery waste, electronic wastes, and other wastes that are prohibited or discouraged from being disposed of at local landfills. The project would be required to adhere to regulations addressing used lead-acid battery management found in California Code of Regulations, Title 22, Sections 66266.80 and 66266.81. Therefore, the project would not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials, and impacts would be less than significant.

b. Less than Significant Impact. As described in Section 15.8.a above, construction and operation of the project would involve the use of asphalt, paints, chemicals to break down the larger boulders, and other solvents. The project would be designed and constructed consistent with applicable safety regulations that would prevent the introduction of accident conditions. Therefore, the project would not create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment, and impacts would be less than significant.

c. No Impact. The project site is not located within one-quarter mile of any schools. The nearest school is Faith Trolley Preschool, located approximately 0.5 mile west of the project site. The project would not result in hazardous emissions or include the handling of acutely hazardous materials, substances, or waste. No impact would occur.

d. Less than Significant Impact. The State Water Resources Control Board GeoTracker database identified two active cleanup sites adjacent to the project located at 10460 Mission Gorge Road and 8888 Magnolia Avenue. The Department of Toxic Substances Control Envirostor Database identified

one active cleanup site adjacent to the project located at 10438 Mission Gorge Road. No cleanup sites were identified within the project boundaries and no permanent structures are located on the project site. Therefore, the project is not located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5, and impacts would be less than significant.

e. Less than Significant Impact.

The project site is within Airport Influence Area 1 of the Gillespie Field Airport, which is approximately 0.5 mile from the project site. On January 18, 2023, the project received a Determination of No Hazard to Air Navigation from the Federal Aviation Administration based on the placement and maximum height (33 feet) of the proposed buildings. Portions of the project site area are also within Safety Zones 2, 3, and 4 of the Gillespie Field ALUCP. Each Airport Safety Zone serves as a guideline for the maximum density of people that should be located within the Safety Zone based on the level of risk of an aircraft accident within the respective Safety Zone. Safety Zone 2, the Inner Approach/Departure Zone, has a higher risk and as such it is recommended that developments within this Safety Zone limit density to 70 persons per acre without risk reduction measures (per ALUCP page 3-48 Table III-2). Risk reduction measures include hardening the buildings and/or incorporating increased fire protection systems. The project does not propose risk reduction measures. Safety Zones 3 and 4, respectively the Inner Turning Zone and Outer Approach/Departure Zone, are at a lower risk for aircraft accidents and as such their recommended maximum density limit is 130 persons per acre without risk reduction measures (per ALUCP page 3-48 Table III-2).

Under the ALUCP (page 3-48, Table III-2), the proposed use would be classified as "Low-Intensity or Outdoor-Oriented Retail or Wholesale Trade: furniture, automobiles, heavy equipment, nurseries, lumber yards, boat yards [approximately 250 square feet per person]." Such a classification assumes the use would generate an estimated one person for every 250 square feet of proposed building floor area. A 6.9-acre portion of the site is within Safety Zone 2. Within this portion of the site, a total building floor area of 69,635 square feet is proposed. Based on the ALUCP, this building square footage would yield an estimated 279 persons on this portion of the site (one person per acre recommended maximum), resulting in a density of 40 persons per acre, which is compatible with Airport Safety Zone 2 (70 persons per acre). A 2.72-acre portion of the site is within Safety Zone 3, and within this portion of the site, a total building floor area 21,805 square feet is proposed. This building square footage would yield an estimated 87 persons on this portion of the site (one person per 250 square feet), resulting in a density of 32 persons per acre, which is compatible with Safety Zone 3 (130 persons/acre recommended maximum). The remainder of the site would be within Safety Zone 4, and no buildings are proposed within this portion of the site. As a whole, the project would be compatible with the ALUCP. As an entitlement condition of approval, the project is also required to obtain an ALUCP consistency determination from the San Diego County Airport Land Use Commission. Therefore, impacts related to an ALUCP would be less than significant.

f. Less than Significant Impact. The project site is located in an existing developed area with access to major roadways that would allow for emergency evacuation. The project would comply with all design recommendations and requirements for construction and operations as provided by the Santee Fire Department to ensure that emergency access meets City standards. In addition, the project would be consistent with the existing General Commercial (GC) district. Therefore, the project

would not impair implementation of, or physically interfere with emergency response, and impacts would be less than significant.

g. Less than Significant Impact. Wildland fires present a significant threat in the city, particularly in the summer months when temperatures are high and precipitation is limited. Areas in the city that are particularly susceptible to fires are designated as “very high hazard” or “high hazard” areas and are delineated on the Very High Fire Hazard Severity Zones for Local Responsibility Areas as recommended by the California Department of Forestry and Fire Protection. The project site is not located within land mapped as a fire hazard severity zone. Similarly, the project site is not located within a Wildland Urban Interface area. Additionally, the project would install fire prevention features consistent with comments provided by the Santee Fire Department, including an automatic fire sprinkler system. Therefore, the project would not expose people or structures to a significant risk of loss, injury or death involving wildland fires, and impacts would be less than significant.

15.10 Hydrology and Water Quality

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or through the addition of impervious surfaces in a manner, which would:				
i. result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Drainage Study prepared by Rick Engineering, 2023 (Appendix H); Storm Water Quality Management Plan, 2023 (Appendix I); Padre Dam Municipal Water District 2020 Urban Water Management Plan; City of Santee General Plan Safety Element.

a. Less than Significant Impact.

Pre-Project Condition

The pre-project condition drains generally northwest to a single point of compliance. The point of compliance consists of a curb inlet located along the eastern edge of Cottonwood Avenue, just south of the intersection of Cottonwood Avenue and Mission Gorge Road. Runoff from the southern portion of the project site flows from the eastern boundary westward along Happy Lane before it rounds the corner at Cottonwood Avenue and flows north along the street where it is collected by the curb inlet. Runoff from the northern portion of the project site flows north to Mission Gorge Road, then concentrates and flows west along the street until it rounds the corner and travels south at Cottonwood Avenue where it is collected by the curb inlet. After entering the curb inlet, runoff from the project site drains to the San Diego River, which flows approximately west and ultimately to the Pacific Ocean.

Post-Project Condition

Drainage patterns in the post-project condition would be similar to the pre-project condition. In the post-project condition, the project area is divided into three lots, each of which would drain to a separate underground storage vault and proprietary compact biofiltration system. It is anticipated that peak flows during a 100-year, six-hour storm event on the post-project site would be the same as peak flows in the pre-project condition. After exiting the vaults and compact biofiltration system, stormwater would be conveyed via a private storm drain to a connection with the public storm drain

system located at the curb inlet at the southeast corner of the intersection of Cottonwood Avenue and Mission Gorge Road. After connecting to the public storm drain system, flows from the site would be conveyed to the San Diego River.

Post-project storm water runoff would be managed via underground storage vaults and proprietary compact biofiltration BMPs, designed pursuant to the guidelines from the City's BMP Design Manual, dated February 2016. The "Priority Development Project Storm Water Quality Management Plan" specific to the project is dated December 16th, 2023, and prepared by Rick Engineering Company (see Appendix I). Implementation of the underground storage vaults and proprietary compact biofiltration BMPs would result in the same peak flows from the pre-project condition. Therefore, the project would not violate any water quality standards or waste discharge requirements, and impacts would be less than significant.

b. Less than Significant Impact. The project would obtain its water supply from the PDMWD and would not use groundwater supply for any purpose. Additionally, the proposed land uses would not be associated with activities known to degrade groundwater. The project would increase the amount of impermeable surfaces on-site from 3.23 acres to 14.2 acres. However, water would continue to infiltrate through the 3.5 acres of the post-construction development footprint that would remain pervious. Furthermore, water would continue to infiltrate through undeveloped land throughout the groundwater basin. Therefore, the project would not substantially decrease groundwater supplies or interfere with groundwater recharge, and impacts would be less than significant.

c(i). Less than Significant Impact. Prior to construction, the project applicant shall prepare a site-specific SWPPP consistent with the SWRCB Construction General Permit as a condition of approval. The SWPPP shall describe BMPs to be used during construction to prevent discharge of sediment and other pollutants in storm water runoff from the project site. Typical construction BMPs include silt fencing, fiber rolls, and sweeping. Specific BMPs would be determined by the project contractor and engineer based on site-specific conditions. As part of the project, the contractor would monitor the construction BMPs, including conducting routine inspections of disturbed areas to ensure that the BMPs remain intact and effective. Adherence to these BMPs would ensure that project construction would not result in substantial soil erosion, and impacts would be less than significant.

c(ii). Less than Significant Impact. As described in Section 15.10.a, post-project storm water runoff would be managed via underground storage vaults and proprietary compact biofiltration BMPs that would reduce peak flows during a 100-year, six-hour storm event to less than or equal to pre-project flows. Review of Figure 8-1 of the General Plan Safety Element determined that the project site is not located within the 100-year floodplain. Therefore, the project would not substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site, and impacts would be less than significant.

c(iii). Less than Significant Impact. As described in Section 15.10.a, post-project storm water runoff would be managed via underground storage vaults and proprietary compact biofiltration BMPs that would reduce peak flows during a 100-year, six-hour storm event to less than or equal to pre-project flows. Therefore, project runoff would not exceed the capacity of storm water drainage systems and would not provide substantial sources of polluted runoff, and impacts would be less than significant.

c(iv). Less than Significant Impact. Review of Figure 8-1 of the General Plan Safety Element determined that the project site is not located within the 100-year floodplain. As described in Section 15.10.a, post-project storm water runoff would be managed via underground storage vaults and proprietary compact biofiltration BMPs that would reduce peak flows during a 100-year, six-hour storm event to less than or equal to pre-project flows. Therefore, the project would not impede or redirect flood flows, and impacts would be less than significant.

d. Less than Significant Impact. The project site, along with the rest of the city, is located in the San Diego River valley. Review of Figure 8-1 of the General Plan Safety Element determined that the project site is not located within the 100-year floodplain. The project site is relatively flat and would not be subject to inundation by mudflow. Review of Figure 8-2 of the General Plan Safety Element determined that the project site is located within the El Capitan Reservoir and San Vicente Reservoir Inundation Areas. However, the project does not possess any features that would exacerbate risk associated with flood inundation beyond what exists for surrounding land uses in the existing condition. The nearest enclosed body of water is Santee Lakes, located approximately 2.2 miles northwest of the project site, is not located in an area where seismically induced seiches are considered a potential hazard. Additionally, the project site is located approximately 20 miles inland from the Pacific Ocean, and therefore is not subject to risk associated with tsunamis. Therefore, project impacts regarding risk of release of pollutants due to project inundation associated with flood hazards, tsunamis, or seiche zones would be less than significant.

e. Less than Significant Impact. The project would not require the use of, or otherwise substantially interfere with, groundwater supplies or recharge compared to existing conditions. The project would not involve any use of groundwater and would obtain its water supply from the PDMWD, which obtains its water from surface reservoirs and imported sources. Padre Dam's Urban Water Management Plan addresses the PDMWD water system. Potable demands under normal conditions are anticipated to increase to 15,944 acre-feet per year by the year 2045. The project uses are permitted within the existing general plan and zoning designations for the property, and therefore were accounted for in Padre Dam's Urban Water Management Plan. Due to the increase in on-site impervious surfaces, the area available for groundwater recharge would decrease. Water would continue to infiltrate through 3.5 acres of the post-construction development footprint that would remain pervious. In addition, the project would comply with applicable groundwater regulations and BMPs. As a result, the project would not conflict with a water quality control plan or sustainable groundwater management plan and impacts would be less than significant.

15.11 Land Use and Planning

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Project Description; City of Santee General Plan–Land Use Element.

a. No Impact. The project proposes the construction of two auto dealerships with sales and service buildings, a detail bay, a self-service car wash, and a body shop. Land uses surrounding the project site include commercial uses to the north, multi-family residential uses to the east, single-family residences to the south, and multi-family residential uses to the west. The project would be constructed entirely within the boundaries of the project site and would not affect any of the surrounding properties or land use pattern. The project would also be consistent with the existing General Commercial (GC) zone that exists on the property and would not create any significant impacts relative to the surrounding residentially zoned lands. Implementation of the project would not create any new land use barriers or otherwise divide or disrupt the physical arrangement of the surrounding established community. Therefore, the project would not physically divide an established community. No impact would occur.

b. Less than Significant Impact with Mitigation. The project site is zoned General Commercial (GC) and is designated General Commercial (GC) within the City’s General Plan. The project proposes the construction of two auto dealerships with sales and service building, a detail bay, a self-service car wash, and a body shop. The proposed uses are permitted within the existing general plan and zoning designations for the property. Further, the project is consistent with the General Plan policies found in Table 5.

Table 5 Project Consistency with General Plan	
Policies	Project Consistency
Policy 1.2 The City shall utilize noise studies and noise contour maps when evaluating development proposals during the discretionary review process.	A Noise Analysis was prepared by RECON Environmental, Inc. for the project (Appendix J). The Noise Analysis includes noise contour maps.
Policy 1.6 The City shall continue to monitor noise throughout Santee and enforce the standards and regulations of the City's Noise Ordinance.	As stated in Section 15.13 of this MND, project construction and operational noise would be in compliance with the Santee Municipal Code. Mitigation measures NOISE-1 and NOISE-2 would enforce project compliance with the standards and regulations of the City's Noise Ordinance.
Policy 4.1 The City should ensure that all residential areas are adequately provided for in terms of day-to-day shopping needs which include convenience goods, food and personal services.	The project would construct two auto dealerships with sales and service buildings, a detail bay, a self-service car wash, and a body shop. The project would provide convenient day-to-day services for the local residential areas.
Policy 6.2 The City should promote the use of innovative site planning to avoid on-site hazards and minimize risk levels.	The City has worked with the project applicant to design a project that avoids on-site hazards.
Policy 7.1 The City should review all development proposed within the Gillespie Field Airport Influence Area to ensure that design features are incorporated into the site plan to address identified aircraft safety and noise hazards.	As stated in Section 15.9 and 15.13 of this MND, the project has been reviewed for aircraft safety and noise hazards. As a whole, the project would be compatible with the ALUCP. As an entitlement condition of approval, the project is also required to obtain an ALUCP consistency determination from the San Diego County Airport Land Use Commission. The noise analysis concluded that with mitigation, noise impacts from construction and operation of the project would be less than significant.
Policy 7.2 The City shall require that all development proposals provide appropriate mitigation for identified significant biological resources including selective preservation, sensitive site planning techniques and in-kind mitigation for identified impacts.	As stated in Section 15.4 of this MND, if vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30), mitigation measure BIO-1 would require a pre-construction clearance survey for nesting birds and raptors.

A portion of the project site is located within the Transit Priority Area. SB 743 (Steinberg 2013) provides for streamlined environmental review for projects within Transit Priority Areas, which is an area within a half mile of a “major transit stop.” A major transit stop, as defined in Public Resources Code 21064.3, is a site that contains any of the following:

- An existing rail or bus rapid transit station.
- A ferry terminal served by either a bus or rail transit service.
- The intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods.

Bus routes serving the immediate project area include MTS Routes 832, 833, and 834. The bus stop closest to the project site is less than 300 feet on Mission Gorge Road along the project frontage.

As described in Section 15.8.a above, the project would be consistent with the Sustainable Santee Plan (see Appendix G). As described in Section 15.9.e above, the project would be compatible with the ALUCP. As an entitlement condition of approval, the project is also required to obtain an ALUCP consistency determination from the San Diego County Airport Land Use Commission. Therefore, the project would not result in a significant environmental impact due to a conflict with any land use plan, policy, or regulation and impacts would be less than significant.

15.12 Mineral Resources

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Source: City of Santee General Plan–Conservation Element.

a. No Impact. The Conservation Element of the General Plan documents that known mineral resources within Santee include sand, gravel, and crushed rock, which are collectively referred to as aggregate. These resources have been identified within the floodplain of the San Diego River. As identified in the California Department of Conservation Mineral Lands Classification Map (1996), the project site is classified as MRZ-3, which is defined as areas that contain known mineral deposits that could qualify as mineral resources. The project site is surrounded by commercial, residential, and roadway uses that would preclude the type of extraction operations typically associated with aggregate minerals (i.e., large-scale pits or quarries). Therefore, extraction of mineral resources is not a viable use of the site. No impact would occur.

b. No Impact. See response to 15.12.a. The project site is not delineated as a mineral resource recovery area on any land use plans. No impact would occur.

15.13 Noise

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Generation of excessive ground borne vibration or ground borne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. For a project located within the vicinity of a private airstrip or an airport land use plan, or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: City of Santee General Plan–Noise Element; Santee Municipal Code; Technical Noise Supplement (Caltrans 2013a); Transportation and Construction Vibration Guidance Manual (Caltrans 2013b); Roadway Construction Noise Model (Federal Highway Administration 2006); Transit Noise and Vibration Impact Assessment Manual (Federal Transit Administration 2018); Gillespie Field Airport Land Use Compatibility Plan (ALUC 2010); and Noise Analysis prepared by RECON Environmental, Inc. (see Appendix J).

a. Less than Significant with Mitigation. Noise is defined as sound that is loud, unpleasant, unexpected, or undesired and, therefore, may cause general annoyance, interference with speech communication, sleep disturbance, and, in the extreme, hearing impairment. Decibels (dB) are the standard unit of measurement of the sound pressure generated by noise sources and are measured on a logarithmic scale that quantifies sound intensity in a manner similar to the Richter scale for earthquake magnitudes. A doubling of the energy of a noise source, such as doubling of traffic volume, would increase the noise level by 3 dB; a halving of the noise energy would result in a 3 dB decrease.

The human ear is not equally sensitive to all frequencies within the sound spectrum. To accommodate this phenomenon, the A-weighted scale, which approximates the frequency response of the average young ear when listening to most ordinary everyday sounds, was devised. Noise levels using A-weighted measurements are written as dB(A). It is widely accepted that the average healthy ear can barely perceive changes of 3 dB(A) (increase or decrease) and that a change of 5 dB(A) is readily perceptible. An increase of 10 dB(A) is perceived as twice as loud, and a decrease of 10 dB(A) is perceived as half as loud (Caltrans 2013a).

The impact of noise is not a function of loudness alone. The time of day when noise occurs and the duration of the noise are also important. In addition, most noise that lasts for more than a few seconds is variable in its intensity. Consequently, a variety of noise descriptors has been developed. The noise descriptors used for this study are the equivalent noise level (L_{eq}) and the maximum noise level (L_{max}).

The L_{eq} is the equivalent steady-state noise level in a stated period of time that is calculated by averaging the acoustic energy over a time period; when no period is specified, a 1-hour period is assumed. The maximum noise level is the highest sound level occurring during a specific period.

Existing Noise Measurements

Existing noise levels at the project site were measured on July 21, 2022, using one Larson-Davis LxT Sound Expert Sound Level Meters, serial number 3897. The following parameters were used:

Filter:	A-weighted
Response:	Slow
Time History Period:	5 seconds

The meter was calibrated before and after the measurements. The meter was set 5 feet above the ground level for each measurement, which is the average height of the human ear.

Noise measurements were taken to obtain typical ambient noise levels at the project site and in the vicinity. The weather was warm and sunny with a slight breeze. Four 15-minute measurements were taken, as described below. The measurement locations are shown on Figure 5, and detailed data is contained in Appendix J.

Measurement 1 was located at the northern project boundary, approximately 50 feet south of Mission Gorge Road. The main source of noise at this location was vehicle traffic on Mission Gorge Road. Secondary sources of noise included aircraft flyovers from Gillespie Field. During the 15-minute measurement period, vehicle traffic on Mission Gorge Road was counted. The average measured noise level was 60.3 dB(A) L_{eq} .





-  Project Boundary
-  Noise Measurement Location



FIGURE 5
Noise Measurement Locations

Measurement 2 was located at the western project boundary, approximately 50 feet east of Cottonwood Avenue. The main source of noise at this location was vehicle traffic on Mission Gorge Road and Cottonwood Avenue. During the 15-minute measurement period, vehicle traffic on Cottonwood Avenue was counted. The average measured noise level was 58.8 dB(A) L_{eq} .

Measurement 3 was located at the southern portion of the project site, approximately 10 feet from the southern project boundary. The main source of noise at this location was vehicle traffic on Mission Gorge Road. Secondary sources of noise included barking dogs. The average measured noise level was 48.1 dB(A) L_{eq} .

Measurement 4 was located at the eastern portion of the project site, approximately 10 feet from the eastern project boundary. The main source of noise at this location was vehicle traffic on Mission Gorge Road. Secondary sources of noise included aircraft flyovers and barking dogs. The average measured noise level was 56.6 dB(A) L_{eq} .

Construction Noise

Noise level limits for construction activities are established in Section 5.04.090 of the Santee Municipal Code. These limits state that notice must be provided to all owners and occupants within 300 feet of the project site if the construction equipment has a manufacturer's noise rating of 85 dB and operates at a specific location for 10 consecutive workdays.

In addition, Section 5.04.090 of the Santee Municipal Code states that no construction equipment is permitted before 7:00 a.m. or after 7:00 p.m. on Mondays through Saturdays and at all times on Sundays and holidays.

Project construction noise would be generated by diesel engine-driven construction equipment used for site preparation and grading, rock processing, building construction, loading, unloading, and placing materials and paving. Diesel engine-driven trucks also would bring materials to the site and remove the soil from excavation.

There are large boulders and bedrock located in the southeast portion of the project site. At the beginning of project construction, prior to site grading, rock breaking and crushing activities would also be required. It is anticipated that these activities would last approximately two months. First, the larger boulders would be drilled with a rock drill and chemicals would be used to break them down to manageable sizes. A rock drill would generate an average hourly noise level of 78 dB(A) L_{eq} at 50 feet. Then, an excavator with a mounted 10,000-pound hydraulic hammer/breaker would break those rock pieces down to two-foot diameter or less fragments. An excavator with a mounted impact hammer would generate a combined noise level of 85 dB(A) L_{eq} at 50 feet. These smaller rocks would then be hauled off-site or crushed on-site. As a worst-case noise analysis, a rock crusher was modeled on the project site. Based on noise measurements taken at a temporary rock crushing operation, the rock crusher would generate a noise level of 88 dB(A) L_{eq} at 50 feet. This noise level would attenuate to 75 dB(A) L_{eq} at a distance of 165 feet. Therefore, as a noise reduction feature, the rock crusher was modeled at a distance of more than 165 from all adjacent residential property lines (refer to mitigation measure NOISE-1). A loader may also be required to transfer rock to the crusher. A loader generates a noise level of 76 dB(A) L_{eq} at 50 feet.

During the remaining excavation, grading, and paving operations, equipment moves to different locations and goes through varying load cycles (i.e., amount of time that equipment operates at full power), and there are breaks for the operators and for non-equipment tasks, such as measurement. Although maximum noise levels may be 70 to 95 dB(A) at a distance of 50 feet during most construction activities, hourly average noise levels from the grading phase of construction would be 85 dB(A) L_{eq} at 50 feet from the center of construction activity when assessing the loudest pieces of equipment—dozer, excavator, and loader—working simultaneously.

Table 6 summarizes the modeled construction equipment for the rock drilling, rock breaking, rock crushing, and grading phases of construction. Once grading is complete, all other on-site construction activities, such as building construction and paving, would be quieter than these modeled phases.

Table 6 Modeled Construction Equipment			
Phase	Equipment	Average Noise Level at 50 Feet [dB(A) L_{eq}]	Sound Power Level [dB(A) L_{PW}]
Rock Drilling	Rock Drill	78	109.7
Rock Breaking	Excavator	81	112.7
	Mounted Impact Hammer	83	114.7
	Combined Noise Level	85	116.8
Rock Crushing	Rock Crusher	88	119.9
	<i>Jaw Crusher</i>	81	112.7
	<i>Cone Crusher</i>	81	113.1
	<i>Screens</i>	86	117.4
	<i>Conveyor</i>	73	104.4
	Loader	76	107.7
	Combined Noise Level	88	120.1
Grading	Dozer	81	112.7
	Excavator	81	112.7
	Loader	76	107.7
	Combined Noise Level	85	116.3

Residential uses are located south, east, and west of the project site and commercial uses are located north of the project site. The Noise Element defines noise sensitive areas as rear yard areas on single-family residences and ground floor common areas and private patio areas for multi-family residences. Noise levels were modeled at a series of 25 receivers located at the adjacent uses. The results are summarized in Table 7. Modeled receiver locations and construction noise contours are shown in Figures 6a through 6d.



- Project Boundary
- Receivers
- Rock Drilling Noise**
- 60 dB(A) L_{eq}
- 65 dB(A) L_{eq}
- 70 dB(A) L_{eq}
- 75 dB(A) L_{eq}



FIGURE 6a
Construction Noise Contours -
Rock Drilling

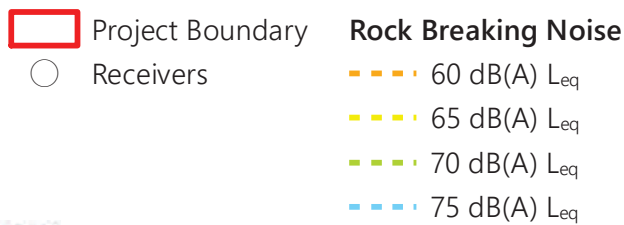


FIGURE 6b
Construction Noise Contours -
Rock Breaking

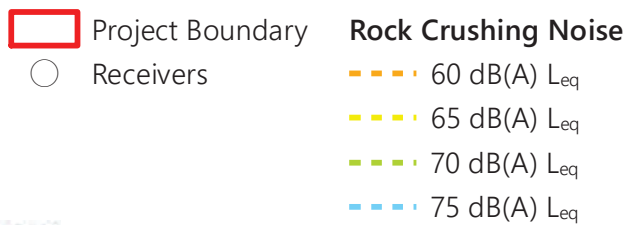


FIGURE 6c
Construction Noise Contours -
Rock Crushing



- Project Boundary
- Receivers
- Grading Noise**
- 60 dB(A) L_{eq}
- 65 dB(A) L_{eq}
- 70 dB(A) L_{eq}
- 75 dB(A) L_{eq}



FIGURE 6d
Construction Noise Contours -
Grading

Table 7 Construction Noise Levels at Off-site Receivers					
Receiver	Land Use	Construction Noise Level [dB(A) L_{eq}]			
		Rock Drilling	Rock Breaking	Rock Crushing	Grading
1	Residential	62	45	52	61
2	Residential	63	45	52	61
3	Residential	63	45	52	60
4	Residential	66	47	54	62
5	Residential	67	48	55	64
6	Residential	67	50	57	65
7	Residential	67	51	58	67
8	Residential	67	54	61	67
9	Residential	62	60	68	63
10	Residential	62	63	70	61
11	Residential	65	67	74	64
12	Residential	65	67	74	65
13	Residential	64	66	73	64
14	Residential	64	64	72	64
15	Residential	67	60	67	65
16	Residential	63	58	65	63
17	Commercial	60	55	62	61
18	Commercial	61	54	61	63
19	Commercial	62	54	61	67
20	Commercial	63	53	60	69
21	Commercial	63	51	58	72
22	Commercial	63	50	57	71
23	Commercial	63	48	55	68
24	Commercial	62	47	54	65
25	Commercial	62	47	54	64

As shown in Table 7, construction noise levels are not anticipated to exceed 75 dB(A) L_{eq} at the property lines of the adjacent residential uses and would therefore would not exceed 75 dB(A) L_{eq} at the noise sensitive areas. In accordance with Santee Municipal Code Section 5.04.090, construction activities would not occur before 7:00 a.m. or after 7:00 p.m. on Mondays through Saturdays and would not occur at any time on Sundays and holidays. Although the adjacent residences would be exposed to construction noise levels that could be heard above ambient conditions, the exposure would be temporary. Additionally, as required by the Municipal Code, a notice would be provided to all owners and occupants within 300 feet of the project site if the construction equipment has a manufacturer’s noise rating of 85 dB and operates at a specific location for 10 consecutive workdays. The noise levels summarized in Table 7 are based on the assumption that the stationary rock crusher would be located 165 feet distance away from the adjacent residential property lines. If rock crushing were to occur closer to the property lines, a significant noise impact would occur. Therefore, the following mitigation measure would be required.

NOISE-1: Construction Noise

Prior to issuance of any grading permit(s) for the project, the project applicant or its contractor(s) shall ensure that:

- On-site rocks and boulders shall be relocated off-site to the maximum extent feasible.
- All on-site rock crushing shall occur at a distance of 165 feet or more from the southern, eastern, and western property lines.
- All construction equipment, fixed or mobile, shall be equipped with properly operating and maintained mufflers.
- Construction noise reduction methods such as shutting off idling equipment, maximizing the distance between construction equipment staging areas and occupied residential areas, and use of electric air compressors and similar power tools, rather than diesel equipment, shall be used where feasible.
- During construction, stationary construction equipment shall be placed such that emitted noise is directed away from or shielded from sensitive noise receivers.
- During construction, stockpiling and vehicle staging areas shall be located as far as practical from noise sensitive receptors.
- The project shall be in compliance with the City's Noise Abatement and Control Ordinance such that construction shall occur on the weekdays (Monday through Friday) and Saturday between the hours of 7:00 a.m. to 7:00 p.m. and a notice of construction shall be mailed to all owners and occupants within 300 feet of the project site no more than 10 days before the start of construction. Construction hours, allowable workdays and the phone number of the job superintendent shall be clearly posted at all construction entrances to allow surrounding property owners and residents to contact the job superintendent. In the event that the City receives a complaint regarding construction noise, appropriate corrective actions shall be implemented and a report of the action provided to the reporting party.

With implementation of mitigation measure NOISE-1, impacts associated with temporary increases in noise during construction would be less than significant.

Operational Noise

In accordance with the Noise Element of the General Plan, the noise level threshold is 65 dB(A) L_{eq} at the property line. On-site generated noise is also regulated by the Santee Municipal Code, Title 5 Health and Safety, Chapter 5.04 Noise Abatement and Control. The sections applicable to the project are as follows:

Section 5.04.040 General Noise Regulations

- A. General Prohibitions. It is unlawful for any person to make, continue, or cause to be made or continued, within the limits of the City, any disturbing, excessive or offensive noise which

causes discomfort or annoyance to reasonable persons of normal sensitivity residing in the area. The characteristics and conditions which should be considered in determining whether a violation of the provisions of this section exists, include, but are not limited to, the following:

1. The level of the noise;
 2. Whether the nature of the noise is usual or unusual;
 3. Whether the origin of the noise is natural or unnatural;
 4. The level of the background noise;
 5. The proximity of the noise to sleeping facilities;
 6. The nature and zoning of the area within which the noise emanates;
 7. The density of the inhabitation of the area within which the noise emanates;
 8. The time of day or night the noise occurs;
 9. The duration of the noise;
 10. Whether the noise is recurrent, intermittent, or constant; and
 11. Whether the noise is produced by a commercial or noncommercial activity.
- B. Disturbing, Excessive or Offensive Noises. The following acts, among others, are declared to be disturbing, excessive and offensive noises in violation of this section:
1. Horns, Signaling Devices or Similar Devices. Violations for disturbing, excessive or offensive noises associated with the use or operation of horns, signaling device or similar devices, on automobiles, motorcycles, or any other vehicle, except as provided elsewhere in this code, will be prosecuted under applicable provisions of the California Vehicle Code.
 2. Radio, Television, Music, Sound Amplifiers, and Similar Devices.
 - a. Uses Restricted. No person is permitted to play, use, operate, or allow to be played, used or operated, any radio, musical instrument, television, loudspeaker, bullhorn, amplifier, public address system, musical instrument, or other machine or device that produces sound in such manner that disturbs the peace, quiet and comfort of persons of normal sensitivity in the area.
 - b. Prima Facie Violations. The operation of any device in subsection (B)(2)(a) between the hours of 10:00 p.m. and 7:00 a.m., in such a manner as to be louder than the average conversational level at a distance of 50 feet from the building, structure or vehicle in which it is located, measured vertically or horizontally, is prima facie evidence of a violation of this section.
 - c. The limitations imposed in this section do not apply between the hours of 7:00 a.m. and 10:00 p.m. to a person participating in: (i) a public assembly; or (ii) a parade, athletic event, or outdoor special event; provided that a permit has been issued for the parade, athletic event or outdoor special event, if required, and the person is in compliance with the permit.

- d. The limitations imposed in this section do not apply to emergency signal devices as described in Section 5.04.100 of this code.
3. Disturbing or raucous yelling, shouting, hooting, whistling or singing on the public streets, between the hours of 10:00 p.m. and 7:00 a.m. or at any time or place so as to annoy or disturb the quiet, comfort or repose of neighboring residents or persons of normal sensitivity within the area for whatever reason, is prohibited. This provision may not be construed to prohibit the selling by outcry of merchandise, food and beverages at sporting events, parades, fairs, celebrations, festivals, circuses, carnivals and other similar special events for public entertainment.
 4. Heating and Air Conditioning Equipment and Generators.
 - a. It is unlawful for any person to operate or allow the operation of any generator, air conditioning, refrigeration or heating equipment in such manner as to create a noise disturbance on the premises of any other occupied property, or if a condominium, apartment house, duplex, or attached business, within any adjoining unit.
 - b. All generators, heating, air conditioning, or refrigeration equipment are subject to the setback and screening requirements in this code.

Section 5.04.070 Motorized Equipment

It is unlawful to operate any lawn mower, backpack blower, lawn edger, leaf blower, riding tractor, or any other machinery, equipment, or other device, or any hand tool which creates a loud, raucous or impulsive sound, within or adjacent to any residential zone between the hours of 10:00 p.m. and 7:00 a.m. of the following day.

Section 5.04.130 Loading and Unloading Operations

- A. It is unlawful for any person to engage in loading, unloading, opening, idling of trucks, closing or other handling of boxes, crates, containers, building materials, garbage cans, dumpsters or similar objects between the hours of 10:00 p.m. and 7:00 a.m. in such a manner as to cause a noise disturbance within or adjacent to a residential district.

Section 5.04.160 Limitations on sources of noise not otherwise addressed:

- A. Between 10:00 p.m. and 7:00 a.m., it is unlawful for any person to generate any noise on the public way that is louder than average conversational level at a distance of 50 feet or more, vertically or horizontally, from the source.
- B. Between 10:00 p.m. and 7:00 a.m., no person is permitted to generate any noise on any private open space that is louder than average conversational level at a distance of 50 feet or more, measured from the property line of the property from which the noise is being generated.

The post-project noise sources would include rooftop mechanical ventilation equipment on the auto dealership and body shop buildings, car wash blowers, car wash vacuums, and the auto service departments and body shop. Noise levels associated with these on-site noise sources were modeled using the SoundPLAN program. Modeled noise levels take into account the six-foot masonry screening walls that would be constructed along the western, southern, and eastern property lines. The masonry wall would be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue.

The rooftop mechanical ventilation systems would include rooftop units (RTUs) and condensing units (CUs). RTU and CU locations were obtained from the project roof plans. The 5-ton RTUs would be similar to Trane packaged rooftop air conditioners Model YSC, the 6- through 15-ton RTUs would be similar to Trane packaged rooftop air conditioners Model YSJ, and the CUs would be similar to Trane Mitsubishi heat pump Model TRUZ. The sound power levels for this equipment is provided in Appendix J. All units were modeled at 100 percent capacity during the daytime hours and the nighttime hours.

The car wash tunnel would include a blower system located within the tunnel approximately 15 feet from the exit. The dryers would be similar to Proto-Vest Dryer systems. Each of the dryers would be equipped with the manufacturer's silencer packages. Noise specifications for the dryers were obtained from the manufacturer and are included in Appendix I. Based on the traffic impact analysis prepared for the project, the car wash would generate up to 39 peak hour trips. A 1-minute drying cycle time was modeled for each wash, for a total blower operating time of 39 minutes per hour. The car wash exit would also include a door that would be closed during a majority of the washing and drying process and would open to allow vehicles to exit. This door would reduce property line noise levels due to operation of the car wash. However, as a conservative analysis, noise reduction due to this door was not included in the model.

The car wash would also include a vacuum system consisting of two central enclosed vacuum motors and 22 vacuum hoses. Vacuum hoses were modeled at each of the proposed vacuum parking spaces located on the west side of the car wash tunnel. A sound power level of 77.3 dB(A) L_{pw} was modeled at each vacuum location (AcoustiControl 2017a). The car wash would operate during the daytime hours only from 7 a.m. to 7 p.m.

The automobile dealerships on Parcels A and B would include service departments and an auto body shop is proposed on Parcel C. Noise-generating equipment at the service stations and auto body shop would include tools such as air compressors, pneumatic tools, and tire machines. All auto body repair services and activities would occur within the proposed buildings with doors closed. However, as a conservative analysis, noise levels were modeled with the doors open. The enclosed buildings would act as noise reducing barriers to minimize exposure of adjacent receivers to excess noise levels. Modern construction materials, consistent with the Universal Building Code, typically provide an exterior-to-interior noise level reduction of 25 to 30 dB with all exterior openings sealed and provide an exterior-to-interior noise level reduction of 10 dB with windows and doors open (Caltrans 2013a). Noise levels due to the operation of an air compressor and pneumatic tools within the proposed buildings were modeled using the maximum noise levels summarized in Table 4 and a usage factor of 20 percent. To account for an enclosed building with doors open, equipment noise levels were reduced by 10 dB. The service departments would operate during the hours of 7 a.m. to

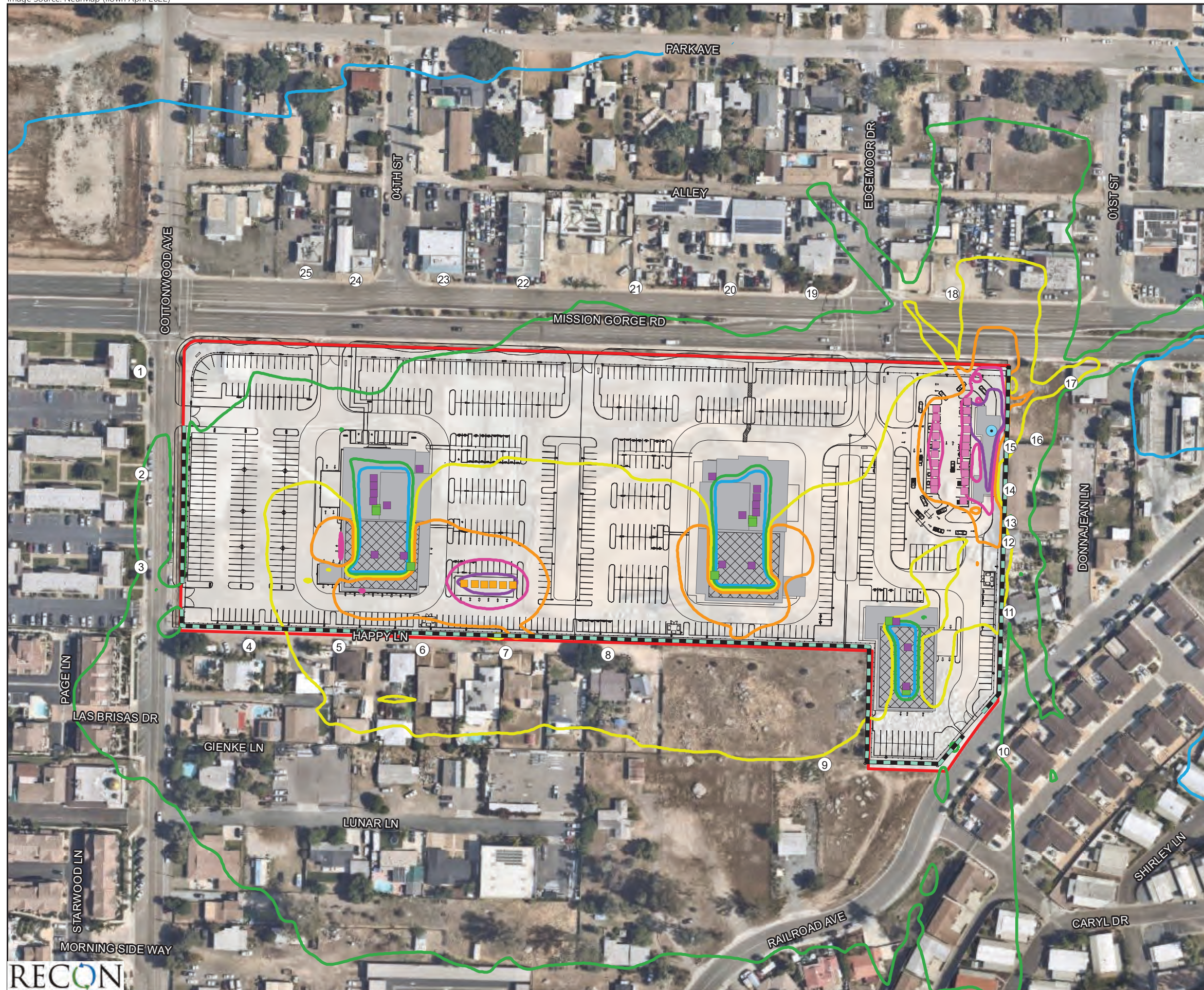
9 p.m. Monday through Sunday. The auto body shop would operate during the hours of 8 a.m. to 5:30 p.m. Monday through Friday.

Parcel A would also include detail bays for up to six vehicles. Noise generated at the detail bays would be generated by Rigid WD1450 shop vacuums that generate a sound power level of 87 dB(A) L_{pw} . Noise levels were modeled with the vacuums operating simultaneously at full power at each of the six open detail bays.

Noise levels due to on-site operations were modeled at a series of 25 receivers located at the adjacent uses. The results are summarized in Table 8. Modeled receiver locations and daytime and nighttime operational noise contours are shown in Figures 7a and 7b, respectively. Calculations are provided in Appendix J.

Table 8 On-Site Generated Noise Levels at Adjacent Property Lines					
Receiver	Land Use	Daytime Noise Level [dB(A) L_{eq}]		Nighttime Noise Level [dB(A) L_{eq}]	
		First-Floor	Second-Floor	First-Floor	Second-Floor
1	Residential	48	49	40	41
2	Residential	50	51	41	42
3	Residential	50	51	40	41
4	Residential	54	--	42	--
5	Residential	57	--	42	--
6	Residential	57	--	42	--
7	Residential	58	--	44	--
8	Residential	57	--	44	--
9	Residential	55	--	45	--
10	Residential	51	52	39	42
11	Residential	52	--	44	--
12	Residential	56	--	41	--
13	Residential	50	55	40	44
14	Residential	50	51	39	43
15	Residential	56	55	38	43
16	Residential	52	52	41	42
17	Commercial	57	--	40	--
18	Commercial	53	--	41	--
19	Commercial	49	--	41	--
20	Commercial	49	--	41	--
21	Commercial	50	--	42	--
22	Commercial	49	--	42	--
23	Commercial	49	--	42	--
24	Commercial	48	--	41	--
25	Commercial	47	--	39	--

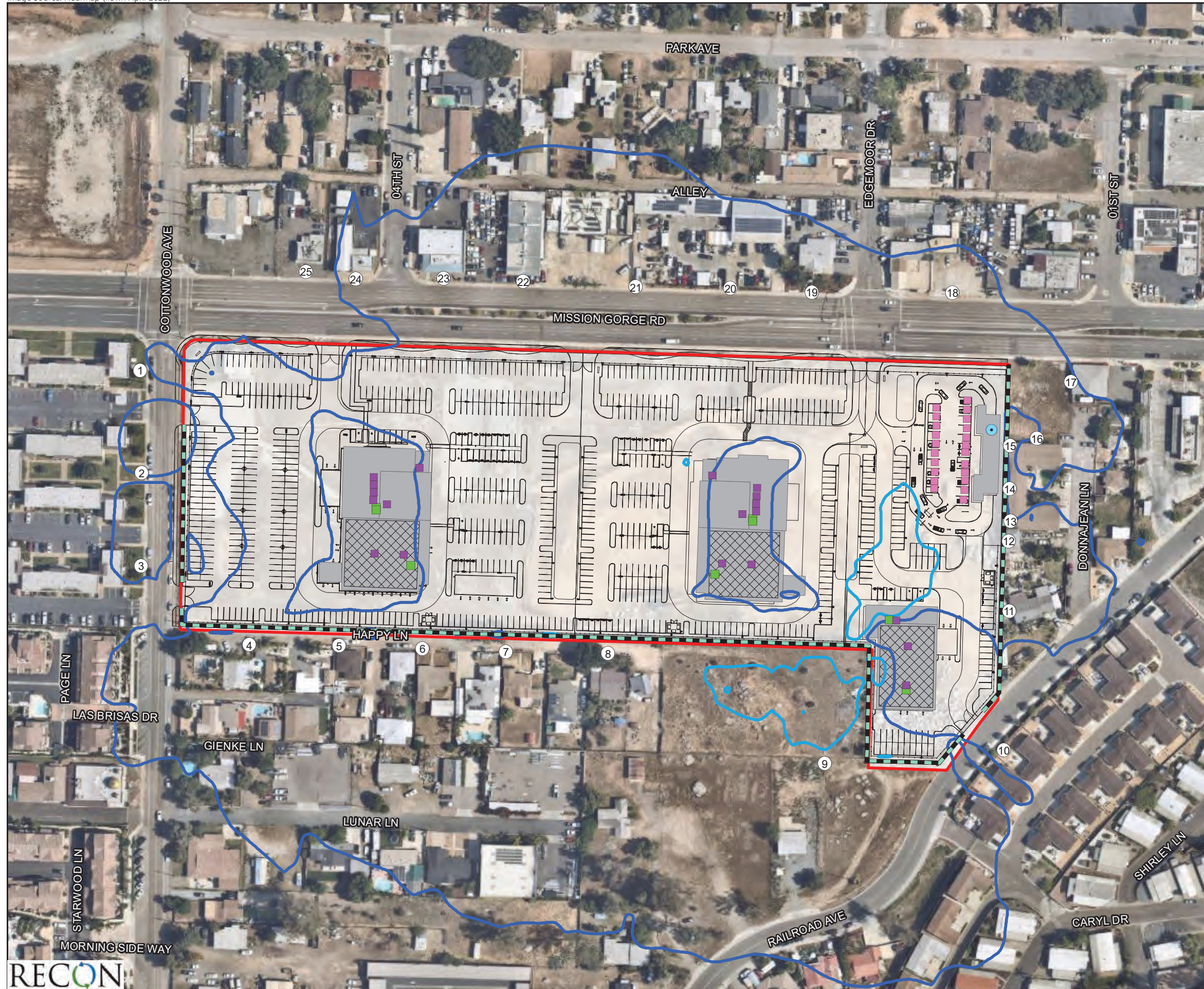
As shown in Table 8, peak hour daytime operational noise levels at the adjacent residential uses would be 58 dB(A) L_{eq} or less and nighttime operational noise levels would be 45 dB(A) L_{eq} or less. At the adjacent commercial uses, peak hour daytime operational noise levels would be 57 dB(A) L_{eq} or less and nighttime operational noise levels would be 42 dB(A) L_{eq} or less. Noise levels are not anticipated to exceed 65 dB(A) L_{eq} during the operational hours.



- Project Boundary
 - Site Plan
 - Receivers
 - Blowers
 - Vacuums
 - Shop Vacs
 - RTUs
 - CUs
 - Wall
 - Body Shop
 - Buildings
- Daytime Operational Noise**
- 40 dB(A) Leq
 - 45 dB(A) Leq
 - 50 dB(A) Leq
 - 55 dB(A) Leq
 - 60 dB(A) Leq
 - 65 dB(A) Leq
 - 70 dB(A) Leq



FIGURE 7a
Operational Noise Contours -
Daytime



- Project Boundary
 - Site Plan
 - Receivers
 - Blowers
 - Vacuums
 - RTUs
 - CUs
 - Wall
 - Body Shop
 - Buildings
- Nighttime Operational Noise**
- 40 dB(A) Leq
 - 45 dB(A) Leq



FIGURE 7b
Operational Noise Contours -
Nighttime

As discussed, the car wash exit would include a door that would be closed during a majority of the washing and drying process and would open to allow vehicles to exit. This door would reduce property line noise levels due to operation of the car wash beyond the levels summarized in Table 8. However, as a conservative analysis, noise reduction due to this door was not included in the model.

Section 5.04.160 of the Santee Municipal Code places limitations on noise sources that occur during the nighttime hours between 10:00 p.m. and 7:00 a.m. During the nighttime hours, "no person is permitted to generate any noise on any private open space that is louder than average conversational level at a distance of 50 feet or more, measured from the property line of the property from which the noise is being generated." The car wash dryers, vacuums, service department, and auto body shop would not be operational during the nighttime hours. Nighttime noise levels due to rooftop mechanical equipment would range from 38 to 45 dB(A) L_{eq} , which is not considered louder than the average conversational noise level.

To ensure that noise levels do not exceed those summarized in Table 8, the following mitigation measure would be required:

NOISE-2: Operational Noise

The project shall incorporate the following measures:

- Prior to precise grading, a six-foot masonry wall noise barrier shall be constructed along the western, southern, and eastern project boundaries as depicted in Figure 6a. The masonry wall shall be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue. The sound attenuation walls must be solid and free of cracks, gaps, or holes through or below the wall. Any seams or cracks must be filled or caulked.
- The manufacturer noise specifications for the car wash blower system selected for the project shall include a silencer package and shall not exceed a sound power level of 106 dB(A) L_{pw} .
- The manufacturer noise specifications for the car wash vacuum hoses selected for the project shall not exceed a sound power level of 77.3 dB(A) L_{pw} .
- Operation of the car wash shall be prohibited during the hours of 7 p.m. and 7 a.m.

With implementation of mitigation measure NOISE-2, operational noise levels are not anticipated to violate the Santee Municipal Code, and impacts would be less than significant.

b. Less than Significant Impact. Construction activities would have the potential to result in varying degrees of temporary ground vibration, depending on the specific construction equipment used and operations involved. Ground vibration generated by construction equipment spreads through the ground and diminishes in magnitude with increases in distance. The effects of ground vibration may be imperceptible at the lowest levels, low rumbling sounds and detectable vibrations at moderate levels, and damage to nearby structures at the highest levels. Vibration perception would occur at structures, as people do not perceive vibrations without vibrating structures.

Human reaction to vibration is dependent on the environment the receiver is in as well as individual sensitivity. For example, vibration outdoors is rarely noticeable and generally not considered annoying. Typically, humans must be inside a structure for vibrations to become noticeable and/or annoying. Based on several federal studies, the threshold of perception is 0.035 inch per second (in/sec) peak particle velocity (PPV), with 0.24 in/sec PPV being distinctly perceptible (Caltrans 2013b). Neither cosmetic nor structural damage of buildings occurs at levels below 0.1 in/sec PPV.

Construction equipment could include a rock drill, rock crushing/processing equipment, loaded trucks, an excavator, as well as a dozer or loader. This equipment would generate maximum vibration levels up to 0.089 in/sec PPV at 25 feet. This range of construction vibration levels would be below the distinctly perceptible threshold of 0.24 in/sec PPV and below the cosmetic and structural damage of buildings threshold of 0.1 in/sec PPV. Therefore, project construction would not generate excessive groundborne vibration or groundborne noise levels, and impacts would be less than significant. Once operational, the project would not be a source of groundborne vibration.

c. Less than Significant Impact. The property is located within the Airport Influence Area, Review Area 1 of Gillespie Field Airport. However, the project site is located just outside the 60 Community Noise Equivalent Level contour for Gillespie Field. Therefore, the project would not expose people to excessive noise levels from airport noise, and impacts would be less than significant.

15.14 Population and Housing

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: Project Description; City of Santee General Plan–Land Use Element; San Diego Association of Governments (SANDAG) Data Surfer (SANDAG 2021).

a. Less than Significant Impact. Per the SANDAG Series 14 growth forecast, the population within the city was estimated to be 58,358 in 2025 and is estimated to increase by 3,539 people to 61,897 in 2035. The project does not include the construction of residential uses and would not extend any existing roads or expand existing infrastructure facilities that could induce growth. The project would result in 227 new jobs in the city. These jobs are expected to be accommodated by the existing

workforce. Therefore, implementation of the project would not directly or indirectly induce substantial population growth in the area. No impact would occur.

b. No Impact. The project site is vacant and currently undeveloped. Therefore, the project would not displace any existing people or housing. No impact would occur.

15.15 Public Services

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
(i) Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(ii) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(iii) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(iv) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(v) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: City of Santee General Plan; City of Santee Fire Department; San Diego County Sheriff’s Department; and Fire and Rescue Mutual Aid Operations (County of San Diego 2014).

a(i). Less than Significant Impact. The City operates two fire stations: one located at 8950 Cottonwood Avenue and the other at 9130 Carlton Oaks Drive. The project would be consistent with the existing land use and zoning designations for the project site, and therefore would be consistent with the growth assumptions utilized in the City’s fire protection planning. Furthermore, the project is located approximately 0.1 mile south of Fire Station 4, located on Cottonwood Avenue, which would be able to respond to an emergency at the project site within the City’s goal of six minutes. Therefore, the project would not result in the need for new or altered fire protection facilities, and impacts would be less than significant.

a(ii). Less than Significant Impact. Police protection is provided by the San Diego County Sheriff’s Department under contractual agreement with the City and operating out of the Santee Substation at 8811 Cuyamaca Street. The average priority call response time for general law enforcement within

the city is 8.2 minutes and the average for traffic law enforcement is 7.5 minutes. Appropriate staffing levels for law enforcement personnel are evaluated at every contract renewal. The project would be consistent with the existing land use and zoning designations for the project site. Consequently, the project would be consistent with growth projections that were utilized to forecast future police protection within the city. Therefore, the project would not result in the need for new or altered police facilities, and impacts would be less than significant.

a(iii). No Impact. The project would be consistent with the existing General Commercial (GC) district. The project does not include the construction of residential uses that would require school services and would be consistent with growth projections that were utilized to forecast future demand for school services. Pursuant to Government Code Section 65995 et seq., the project proponent would be required to pay applicable school fees before a construction permit is issued. No impact would occur. Therefore, the project would not result in the need for new or altered school facilities, and no impact would occur.

a(iv). No Impact. The project would be consistent with the existing General Commercial (GC) district. The project does not include the construction of residential uses and would be consistent with growth projections that were utilized to forecast future park demand within the city. Therefore, the project would not result in the need for new or altered park facilities, and no impact would occur.

a(v). No Impact. The County of San Diego Library operates a Santee Branch at 9225 Carlton Hills Boulevard, Suite 17. The project would be consistent with the existing General Commercial (GC) district. The project does not include the construction of residential uses and would be consistent with growth projections that were utilized to forecast future library demand within the city. Therefore, the project would not result in the need for new or altered library facilities, and no impact would occur.

15.16 Recreation

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Source: Project Description.

a. No Impact. The project would be consistent with the existing General Commercial (GC) district. The project does not include the construction of residential uses and would not result in a substantial increase in the use of parks that would accelerate their physical deterioration. Thus, no impact would occur.

b. No Impact. The project does not include the provision of recreational facilities or require the construction or expansion of recreational facilities. No impact would occur.

15.17 Transportation/Traffic

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Conflict with a program plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Traffic Impact Analysis prepared by Linscott, Law, and Greenspan, 2023 (see Appendix BE).

a. Less than Significant Impact.

The City uses the regionally adopted San Diego Traffic Engineers' Council/ITE guidelines for the purposes of traffic impact analysis. The project trip generation was calculated using the trip rates based on the ITE 11th Edition of the Trip Generation Manual and SANDAG (*Not So*) *Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region* (2002). For the auto dealership, detail bays, and body shop, the trip rates for Land Use 840, Automobile Sales (New) from the ITE Trip Generation Manual were used. For the car wash, the trip rates for Land Use 948 Automated Car Wash from the ITE Trip Generation Manual were used to estimate the trip generation for this project. However, the ITE Trip Generation Manual only provides the PM peak hour rates. Therefore, the rate per car wash from the SANDAG (*Not So*) *Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region* (2002) was used to estimate the trip generation.

Construction of the project would result in impacts to the following intersections:

- Mission Gorge Road/Cuyamaca Street
- Mission Gorge Road/Cottonwood Avenue

In order to reduce potential impacts to the above intersections, the project would incorporate the following improvements:

- The project would align the project driveway opposite Edgemoor Drive, modify the existing traffic signal and provide a 40-foot-wide driveway with one left-turn lane and one shared through/right lane in the northbound direction (project driveway) at this intersection.
- At the northerly driveway on Cottonwood Avenue, the southbound left-turn movement would be prohibited and only right-in/right-out and outbound left-turn movements would be permitted. Stop signs would be installed at all unsignalized driveways for traffic exiting the driveways.

Therefore, operation of the project would not conflict with a program plan, ordinance or policy addressing the performance of the roadway circulation system, and impacts would be less than significant.

Bus service is provided by the MTS. The bus routes serving the immediate area surrounding the project site include MTS Routes 832, 833, and 834. The bus stop closest to the project site is less than 300 feet away on Mission Gorge Road along the project frontage. A bicycle network inventory was conducted for the study area. Class II bike lanes are provided along Mission Gorge Road, Magnolia Avenue, Riverview Parkway, Town Center Parkway, Cuyamaca Street, Carlton Hills Boulevard, Carlton Oaks Drive, and Mast Boulevard. There are no bike lanes or bike routes provided on the segments of Cottonwood Avenue and Edgemoor Drive adjacent to the project site. Continuous sidewalks are provided along both sides of Mast Boulevard, Carlton Oaks Drive, Mission Gorge Road, and Town Center Parkway within the study area. Sidewalks are missing on Cuyamaca Street south of Prospect Avenue, Cottonwood Avenue, and the west side of Magnolia Avenue between Chubb Lane and Park Avenue. Implementation of the project would not include any off-site improvements that would impact any of these facilities. The project would improve pedestrian access by constructing a meandering, non-contiguous sidewalk along Mission Gorge Road. Therefore, the project would not conflict with a program plan, ordinance or policy addressing transit, bicycle and pedestrian facilities and impacts would be less than significant.

b. Less than Significant Impact. In December 2018, the Natural Resources Agency adopted amendments to the CEQA Guidelines, including the incorporation of Senate Bill 743 (SB 743) modifications. The Office of Planning and Research also published an update to its Technical Advisory on Evaluating Transportation Impacts in CEQA (Technical Advisory) to assist professional planners, land use officials, and CEQA practitioners. The Technical Advisory provides recommendations on how to evaluate transportation impacts under SB 743 that agencies and other entities may use at their discretion. The Technical Advisory recommends the use of VMT as the preferred CEQA transportation metric. To comply with the new legislation, the City has identified VMT analysis

methodology, established VMT thresholds for CEQA transportation impacts, and identified possible mitigation strategies.

VMT is a metric that accounts for the number of vehicle trips generated and the length or distance of those trips. VMT does not directly measure traffic operations but instead is a measure of network use or efficiency, especially if expressed as a function of population or employment (e.g., VMT/capita). VMT tends to increase as land use density decreases and travel becomes more reliant on the use of the automobile due to the long distances between origins and destinations.

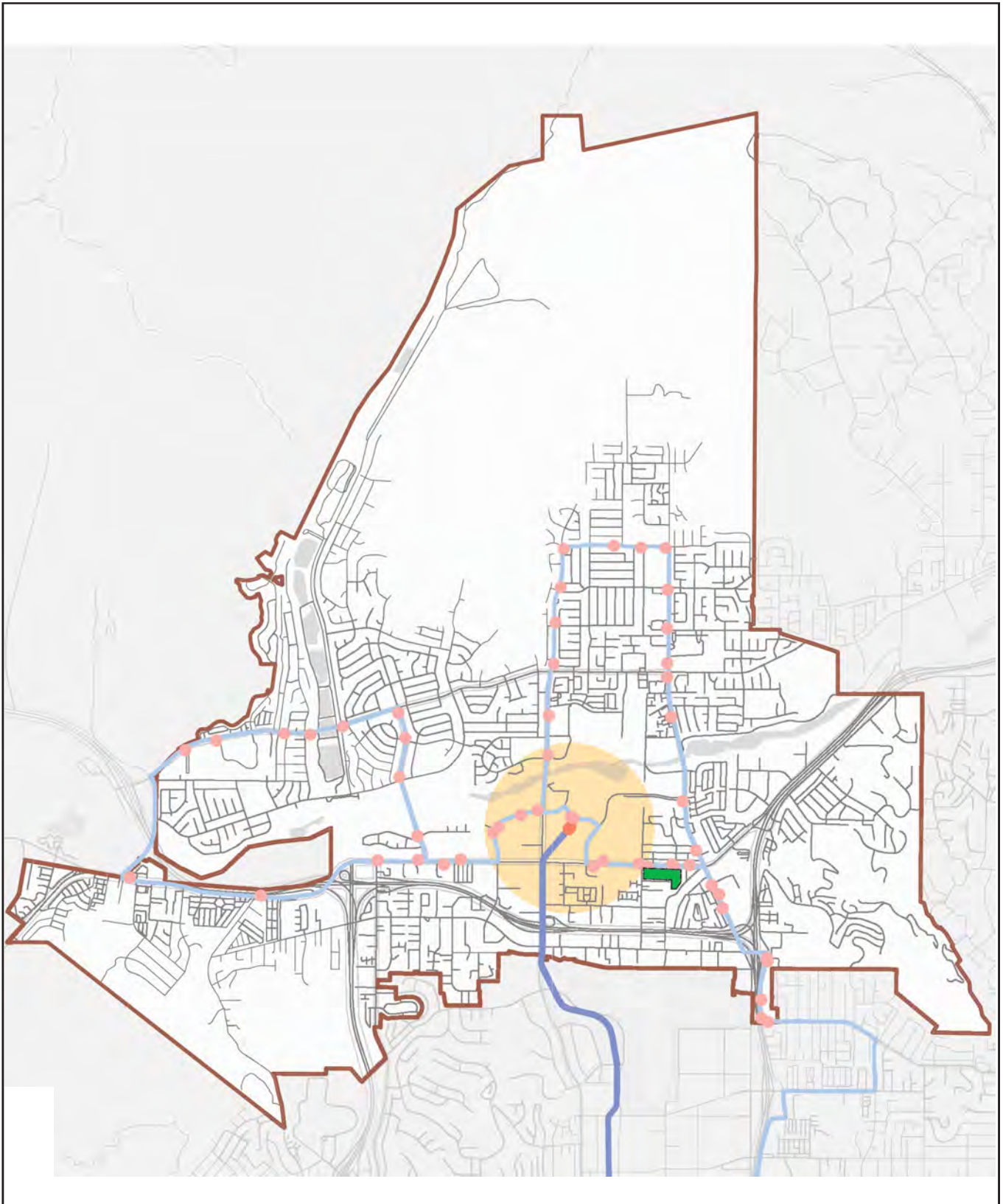
The requirements to prepare a detailed transportation VMT analysis apply to all discretionary land development projects that are not exempt from CEQA, except those that meet at least one of the City's transportation screening criteria. A project that meets at least one of the City's screening criteria would be presumed to have a less than significant VMT impact due to project characteristics and/or location. Senate Bill 743 (Steinberg, 2013) provides for streamlined environmental review for projects within Transit Priority Areas, which is an area within a half mile of a "major transit stop." A major transit stop, as defined in Public Resources Code 21064.3, is a site that contains any of the following:

- An existing rail or bus rapid transit station.
- A ferry terminal served by either a bus or rail transit service.
- The intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods (high-quality transit corridor).

Projects located within a half-mile radius of an existing major transit stop or an existing stop along a high-quality transit corridor may be presumed to have a less-than-significant impact per the City's VMT screening criteria. As described in 15.17.a, bus routes serving the immediate project area include MTS Routes 832, 833, and 834. The bus stop closest to the project site is less than 300 feet on Mission Gorge Road along the project frontage. In addition, as shown in Figure 8, a portion of the project is located within the Transit Priority Area. Therefore, the project would not conflict or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b), and impacts would be less than significant.

c. Less than Significant Impact. As described in 15.17.a above, the project would incorporate roadway and site access improvements that would improve safety. Therefore, the project would not increase potential hazards associated with any new design feature or create an incompatible use, and impacts would be less than significant.

d. Less than Significant Impact. Access to the project site would be provided via Cottonwood Avenue, Mission Gorge Road, and Railroad Avenue. As described in 15.17.a, the project would incorporate roadway and site access improvements that would improve operations at the intersections of Mission Gorge Road/Cuyamaca Street and the Mission Gorge Road/Cottonwood Avenue. Therefore, the project would not impact surrounding roadways which would result in impediments to emergency access, and impacts would be less than significant.



-  City of Santee
-  Transit Priority Area
-  High Quality Transit Route
-  Other Transit Routes
-  High Quality Transit Stop
-  Other Transit Stops



FIGURE 8
High-Quality Transit Corridors
and Transit Stops

15.18 Tribal Cultural Resources

Would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code Section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:				
i. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ii. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: City of Santee General Plan-Conservation Element; Cultural Resources Due Diligence Study prepared by Rincon Consultants, Inc. (2019, Appendix C); Phase II Cultural Resources Testing and Evaluation Report prepared by Rincon Consultants, Inc. (2019, Appendix D).

a.i. and a.ii. Less than Significant with Mitigation.

As discussed in Section 15.5, the prehistoric or historic components of the site were recommended as ineligible for listing under NRHP Criterion D/CRHR Criterion 4. The project is not subject to Senate Bill (SB) 18 as the project is not a General Plan, Specific Plan or an amendment to these plans. However, the project is subject to Assembly Bill (AB) 52 as it is subject to CEQA.

On July 3, 2023, the City prepared and sent AB 52 notification letters to the four tribal contacts that formally requested notification of projects in the City via certified mail. The City received evidence that three of the four tribes received the notification. No evidence was provided to the City that the fourth tribal organization received the notice. Of the three tribal organizations that received the notice, one was returned to the City with a "return to sender" notification.

Under California Public Resources Code, Section 21080.3.1(b), the tribes had 30 days from the receipt of the notification letters to request consultation under AB 52. Within the 30-day response period, the City received one response to the AB 52 consultation letters from the Barona Band of Mission Indians (Barona).

In an email dated July 7, 2023, Barona requested AB 52 consultation for the project, copies of prior cultural resource surveys or reports and more information on the proposed monitoring program. City staff coordinated with Barona as to the preferred format of the reports (i.e., hard copy, electronic, thumb drive). Three reports were sent to Barona electronically in three separate emails on July 12, 2023. On July 13, 2023, Barona confirmed receipt of all the reports and concluded that the mitigation recommended (starting on page 6 of the Rincon report) "seems adequate for the resources that have been identified." Barona further stated: "However, if anything significant is encountered before or during construction, I definitely want to hear about it." On July 14, 2023, Barona confirmed that the AB 52 consultation was concluded "as long as the mitigation measures are included as conditions of approval and are implemented as the project goes forward." As of the publication of this MND, the AB 52 consultation has concluded.

As addressed above, implementation of mitigation measures CUL-1 through CUL-4 would require a Worker's Environmental Awareness Program prior to the start of ground disturbance for the project and that archaeological and Native American monitoring take place during initial project related ground disturbance to assure that any resources found during project grading be protected. Therefore, impacts to tribal cultural resources would be less than significant with the incorporation of mitigation.

15.19 Utilities and Service Systems

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry, and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Result in a determination by the wastewater treatment provided which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: City of Santee, General Plan, Conservation Element; Santee Municipal Code; Project Site Plan; County of San Diego Countywide Five-Year Review Report of the Countywide Integrated Waste Management Plan (County of San Diego 2012); Padre Dam Municipal Water District website (<http://www.padredam.org/>).

a. Less than Significant Impact. The project would be consistent with the existing land use and zoning designations. Consequently, the project would not require water and wastewater service beyond

what has been anticipated by regional growth projections. Existing water and sewer facilities are available adjacent to the site, and improvements would be limited to the extension of pipelines onto the project site. Consequently, potential impacts associated with these water and wastewater connections have been evaluated throughout this Draft IS/MND. Therefore, the project would not require relocation or construction of new or expanded water or wastewater treatment facilities that would cause significant environmental effects, and impacts would be less than significant.

The project would be required to prepare a site-specific SWPPP consistent with the SWRCB Construction General Permit as a condition of approval. In addition, the developed run-off would flow into proposed bio-infiltration areas and then into detention basins and ultimately released at less than or equal to pre-project flows. Therefore, the project would not require relocation or construction of new or expanded storm water drainage facilities, and impacts would be less than significant.

The project would be consistent with the existing land use and zoning designations. Consequently, the project would not consume additional electric power, natural gas, or telecommunication services beyond what has been anticipated by regional growth projections. Existing energy and telecommunication facilities are available adjacent to the site, and improvements would be limited to extensions onto the project site. Potential impacts associated with these energy and telecommunication connections have been evaluated throughout this Draft Initial Study/Mitigated Negative Declaration. Therefore, the project would not require relocation or construction of new or expanded electric power, natural gas, or telecommunication services facilities, and impacts would be less than significant.

b. Less than Significant Impact. The project would be consistent with the existing land use and zoning designations. Consequently, the project would not require water service beyond what has been anticipated by regional growth projections. Therefore, the project would have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry, and multiple dry years, and impacts would be less than significant.

c. Less than Significant Impact. The project would be consistent with the existing land use and zoning designations. Consequently, the project would not require wastewater treatment service beyond what has been anticipated by regional growth projections. Therefore, adequate wastewater treatment capacity exists to serve the project, and impacts would be less than significant.

d. Less than Significant Impact. Santee Municipal Code Section 13.38.060 requires that a minimum of 65 percent by weight of construction and demolition debris be diverted from landfills through recycling, reuse, and diversion programs. The project would develop a construction and demolition debris management plan demonstrating how the project would comply with the Santee Municipal Code diversion requirements prior to issuance of a building or demolition permit.

Solid waste generated during operation of the project that cannot be recycled would be sent to area landfills. Based on the Five-Year Review Report of the County of San Diego Integrated Waste Management Plan for the County, remaining capacity at area landfills would be adequate to handle the project's solid waste disposal needs. Most solid waste collected in the city is disposed of at the Sycamore Sanitary Landfill, which has remaining capacity through the year 2054. Other landfills that handle waste from Santee include the Miramar Landfill and the Otay Landfill, which have remaining

capacity. Therefore, the project would be served by landfill(s) with sufficient permitted capacity, and impacts would be less than significant.

e. Less than Significant Impact. The project would comply with the City’s construction and demolition recycling ordinance (Santee Municipal Code Section 13.38.060) and Solid Waste Ordinance #3239-A, which is consistent with the state solid waste and recycling regulations requiring a minimum of 65 percent of the project’s construction and demolition be diverted from the landfills. Therefore, the proposed would comply with applicable management and reduction statutes and regulations related to solid waste, and impacts would be less than significant.

15.20 Wildfire

If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines, or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: California Department of Forestry and Fire Protection (<https://egis.fire.ca.gov/FHSZ/>); General Plan Safety Element.

a. Less than Significant Impact. As described in Section 15.9.f, the project site is located in an existing developed area with access to major roadways that would allow for emergency evacuation. Therefore, the project would not impair implementation of, or physically interfere with emergency response and impacts would be less than significant.

b. Less than Significant Impact. As shown on the California Department of Forestry and Fire Protection Hazard Severity Zones Map, the project site is not located within land mapped as a fire hazard severity zone. The project is located in a generally flat area and is surrounded by existing development on all sides. In addition, Fire Station 4 is located approximately 0.1 mile north of the project site at 8950 Cottonwood Avenue. Therefore, there are no characteristics of the surrounding environment that would exacerbate wildfire risks, and impacts would be less than significant.

c. Less than Significant Impact. As described in Section 15.19.a, the project would not require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities. Additionally, the project would not require construction or maintenance of any other infrastructure facilities. Therefore, the project would not require the installation or maintenance of associated infrastructure that may exacerbate fire risk, and impacts would be less than significant.

d. No Impact. As described in Section 15.9.g, the project site is not within the 100-year floodplain. Although the project is located within the potential inundation areas delineated on Figure 8-2 of the General Plan Safety Element, the project does not possess any features that would exacerbate risk associated with flood inundation beyond what exists for surrounding land uses in the existing condition. Furthermore, the project site is located in a generally flat area and surrounded by existing development on all sides. Therefore, the project would not expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes. No impacts would occur.

15.21 Mandatory Findings of Significance

Does the project:

Issue	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a. Have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable futures projects)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Santee Active Projects,
<https://www.cityofsanteeca.gov/home/showpublisheddocument/22956/638066340519230000>

a. Less than Significant with Mitigation. As described in Section 15.4, if vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30), mitigation measure BIO-1 would require a pre-construction clearance survey for nesting birds and raptors. Implementation of mitigation measure BIO-1 would reduce potential impacts to nesting birds and raptors to a level less than significant.

As described in Section 15.5, there is potential for buried archaeological deposits that could be encountered during project related ground disturbance. Due to the overall sensitivity of the project area, the project would require completion of a Worker's Environmental Awareness Program prior to the start of ground disturbance for the project and that archaeological and Native American monitoring take place during initial project related ground disturbance. Implementation of mitigation measures CUL-1 through CUL-4 would reduce impacts to historic and unique archaeological resources to a level less than significant.

b. Less than Significant Impact. In addition to evaluation of potential project-specific effects, this evaluation considered the project's potential for incremental effects that may be cumulatively considerable when viewed in connection with the effects of past, current, or probable future projects in the area. The City's Active Projects Map identified cumulative projects in the project area for review.

As discussed in this Initial Study, project impacts and future development impacts would be reduced with mitigation measures and adherence with all regulatory compliance. Air quality is a regional issue and the cumulative study area for air quality impacts encompasses the SDAB as a whole. Therefore, the cumulative analysis addresses regional air quality plans and policies, such as the RAQS, as well as the project's contribution to a net increase of any criteria pollutant for which the SDAB is listed as a non-attainment area. As described in Section 15.3.a, the project would be consistent with the growth projections of the General Plan and would not result in an increase in emissions that are already accounted for in the RAQS. Climate change is, by its nature, a cumulative issue. As described in Section 15.8.b, the project would not conflict with the applicable plans developed to reduce GHG emissions at the regional level. As described in Section 5.13 all project-level noise impacts would be mitigated to a level less than significant. Due to the varied schedules and for construction of cumulative projects listed in the City's Active Projects Map, it is unlikely construction activities would overlap, thereby avoiding significant cumulative noise impacts on sensitive receptors. In the event of other future developments in the surrounding area, adherence to all applicable local, state, and federal regulations would be required to reduce potential impacts to a less than significant level. Therefore, the project is not anticipated to contribute to considerable environmental impacts, and impacts would be less than significant.

c. Less than Significant Impact. As discussed throughout this document, no hazardous conditions on the project site or in the surrounding area were identified that could adversely affect human beings. It is not anticipated that demolition or construction activities would create conditions that would significantly directly or indirectly impact human beings. Development of the project site would comply with all state and City regulations that would ensure the building is safe and designed to protect future occupants. The project would not result in any substantial adverse effects on human beings directly or indirectly.

16. Checklist References

Project documents—including all plans, documents, departmental comments and information contained in the files for the Schoolyard Project.

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2. Airport Land Use Commission (ALUC). Gillespie Field Airport Land Use Compatibility Plan, 2010.
3. California Air Pollution Control Officers Association (CAPCOA), CEQA and Climate Change: Evaluating and Addressing Greenhouse Gas Emissions from Projects Subject to the California Environmental Quality Act, January 2008.
4. CAPCOA, California Emissions Estimator Model Version 2016.3.1, 2017.
5. California Air Resources Board (CARB), Climate Change Scoping Plan, 2017.
6. CARB, Climate Change Scoping Plan: A Framework for Change, 2008. Accessed at: http://www.arb.ca.gov/cc/scopingplan/document/adopted_scoping_plan.pdf.
7. CARB, Emission Factors Web Database model, 2017. Accessed at <https://www.arb.ca.gov/emfac/2017/>.
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11. Caltrans, Transportation and Construction Vibration Guidance Manual. September 2013b.
12. California Energy Code, California Code of Regulations, Title 24, Part 6, 2019.
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24. Federal Highway Administration (FHWA). Roadway Construction Noise Model User's Guide. FHWA-HEP-05-054, SOT-VNTSC-FHWA-05-01, Final Report, January 2006.
25. Federal Transit Administration. Transit Noise and Vibration Impact Assessment Manual. September 2018. https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/research-innovation/118131/transit-noise-and-vibration-impact-assessment-manual-fta-report-no-0123_0.pdf
26. Institute of Transportation Engineers (ITE). Guidelines for Traffic Impact Studies in the San Diego Region, May 2019
27. California Department of Conservation. Mineral Lands Classification. 1996.
28. Office of Environmental Health Hazard Assessment (OEHHA) Air Toxics Hot Spots Program Guidance Manual for the Preparation of Risk Assessments (Guidance Manual), February 2015.
29. Padre Dam Municipal Water District (PDMWD). Website Available at <http://www.padredam.org/>.
30. Padre Dam Municipal Water District Urban Water Management Plan. 2020. <https://www.padredam.org/DocumentCenter/View/5620/2020-Urban-Water-Management-Plan>.
31. San Diego Air Pollution Control District (SDAPCD) Resolution Adopting Amended Rule 20.1 – New Source Review – General Provisions; Rule 20.2 – New Source Review – Non-Major Stationary Sources; Rule 20.3 – New Source Review – Major Stationary Sources And Prevention of Significant Deterioration (PSD) Stationary Sources; Rule 20.4 – New Source Review – Portable Emission Units; and Rule 20.6 – Standards for Permit to Operate Air Quality Analysis, of Regulation II of the Rules and Regulations of the San Diego Air Pollution Control District. Resolution Number 16-041, April 2016.
32. San Diego Air Pollution Control District (SDAPCD). Facility Emissions – 2020 Industry Wide Emissions Inventory Report. Accessed at <https://www.sdapcd.org/content/sdapcd/permits/toxics-emissions/facility-emissions.html>.

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APPENDICES

(Under Separate Cover)

MITIGATION MONITORING AND REPORTING PROGRAM INCORPORATED INTO THE PROJECT

Section 21081.6 of the CEQA Guidelines requires that a MMRP be adopted upon certification of an Environmental Impact Report or adoption of a Mitigated Negative Declaration to ensure that the mitigation measures are implemented. The MMRP specifies the mitigation for the project, when in the process it should be accomplished, and the entity responsible for implementing and/or monitoring the mitigation. Public Resources Code Section 21081.6 requires monitoring of only those impacts identified as significant or potentially significant.

The following MMRP table identifies measures that specifically apply to this project. As shown in the table, each mitigation measure is categorized by impact area and corresponding number, with an accompanying identification of:

- The time of the project during which the measure should be implemented;
- The responsible monitoring party; and
- The action indicating compliance with the mitigation measure.

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
Biological Resources				
<p>BIO-1 – Nesting Birds and Raptors: If vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30), a pre-construction clearance survey for nesting birds and raptors shall be conducted. The survey shall cover all suitable nesting habitats that occur within the proposed project site. The pre-construction survey shall be conducted within three days of the start of work.</p> <p>If any active nests are detected, the area will be flagged and mapped along with a buffer as recommended by the qualified biologist. The buffer area(s) established by the qualified biologist will be avoided until the nesting cycle is complete or it is determined that the nest is no longer active. The qualified biologist shall be a person familiar with bird breeding behavior and capable of identifying the bird species of San Diego County by sight and sound and determining alterations of behavior as a result of human interaction. Buffers will be based on species-appropriate buffers and/or local topography and line of sight, species behavior and tolerance to disturbance, and existing disturbance levels, as determined appropriate by the qualified biologist.</p>	<p>Prior to grading or ground disturbance, if vegetation clearing activities are proposed during or continue into the general bird breeding season (February 15–August 30).</p>	<p>Project Applicant</p>	<p>Pre-grading survey for nesting birds and raptors</p>	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
Cultural Resources				
CUL-1: Worker Environmental Awareness Program: Prior to the commencement of project-related ground-disturbing activities, including but not limited to site clearing, grubbing, trenching, and excavation, a qualified archaeologist who meets or exceeds the Secretary of the Interior's Professional Qualifications Standards for archaeology shall provide a Worker Environmental Awareness Program for the general contractor, subcontractors, and construction workers participating in ground-disturbing activity for project construction. The Worker Environmental Awareness Program training shall describe the potential of exposing archaeological resources, types of cultural materials that may be encountered, and directions on the steps that shall be taken if such a find is encountered. This training may be presented alongside other environmental training programs required prior to construction. A Worker Environmental Awareness Program acknowledgment form shall be signed by workers who receive the training.	Prior to grading or ground disturbance	Project Applicant	A qualified archaeologist who meets or exceeds the Secretary of the Interior's Professional Qualifications Standards for archaeology shall provide a Worker Environmental Awareness Program and training for the general contractor, subcontractors, and construction workers participating in ground-disturbing activity for project construction.	
CUL-2: Preparation of a Cultural Resources Mitigation and Monitoring Program: Prior to the start of any ground-disturbing activity for project construction, including but not limited to site clearing, grubbing, trenching, and excavation, a qualified archaeologist who meets or exceeds the Secretary of Interior's Professional Qualifications Standards for archaeology shall be retained to prepare a Cultural Resources Mitigation and Monitoring Program for unanticipated discoveries during project construction. The Cultural Resources Mitigation and Monitoring Program shall be prepared in consultation with Native American tribes who have participated in consultation for the project. The Cultural Resources Mitigation and Monitoring Program shall	Prior to grading or ground disturbance	Project Applicant	A qualified archaeologist who meets or exceeds the Secretary of Interior's Professional Qualifications Standards for archaeology shall be retained to prepare a Cultural Resources Mitigation and Monitoring Program for unanticipated discoveries during project construction.	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
include provisions for archaeological, historic and Native American monitoring of initial ground disturbance related to construction of the project, project construction schedule, procedures to be followed in the event of discovery of archaeological resources including additional artifacts or features associated with RIN-S-1, and protocols for Native American coordination and input, including review of documents. The Cultural Resources Mitigation and Monitoring Program shall outline the role and responsibilities of Native American monitor(s). It shall include communication protocols and opportunity and timelines for review of cultural resources documents related to discoveries that are Native American in origin. The Cultural Resources Mitigation and Monitoring Program shall include provisions for Native American monitoring during testing or data recovery efforts for unknown resources that are Native American in origin, shall any be encountered. Once completed, the Cultural Resources Mitigation and Monitoring Program shall be reviewed and approved by the Project Planner at the City of Santee prior to the start of any ground disturbing activities.				

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>CUL-3: Cultural Resources Construction Monitoring: A qualified archaeologist who meets or exceeds the Secretary of Interior’s Professional Qualifications Standards for archaeology shall be present during ground-disturbing activity for project construction, including but not limited to site clearing, grubbing, trenching, and excavation, for the duration of the project ground disturbance or until the qualified archaeologist determines monitoring is no longer necessary.</p> <p>At the completion of monitoring, the qualified archaeologist should prepare a Cultural Resources Monitoring Report to document the findings during the monitoring effort for the project. The report shall include the monitoring logs completed for the project and shall document any discoveries made during monitoring. The Cultural Resources Monitoring Report shall be submitted to the City of Santee and the South Coastal Information Center.</p>	During ground-disturbing activity	Project Applicant	<p>A qualified archaeologist who meets or exceeds the Secretary of Interior’s Professional Qualifications Standards for archaeology shall be present during ground-disturbing activity for project construction.</p> <p>At the completion of monitoring, the qualified archaeologist should prepare a Cultural Resources Monitoring Report to document the findings during the monitoring effort for the project.</p>	
<p>CUL-4: Native American Construction Monitoring: A minimum of one Native American monitor shall be present during ground-disturbing activity for project ground disturbance, including but not limited to site clearing, grubbing, trenching, and excavation, for the duration of the project ground disturbance or until the qualified archaeologist determines monitoring is no longer necessary. The Native American monitors shall be of Kumeyaay descent with ancestral ties to the San Diego region and at minimum one year of monitoring experience within Kumeyaay ancestral territory.</p>	During ground-disturbing activity	Project Applicant	A minimum of one Native American monitor of Kumeyaay descent shall be present during ground-disturbing activity.	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>CUL-5: California Health and Safety Code Section 7050.5: During project construction, if human remains are found, the California Health and Safety Code Section 7050.5 states no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. Per the Public Resources Code, in the event of an unanticipated discovery of human remains, the County Coroner shall be notified immediately. If the human remains are determined to be prehistoric, the coroner shall notify the Native American Heritage Commission, which will determine and notify a Most Likely Descendant.</p>	During ground-disturbing activity and construction	Project Applicant	If human remains are found, the California Health and Safety Code Section 7050.5 states no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. Per the Public Resources Code, in the event of an unanticipated discovery of human remains, the County Coroner shall be notified immediately. If the human remains are determined to be prehistoric, the coroner shall notify the Native American Heritage Commission, which will determine and notify a Most Likely Descendant.	
Noise				
<p>NOISE-1: Construction Noise Prior to issuance of any grading permit(s) for the project, the project applicant or its contractor(s) shall ensure that:</p> <ul style="list-style-type: none"> • On-site rocks and boulders shall be relocated off-site to the maximum extent feasible. • All on-site rock crushing shall occur at a distance of 165 feet or more from the southern, eastern, and western property lines. • All construction equipment, fixed or mobile, shall be equipped with properly operating and maintained mufflers. • Construction noise reduction methods such as shutting off idling equipment, maximizing the distance between construction equipment staging areas and occupied residential areas, and use of electric air compressors and similar power tools, rather than diesel equipment, shall be used where feasible. 	Prior to issuance of any grading permit(s)	Project Applicant	<p>The project applicant or its contractor(s) shall ensure that:</p> <ul style="list-style-type: none"> • On-site rocks and boulders shall be relocated off-site to the maximum extent feasible. • All on-site rock crushing shall occur at a distance of 165 feet or more from the southern, eastern, and western property lines. • All construction equipment, fixed or mobile, shall be equipped with properly operating and maintained mufflers. • Construction noise reduction methods such as shutting off idling equipment, maximizing the distance between construction equipment staging areas and occupied residential areas, and use of electric air 	

MITIGATION MONITORING AND REPORTING PROGRAM

Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<ul style="list-style-type: none"> • During construction, stationary construction equipment shall be placed such that emitted noise is directed away from or shielded from sensitive noise receivers. • During construction, stockpiling and vehicle staging areas shall be located as far as practical from noise sensitive receptors. • The project shall be in compliance with the City's Noise Abatement and Control Ordinance such that construction shall occur on the weekdays (Monday through Friday) and Saturday between the hours of 7:00 a.m. to 7:00 p.m. and a notice of construction shall be mailed to all owners and occupants within 300 feet of the project site no more than 10 days before the start of construction. Construction hours, allowable workdays and the phone number of the job superintendent shall be clearly posted at all construction entrances to allow surrounding property owners and residents to contact the job superintendent. In the event that the City receives a complaint regarding construction noise, appropriate corrective actions shall be implemented and a report of the action provided to the reporting party. 			<p>compressors and similar power tools, rather than diesel equipment, shall be used where feasible.</p> <ul style="list-style-type: none"> • During construction, stationary construction equipment shall be placed such that emitted noise is directed away from or shielded from sensitive noise receivers. • During construction, stockpiling and vehicle staging areas shall be located as far as practical from noise sensitive receptors. <p>The project shall comply with the City's Noise Abatement and Control Ordinance such that construction shall occur on the weekdays (Monday through Friday) and Saturday between the hours of 7:00 a.m. to 7:00 p.m. and a notice of construction shall be mailed to all owners and occupants within 300 feet of the project site no more than 10 days before the start of construction. Construction hours, allowable workdays and the phone number of the job superintendent shall be clearly posted at all construction entrances to allow surrounding property owners and residents to contact the job superintendent. In the event that the City receives a complaint regarding construction noise, appropriate corrective actions shall be implemented and a report of the action provided to the reporting party.</p>	

MITIGATION MONITORING AND REPORTING PROGRAM				
Mitigation Measure	Implementation/Timing	Responsible Monitoring Party	Compliance Action	Certified Complete
<p>NOISE-2: Operational Noise</p> <p>The project shall incorporate the following measures:</p> <ul style="list-style-type: none"> • Prior to precise grading, a six-foot masonry wall noise barrier shall be constructed along the western, southern, and eastern project boundaries as depicted in Figure 6a. The masonry wall shall be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue. The sound attenuation walls must be solid and free of cracks, gaps, or holes through or below the wall. Any seams or cracks must be filled or caulked. • The manufacturer noise specifications for the car wash blower system selected for the project shall include a silencer package and shall not exceed a sound power level of 106 dB(A) L_{pw}. • The manufacturer noise specifications for the car wash vacuum hoses selected for the project shall not exceed a sound power level of 77.3 dB(A) L_{pw}. • Operation of the car wash shall be prohibited during the hours of 7 p.m. and 7 a.m. 	<ul style="list-style-type: none"> • Prior to precise grading • Prior to approval of grading permit, noise specifications and operation shall be included as notes on the grading plans 	Project Applicant	<ul style="list-style-type: none"> • Prior to precise grading, a six-foot masonry wall noise barrier shall be constructed along the western, southern, and eastern project boundaries as depicted in Figure 6a. The masonry wall shall be stepped up to eight feet along the eastern project boundary 50 feet south of Mission Gorge Road and 50 feet north of Railroad Avenue. The sound attenuation walls must be solid and free of cracks, gaps, or holes through or below the wall. Any seams or cracks must be filled or caulked. • The manufacturer noise specifications for the car wash blower system selected for the project shall include a silencer package and shall not exceed a sound power level of 106 dB(A) L_{pw}. • The manufacturer noise specifications for the car wash vacuum hoses selected for the project shall not exceed a sound power level of 77.3 dB(A) L_{pw}. • Operation of the car wash shall be prohibited during the hours of 7 p.m. and 7 a.m. 	

RESOLUTION NO.

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SANTEE, CALIFORNIA
APPROVING CONDITIONAL USE PERMIT P2022-10 FOR AN AUTO CENTER
AND RELATED SITE IMPROVEMENTS AT 10335 MISSION GORGE ROAD (APN
384-091-01, -13, -14) IN THE GENERAL COMMERCIAL (GC) ZONE**

**APPLICANT: KEVIN PERRY (HED DESIGN)
RELATED CASE FILES: AEIS2022-11**

WHEREAS, Table 13.12.030.A of the Santee Municipal Code (SMC) allows automotive services including sales, repairs, and carwashes in the General Commercial (GC) zone with the approval of a Conditional Use Permit; and

WHEREAS, on August 14, 2023, Kevin Perry of HED Design submitted a complete application for Conditional Use Permit P2022-10 to establish an auto center (Project) at 10335 Mission Gorge Road in the General Commercial (GC) Zone; and

WHEREAS, the proposed Project includes two dealership buildings, an auto body shop, an auto detailing building, an automatic carwash and related site improvements on a vacant 13.1-acre site; and

WHEREAS, the proposed Project is consistent with the applicable General Plan designation and all applicable general plan policies, and with the Zoning Ordinance land use regulations; and

WHEREAS, pursuant to the requirements of the California Environmental Quality Act (CEQA), an Initial Study (AEIS2022-11) was completed for the Project, which determined that all environmental impacts of the Project would be less than significant with mitigation and a Notice of Intent to Adopt a Mitigated Negative Declaration (State Clearinghouse Number 2023080274) (MND) was prepared and advertised for public review from August 11, 2023, to September 11, 2023; and

WHEREAS, the Mitigation Monitoring and Reporting Program prepared for the Project is attached to this Resolution as Exhibit "A" and made a condition of Project approval; and

WHEREAS, the proposed Project is located within Airport Influence Area 1 of the Gillespie Field Airport Land Use Compatibility Plan (ALUCP). On January 18, 2023, the Federal Aviation Administration determined that the Project, as designed, presented no hazard to air navigation, and the Airport Land Use Commission (ALUC) determined that the Project as conditionally consistent with the ALUCP pursuant to the ALUC determination letter dated August 10, 2023; and

WHEREAS, the Project is subject to the payment of development impact fees based on the Project's commercial use classification and size; and

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WHEREAS, development impact fees ensure that new development will not burden the existing service population with the cost of facilities required to adequately support new development; and

WHEREAS, new development requires the construction of capital improvements, including, without limitation, drainage improvements, traffic improvements, traffic signals, public park facilities, community facilities and other public improvements, public services and community amenities; and

WHEREAS, the purpose of the development impact fees imposed on the subject Project is to provide a funding source from the Project to fund related capital improvements that serve the Project, specifically drainage improvements, traffic improvements and traffic signals; and

WHEREAS, it is in the interest of the public's health, safety and welfare for the Project to pay the costs of constructing these public facilities that are reasonably related to the impacts of the Project; and

WHEREAS, a reasonable relationship exists between the use of the development impact fees and the Project as capital improvements funded by these fees are expected to provide a citywide network of drainage and traffic-related facilities beneficial to the Project; and

WHEREAS, the Project's facilities need, specifically the need for drainage, traffic and traffic signal facilities, is based on the Project's commercial classification and on the demand generated by the Project for those facilities and the Project's corresponding fair share contribution toward funding of said needed facilities; and

WHEREAS, the development impact fees established for the Project are based on the Project's size to ensure a reasonable proportionality between the Project and the cost of the facilities attributable to the Project; and

WHEREAS, the subject Project is not subject to Measure N as the Project is not a General Plan amendment, Planned Development Area, or new Specific Planning Area, nor would it increase the residential density permitted by law, make changes to the General Plan Residential Land Use categories that would intensify use, make changes to the land use designation of any parcel in a manner that intensifies use, nor make changes to slope criteria, minimum parcel sizes, or lot averaging provisions of the General Plan that would permit increased density or intensity of use; and

WHEREAS, the Planning & Building Department scheduled Conditional Use Permit P2022-10 for public hearing on October 25, 2023; and

WHEREAS, on October 25, 2023, the City Council held a duly advertised public hearing on Conditional Use Permit P2022-10; and

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WHEREAS, the City Council considered the Staff Report, the Mitigated Negative Declaration, all recommendations by staff, public testimony, and all other relevant information contained in the administrative record regarding the Project.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Santee, California, after considering the evidence presented at the public hearing, as follows:

SECTION 1: The findings in accordance with Section 13.06.030.E of the Santee Municipal Code for a Conditional Use Permit are made as follows:

- A. *That the proposed use is in accord with the General Plan, the objectives of the zoning ordinance, and the purposes of the district in which the site is located.*

The use is in accord with the General Plan, the objectives of the zoning ordinance, and the purposes of the district in which the site is located because the site is designated for General Commercial use in the Santee General Plan which is applied to areas in the City with direct access to major arterials such as Mission Gorge Road. The proposed automotive services land use is permitted by Conditional Use Permit in accordance with the provisions of Santee Municipal Code Chapter 13.12 which regulates commercial districts. The site will be comprehensively developed to provide services to meet the needs of the community consistent with the intent of General Commercial general plan land use designation and corresponding zoning district.

The proposed auto center groups complimentary auto-oriented uses in one comprehensive development on an appropriately commercially-zoned site. Surrounding properties are commercially and industrially zoned and, as such, the proposed development would be consistent with planned allowable uses in this area. The proposal would be also compatible with existing auto-oriented uses along the north side of Mission Gorge Road. The Project location is also suitable for the proposed use as it is along the City's primary commercial corridor with direct access from Mission Gorge Road, a major arterial in the Mobility Element.

- B. *That the proposed use, together with the conditions applicable thereto, will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.*

The proposed use, together with the conditions applicable thereto, will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity because:

1. The proposed use will be developing the property with a use that supports the planned development along the Mission Gorge Road commercial corridor; and
2. The proposed development incorporates buffering and remedial measures, such as landscape strips and the erection of six- to eight-foot walls along the perimeter of the site adjoining these residential uses in order to create harmony with adjacent residential uses. The proposed meandering sidewalk with

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landscape buffers along Mission Gorge Road will also help enhance the area and provide residents of existing conforming residential development near the site an improved pedestrian experience along this segment of Mission Gorge Road; and

3. Improvements to the property, including landscaping, enhanced streetscapes, and high-quality commercial buildings, will enhance the appearance of the site; and
 4. The Project will dedicate the necessary right-of-way along Mission Gorge Road and make improvements to related public infrastructure; and
 5. The estimated development impact fee payment is \$1,839,751.49. This amount is preliminary and is subject to change based on the actual impact fees in effect at the time of building permit issuance.
- C. *That the proposed use complies with each of the applicable provisions of the zoning ordinance.*

The proposed use complies with each of the applicable provisions of the zoning ordinance including building setbacks, height limitations, and minimum parking standards. All development standards of the applicable zoning code provisions are being met, and all proposed public improvements will meet the City's public works standards. The proposed auto center groups complimentary auto-oriented uses in one comprehensively designed development. Surrounding properties are commercially and industrially zoned and, as such, the proposed development would be consistent with planned allowable uses in this area. The proposal would be also compatible with existing auto-oriented uses along the north side of Mission Gorge Road. The project location is also suitable for the proposed use as it is along the City's primary commercial corridor with direct access from Mission Gorge Road, a major arterial in the Mobility Element.

SECTION 2: Conditional Use Permit P2022-10, dated August 14, 2023, to establish an auto center at 10335 Mission Gorge Road, is hereby approved subject to the following conditions:

- A. Following Project approval, the applicant shall schedule with the City Project Planner a post approval meeting to discuss the Project conditions of approval, timing of design and construction and implementation of the Project conditions. The meeting shall be scheduled within thirty days of Project approval and prior to any plan submittals. The applicant should include their Project design team including Project architect, their design engineer, and their landscape architect.
- B. The applicant shall include provisions in their design contract with their design consultants that following approval by the City, all construction drawings or technical reports accepted by the City, exclusive of architectural building plans, shall become the property of the City. Once accepted, these plans may be freely

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used, copied or distributed by the City to the public or other agencies, as the City may deem appropriate. A letter of acknowledgement of this requirement from each design consultant is required at the time of plan submittal. This letter shall be in a format acceptable to the City Engineer.

- C. To coordinate with the City Geographic Information System, horizontal and vertical control for all construction drawings, grading plans, landscape plans, street improvement plans, plot plans, etc., shall be obtained from ROS 11252. All plans, exclusive of building plans, shall be prepared at an engineering scale of 1"=20' unless otherwise approved by the Project engineer.
- D. The applicant shall ensure that all property corners are properly monumented. If corners have been lost or do not exist, corners shall be set and a Record of Survey filed prior to issuance of a building permit.
- E. Starting with the first plan check submittal, all plan sets shall be submitted concurrently to Padre Dam Municipal Water District (Padre Dam) for review and approval. The City does not coordinate the review process with Padre Dam, this is the responsibility of the design engineer and the landscape architect. Failure to properly coordinate this review may result in delay of issuance of permits required for construction. It is incumbent upon the applicant to oversee the plan submittals of their design consultants.
- F. Minor or Major Revisions to the Conditional Use Permit, such as changes to the building elevations, site design, or landscape design, shall be approved by the Planning & Building Director, unless in the Planning & Building Director's judgment, a Major Revision should be reviewed by the City Council.
- G. The Project shall be in compliance with the adopted California Building Standards Code at the time of building permit application and shall be subject to expirations for plan review per the City of Santee Municipal Code 11.04.030 and 11.06.010.
- H. Project site shall be maintained in a clean, orderly condition at all times free of debris and junk material and with well-maintained landscaping.
- I. Project lighting shall be stationary, directed away from adjacent properties and shielded so that no light or glare is transmitted or reflected in such concentrated quantities or intensities as to be detrimental to the surrounding area.
- J. Proposed signage shall comply with SMC Chapter 13.32 and separate sign permits shall be obtained for proposed signage for the Project.
- K. Hours of operation for the auto body shop, auto detail, and carwash shall be between the hours of 7 a.m. and 7 p.m.

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- L. Hours of operation for outdoor activities associated with the auto dealerships shall be between the hours of 7 a.m. and 9 p.m.
- M. All building permits shall expire per the California Building Code Section 105.
- N. Prior to issuance of Building Permit, the Applicant shall:
 - 1. The applicant shall include provisions in their design contract with their design consultants that following approval by the City, all construction drawings or technical reports accepted by the City, exclusive of architectural building plans, shall become the property of the City. Once accepted, these plans may be freely used, copied or distributed by the City to the public or other agencies, as the City may deem appropriate. A letter of acknowledgement of this requirement from each design consultant is required at the time of plan submittal. This letter shall be in a format acceptable to the City Engineer.
 - 2. Applicant shall ensure that all property corners are properly monumented. If corners have been lost or do not exist, corners shall be set, and a Record of Survey filed prior to issuance of a building permit.
 - 3. Parcel Map - The applicant shall make an electronic submittal via the City of Santee Permitting and Licensing Portal. The items to be submitted include but are not limited to the following:

Please include the following with the first submittal:

- a. Parcel Map
- b. Current preliminary title report (dated within six months of submittal date) which shows current ownership.
- c. All documents listed in the preliminary title report.
- d. All reference documents used to prepare the parcel map.
- e. Closure calculations for the map.
- f. Resolution of Approval approving the Project.

In addition to the above electronic submittal requirements, one hard copy of the full-sized parcel map shall be provided to the Project engineer. Map check fees shall be paid in accordance with the City Fee Schedule. The amount due will be determined by staff after the initial intake. To begin the review process, fees must be paid in full.

The signature submittal of the parcel map mylars shall be by appointment only. Contact the Project engineer to schedule a time for this final submittal.

Please include the following:

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- a. A copy of the map in Autocad format for incorporation into the City GIS data base.
 - b. Mylars of the map with all required signatures and notaries obtained including Padre Dam Municipal Water District if they are to sign the map.
 - c. Copies of all certified return receipts for all signature omission letters.
 - d. Subdivision Guarantee.
4. Street Improvement Plans shall be submitted to the Engineering Department for review and acceptance. Prior to the start of construction of any improvements, public or private, within the limits of the public right-of-way, the applicant shall have plans accepted, agreements executed, securities posted and an encroachment permit issued. All improvements shall be installed in accordance with City standards and at the applicant's cost unless otherwise indicated. The following improvements are conditioned as part of this development:
- a. Construct commercial driveways a minimum width of 30 feet on Cottonwood Avenue and Mission Gorge Road per City of Santee Standards. The driveway design shall be per the City of Santee Public Works Standard Drawing PW-21 and to the satisfaction of the City Engineer.
 - b. Widen Mission Gorge Road to major road standards (82' curb to curb/102' right-of-way). Show curb, gutter, sidewalks, street lighting, fire hydrants, and pedestrian ramps at curbs.
 - c. Modify the traffic signal at Mission Gorge Road and Edgemoor Drive to accommodate the driveway and the fourth leg at the intersection. The driveway shall be a minimum width of 40' with 30' corner radius. The improvement plans shall include a separate plan sheet for the signal modification.
 - d. Construct full intersection improvements at the proposed northeast driveway location, that shall be aligned and consistent with the existing improvements at the north side of Edgemoor Drive and Mission Gorge Road.
 - e. Widen Cottonwood Avenue to collector street standards (64' curb to curb/84' right-of-way). Show curb, gutter, sidewalks, street lighting, fire hydrants and pedestrian ramps at curbs. Install two 82-watt LED streetlights on Cottonwood Ave at approximately 140' south of Mission Gorge Road for one, and a second light approximately 360' south of Mission Gorge Road.

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- f. Construct Cottonwood Avenue along the property frontage to Green Street and Hydromodification standards as per the SWQMP exhibit dated April 21, 2023. The design shall be modified as necessary to accommodate the spacing requirements of the water and sewer agency and/or as directed by the City of Santee Engineering Department.
- g. Modify the existing traffic signal at Mission Gorge Road and Cottonwood Avenue to accommodate the widening of Cottonwood Avenue, and install protected left turn phases for north and south bound (traffic mitigation). The improvement plans shall include a separate plan sheet for the signal modification.
- h. Widen Railroad Avenue to local street standards (36' curb to curb/60' right-of-way). Show curb, gutter, sidewalks, street lighting, fire hydrants, and pedestrian ramps at curbs.
- i. Provide drainage improvements in accordance with the City of Santee Master Drainage Study, Project 3.1A. Remove and replace 18-inch CSP with 470 LF of 36-inch RCP along the west side of Cottonwood Avenue.
- j. Remove and replace 28 x 20-inch CMP at Mission Gorge intersection with 40 LF of 48-inch RCP.
- k. Provide a connection with the proposed 36-inch pipe to the existing 18-inch located approximately 50 feet south of the Project site on the west side of Cottonwood Avenue. The addition and/or replacement of two (2) cleanouts and one (1) inlet is anticipated. Off-site improvements that meet the requirements of a deficient master drainage study may be eligible for fee credit reimbursement up to the developer drainage impact fee amount due. Reimbursements shall be in accordance with City legislative policy memorandum, LPM 93-1.
- l. Provide drainage improvements in accordance with the City of Santee Master Drainage Study, Project 3.1B. Add 40 LF of new 36-inch RCP to meet the above conditioned 36-inch pipe to be installed along the west side of Cottonwood Avenue.
- m. Remove and replace lateral 28 x 20-inch CMP at Mission Gorge intersection with 50 LF of 42-inch RCP. The addition and/or replacement of two (2) inlets is anticipated.
- n. Street Improvement plans shall be one hundred percent (100%) complete at the time of plan check submittal, be prepared in accordance with City guidelines and the requirements set forth herein, and be ready for acceptance by the City. Partial or incomplete submittals will not be accepted for plan check. The applicant shall make an electronic

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submittal via the City of Santee Permitting and Licensing Portal. The items to be submitted include but are not limited to the following:

- 1) 100% complete improvement plans.
- 2) Estimate for the cost of construction.
- 3) Resolution of Approval approving the Project.

In addition to the above electronic submittal requirements, one hard copy of the full-sized improvement plans shall be provided to the City's Project Engineer. Plan check and inspection fees shall be paid in accordance with the City Fee Schedule. The amount due will be determined by staff after the initial intake. To begin the review process, fees must be paid in full.

5. Precise Grading Plans shall be submitted to the Engineering Department for review and acceptance.
 - a. Horizontal and vertical control for all plans shall be obtained from ROS 11252 and shall be prepared at an engineering scale of 1"=20' unless otherwise approved by the City Project engineer.
 - b. Project landscape and irrigation plans shall be included as a part of the grading plan set.
 - c. The grading plans shall reflect the exploratory excavations, and subsequent backfilling, re-compaction, or otherwise restored condition, performed under the direction of an engineering geologist as documented in the geotechnical report dated March 9, 2022.
 - d. All recommended measures identified in the approved geotechnical study shall be incorporated into the Project design and construction.
 - e. All retaining wall footings shall have a minimum of seven (7') feet to daylight. Modification of any kind to the San Diego Regional Standard Drawings, such as the installation of a freestanding masonry wall, will require calculations prepared by a licensed civil engineer showing the factor of safety for bearing, sliding, and overturning. Freestanding masonry walls greater than six (6) feet in height shall require a separate building permit.
 - f. The applicant shall demonstrate the ability to install improvements over the existing utility easements as proposed. Existing services that may exist on the Project site without the benefit of an easement shall be relocated as necessary at the developer's expense, or an easement shall be granted over said existing services. Easement and or relocation shall be to the satisfaction of the utility provider.

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- g. The applicant shall add a note to the plans, preferable on Section G-G, that the on-site proposed gravel area between the screen wall and up to the property line along Happy Lane will be maintained by the Project site. The section shall reflect the existing utility poles and proposed disposition of the existing overhead wires.
- h. Pad elevations of the proposed and all existing structures within 100 feet of the property boundary shall be shown on the grading plans.
- i. Maintenance steps/access shall be provided in all water quality basin areas where the finished surface of the 3" mulch/cobble layer is 3 feet lower than the adjoining level finished surface. The overflow of all basins shall be demonstrated to occur to an area that is a hardened conveyance.
- j. On-site detention shall be provided for all runoff to account for increase in runoff generated by the addition of impermeable area. This storage shall include capacity for the event in which the basins or drainage system becomes incapacitated during a 100-year rain event, i.e., demonstrate the sizing required to contain on-site the Post Project (Un-Mitigated) Q100 of 50.5 cfs. Note, the existing downstream drainage system located in Mission Gorge Road is identified as being deficient.
- k. Grading plans shall include preliminary recommendations for all pavement design sections within the Project limits. The pavement structural section shall be designed based on the "R" value method using a minimum traffic index of 5.0 for parking stalls for automobiles and light-duty vehicles, 5.5 for driveways for automobiles and light duty vehicles, 6.0 for medium truck traffic areas, 7.0 for driveways for heavy truck and fire truck traffic. Structural sections shall consist of asphalt concrete over approved aggregate base material. Minimum concrete section shall be 6 to 7-1/2 inches PCC over compacted, non-expansive soil. Thickness based upon on traffic category and trucks per day and per the geotechnical engineer of record recommendations. Mix design shall be a minimum class 520-C-2500. R-value test data and design calculations shall be submitted for approval to the Engineering Department a minimum of seven days prior to placement of paving. The pavement design report shall conform to City of Santee Form 435 – PAVEMENT DESIGN AND R-VALUE TEST SUBMITTAL PROCEDURES.
- l. Grading plans shall be one hundred percent (100%) complete at the time of plan check submittal, be prepared in accordance with City guidelines and be ready for acceptance by the City. Partial or incomplete submittals will not be accepted for plan check. The applicant shall make an electronic submittal via the City of Santee Permitting and Licensing

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Portal. The items to be submitted include but are not limited to the following:

- 1) 100% complete grading, landscape, and irrigation plans.
- 2) A completed grading permit application.
- 3) Estimate for the cost of construction.
- 4) Drainage Study specified here within.
- 5) Storm Water Quality Management Plan specified here within.
- 6) Operation & Maintenance (O&M) plan specified here within.
- 7) Storm Water Pollution Prevention Plan specified here within.
- 8) Geotechnical Study specified here within.
- 9) Letters of permission from any adjoining property owners if grading is proposed off-site. Letters shall be in a form acceptable to the City.
- 10) Letters of acknowledgement signed and sealed, from each design consultant acknowledging City ownership of all construction drawings following City approval as specified here within.
- 11) Resolution of Approval approving the Project.

In addition to the above electronic submittal requirements, one hard copy of the above-mentioned full-sized plans, documents and reports shall be provided to the Project engineer. Plan check and inspection fees shall be paid in accordance with the City Fee Schedule. The amount due will be determined by staff after the initial intake. To begin the review process, fees must be paid in full.

6. Provide a geotechnical study prepared in accordance with the Santee General Plan. The investigation will be subject to independent third party review to be paid for by the applicant. The applicant shall place a deposit with the Engineering Department in an amount satisfactory to the City Engineer to cover the cost of the review. All recommended measures identified in the approved study shall be incorporated into the Project design. The Geotechnical/Seismic Hazard Study for the Safety Element of the Santee General Plan which details, in Table A-1, study criteria necessary to conform to the General Plan requirements, can be accessed from the City's website.
 - a. The geotechnical report shall analyze any proposed infiltration techniques (trenches, basins, dry wells, permeable pavements with underground reservoir for infiltration) for any potential adverse geotechnical concerns. Geotechnical conditions such as: slope stability, expansive soils, compressible soils, seepage, groundwater depth, and loss of foundation or pavement subgrade strength should be addressed, and mitigation measures provided.

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7. Applicant consents to annexation of the property under development to the Santee Roadway Lighting District and agrees to waive any public notice and hearing of the transfer. Applicant shall pay the necessary annexation costs and upon installation of any streetlights required for the development, pay the necessary street light energizing and temporary operating costs.
8. Replace failed or inadequate pavement to the centerline and/or sidewalk adjacent to the site on Cottonwood Avenue, Mission Gorge Road, and Railroad Avenue to the satisfaction of the City Engineer.
9. Applicant shall pay all development impact fees in effect at the time of issuance of building permits. At present, the fees are estimated to be as follows:
 - a. Drainage \$ 746,276.37 (estimated)
calculated based on \$ 1,452 / 1000 square feet of increased impermeable area.
 - b. Traffic \$ 941,595.50
calculated based on a fee rate of \$ 9,721 / 1000 square feet of building area.
 - c. Traffic Signal \$ 151,879.62
calculated based on a fee rate of \$ 1,568 / 1000 square feet of building area.

Impact fee amounts shall be calculated in accordance with the City Fee Schedule and based on current fee ordinances in effect at issuance of building permit. The drainage fee shall be calculated based on the actual impermeable area created by the Project including off-site street improvements or other improvements beyond the Project boundary. The applicant shall provide certification of final site and building areas by their engineer of work to be approved by the City Engineer for use in calculating the final fee amounts. Fees shall be adjusted on an annual basis in accordance with the Municipal Code.

10. Following issuance of a grading permit the applicant shall complete rough grading in accordance with the approved grading plans and the recommendations of the Project's geotechnical engineer. Following completion of the rough grading and prior to issuance of any building permits, provide three originals of the pad compaction certification from the geotechnical engineer and three originals of the pad elevation certification from the Project civil engineer to the City Project engineer.
11. Provide a drainage study prepared by a registered Civil Engineer, with demonstrated expertise in drainage analysis and experience in fluvial

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geomorphology and water resources management. Storm drainage shall be designed to adequately convey storm water runoff without damage or flooding of surrounding properties or degradation of water quality.

- a. The drainage study shall identify and calculate storm water runoff quantities expected from the site and upstream of the site and verify the adequacy of all on-site or off-site facilities necessary to discharge this runoff. The drainage system design shall be capable of collecting and conveying all surface water originating within the site, and surface water that may flow onto the site from upstream lands, and shall be in accordance with the latest adopted Master Drainage Plan, the requirements of the City of Santee Public Works Standards, including analysis of the 10-year, 50-year and 100-year frequency storms, and be based on full development of upstream areas.
 - b. The drainage study shall compute rainfall runoff characteristics from the Project area including, at a minimum, peak flow rate, flow velocity, runoff volume, time of concentration, and retention volume. These characteristics shall be developed for the 10-year, 50-year and 100-year frequency six-hour storm during critical hydrologic conditions for soil and vegetative cover. Storm events shall be developed using isopluvial maps and in accordance with the San Diego County Hydrology Manual.
 - c. The report shall demonstrate the drainage path to the San Diego River. Note, the existing system, specifically the Las Colinas Channel, has not been demonstrated as a hardened conveyance or a suitable outfall, and hydromodification on site is required by this Project.
 - d. The report shall include sizing of all existing and proposed inlets both on-site and off-site.
12. Provide a Storm Water Quality Management Plan (SWQMP) prepared and in accordance with the City of Santee Storm Water Ordinance and in accordance with the City of Santee Best Management Practices (BMP) Design Manual dated February 2016. The SWQMP must include best management practices (BMPs) to address water quality and hydromodification. An Operation and Maintenance Plan describing maintenance requirements and costs for BMP maintenance and provision of maintenance verification will be provided.

The SWQMP shall include the following:

- a. Develop and implement appropriate Best Management Practices (BMPs) to ensure that the Project does not increase pollutant loads from the site. A combination of respective storm water BMPs, including Site

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Design, Source Control, and Structural Treatment Control shall be implemented in accordance with the approved SWQMP.

- b. The Project design shall incorporate Low Impact Development (LID) and site design BMPs to minimize directly connected impervious areas and to promote infiltration using LID techniques as outlined in the County of San Diego's LID handbook. Parking areas and drive aisles shall be designed to drain to landscape areas.
- c. The site shall comply with full trash capture requirements by providing completely enclosed trash and recycling enclosures, and fitting all storm drain inlets with a State certified grate/screen or trash rack. Said devices must be designed to capture debris of 5 mm or greater, while preventing flooding potential. In addition, any adjacent public storm drain inlet structure to which the site discharges must also be retrofitted with trash capture devices. The device which shall be used for public inlets is the ADS FlexStorm Connector Pipe Screen system or approved equal.
- d. All inlets must be labeled with concrete stamp or equivalent - stating, "No Dumping - Drains to River". If work is performed on a public inlet, the public inlet must be labeled with the following standard specification: Public storm drain inlet markers shall be 4" diameter, stainless steel, natural embossed, inlet marker as manufactured by Almetek Industries or approved equal. Marker shall contain/state "No Dumping" with "Fish w/ Wave" symbol and "Drains to Waterways" legend. Marker shall contain 2" long x 1/4" diameter threaded rod and shall be installed flush and wet-set in top of inlet, centered on width of inlet opening.
- e. Down spouts and HVAC systems are not permitted to be connected to any storm drain conveyance system. All non-storm water discharges must either drain to landscaped areas, or be plumbed to the sewer.
- f. Fire suppression systems must be designed to be able to discharge to a sewer clean out for all maintenance and testing activities, or otherwise captured and contained on-site.
- g. California native/drought-tolerant plants shall be used to the maximum extent feasible to minimize the need for irrigation. Where irrigation is necessary, then the system shall be designed and installed to prevent overspray or irrigation runoff during normal operations and during a break in the line.
- h. Provide a narrative in the source control section regarding the types of material to be stored outdoors and how materials shall be covered and/or protected from the outside elements and be stored above the finished grade to prevent contact with the storm water runoff.

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- i. The final Project submittal shall include a standalone Operation and Maintenance (O&M) Plan in accordance with the City of Santee BMP Design Manual.
 13. Minimum best management practices for storm water and water quality will be incorporated into the Project's Storm Water Quality Management Plan (SWQMP).
 14. Construction Site Storm Water Compliance
 - a. Provide proof of coverage under the General Permit for Discharges of Storm Water Associated with Construction Activity (Construction General Permit, WQ 2022-0057-DWQ) prior to start of construction. This Project disturbs one or more acres of soil or disturbs less than one acre but is part of a larger common plan of development that in total disturbs one or more acres. Construction activity subject to this permit includes clearing, grading, and disturbances to the ground such as stockpiling, or excavation.

Submit a copy of the draft Project specific Storm Water Pollution Prevention Plan (SWPPP) to the City for review and approval. The Construction SWPPP should contain a site map(s) which shows the construction site perimeter, existing and proposed buildings, lots, roadways, storm water collection and discharge points, general topography both before and after construction, and drainage patterns across the Project. The Construction SWPPP must list Best Management Practices (BMP's) the applicant will use to protect storm water runoff and the placement of those BMP's. Section XIV of the Construction General Permit describes the SWPPP requirements.
 15. Provide landscape and irrigation plans and a corresponding landscape bond to the Planning & Building Department in accordance with SMC Chapter 13.36.
 16. Provide proof of upgrading west-facing windows on adjoining apartment building at 8866 Donna Jean Lane for sound attenuation or evidence of rejection of offer to upgrade windows by property owner.
- O. Prior to final occupancy, the Applicant shall:
1. Provide two print copies and a digital copy of both the final approved Storm Water Quality Management Plan and the Operation and Maintenance Plan.
 - a. Submit a print and digital copy of the BMP Certification package. The BMP certification package includes but is not limited to: 'wet' signed and

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stamped certification form(s), all BMP related product receipts and materials delivery receipts, an inspection and installation log sheet, and photographs to document each stage of BMP installation.

- b. Prior to issuance of occupancy, an executed contract must be in place with a qualified storm water service provider and a copy of the SWQMP provided to the consultant and the property manager.
2. A Storm Water Facilities Maintenance Agreement accepting responsibility for all structural BMP maintenance, repair and replacement as outlined in said O&M plan binding on the land throughout the life of the Project will be required prior to issuance of building permit.
3. The applicant shall comply at all times with the following work hour requirements:
 - a. No site work, building construction, or related activities, including equipment mobilization will be permitted to start on the Project prior to 7:00 am and all work for the day shall be completed by 7:00 pm, no exceptions.
 - b. No work is permitted on Sundays or City Holidays.
 - c. No deliveries, including equipment drop off and pick-up, shall be made to the Project except between the hours of 8:00 am and 6:00 pm, Monday through Saturday, excluding Sundays and City Holidays. Deliveries of emergency supplies or equipment necessary to secure the site or protect the public are permitted.
 - d. If the applicant fails or is unable to enforce compliance with their contractors, subcontractors and material suppliers regarding the specified work hours, additional reduction of work hours may be imposed by the Engineering Department.

In addition to the above the applicant shall erect one or more signs stating the work hour restrictions. Signs shall be installed as may be required, in the vicinity of the Project construction trailer if a job site trailer is used, or at such other locations as may be deemed appropriate by the Engineering Department. The sign shall be a minimum of 24" x 36" and shall be weather proofed. The sign content shall be provided by the Engineering Department.

4. Trench work when required within City streets shall be completed within two weeks of the initial start date, including placement of the final trench patch. Trench plates or temporary pavement placement shall be installed at the end of each work day. Advance warning signs on lighted barricades notifying the public of trench plates and or uneven pavement shall be placed and maintained until permanent pavement repairs are made. The

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maximum length of time including weekends and holidays that trench plates may remain on the street is 72 hours after which temporary or permanent asphalt paving shall be placed.

5. Vehicle access on Mission Gorge Road, Cuyamaca Street, Olive Lane, Town Center Parkway, Carlton Hills Boulevard, Woodside Avenue, and Riverview Parkway shall be maintained at all times and all work shall be done at night unless otherwise approved by the City Engineer. When day work is permitted, work hours shall be from 8:30 am to 3:30 pm, including set up and break down of traffic control. No day work will be permitted during the holiday season, defined as beginning the Saturday before Thanksgiving Day and shall extend through New Year's Day, unless otherwise approved by the City Engineer.
6. Comply with all applicable sections of the Municipal Code, Land Development Manual and Public Works Standards of the City of Santee.
7. Applicant shall place all new utilities required to serve the Project underground. No overhead facilities or extension of overhead facilities is permitted.

In addition, the applicant shall underground any existing overhead facilities on-site and underground any overhead facilities adjacent to the Project to the satisfaction of the City Engineer. Adjacent facilities are defined as existing overhead facilities in the abutting half street and may include extension of the undergrounding to either side of the Project to the nearest existing utility pole.

8. Applicant shall obtain parcel map approval and record the parcel map. Once recorded, the applicant shall within thirty days of recordation, provide one mylar copy, digital copies, and two sets of prints of the map to the City for their permanent records. The prints shall be bound and stapled. The prints and mylar shall be in accordance with City standards in effect at the time of recordation.
9. Applicant shall dedicate right-of-way along Cottonwood Avenue adjacent to the site such that the ultimate right-of-way width to centerline is 42 feet. Additional right-of-way may be required as necessary to provide adequate transition to match existing improvements.
10. Applicant shall dedicate right-of-way along Mission Gorge Road adjacent to the site such that the ultimate right-of-way width to centerline is 51 feet. Additional right-of-way may be required as necessary to provide adequate transition to match existing improvements.

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11. Applicant shall dedicate right-of-way along Railroad Avenue adjacent to the site such that the ultimate right-of-way width to centerline is 30 feet. Additional right-of-way may be required as necessary to provide adequate transition to match existing improvements.
12. Applicant shall dedicate a visibility clear space easement at the south east corner of the intersection of Cottonwood Avenue and Mission Gorge Road.
13. Applicant shall dedicate a visibility clear space easements at the proposed driveway at the intersection of Edgemoor Drive and Mission Gorge Road.
14. Plant all new trees in and within 10 feet of the public right-of-way with root control barriers.
15. Construct all improvements within the public right-of-way and improvements as shown on the approved precise grading plans. Improvements shall be completed to the satisfaction of the City Engineer.
16. Provide a minimum 26' wide, paved "fire lane" access roadway through the Project site. The fire lane width shall be measured curb to curb (or edge of pavement to edge of pavement) and shall extend vertically from grade to the highest point of any structures or obstacles constructed adjacent to the fire lane. No building elements, balconies, drains, projections, or any other object shall encroach into this clear space. The fire lane(s) shall be identified by painting curbs red with white-stenciled letters indicating "NO PARKING – FIRE LANE" every 30 feet along all portions of the fire lane. Red stripes with white stenciled letters shall be painted on the curb or asphalt in front of garages along fire lanes as well. Or, signs shall be installed on the edge of the curb indicating the same. Placement of the signs shall be every 75 feet (or other approved spacing), placed in between the curb stenciling. Exact placement shall be approved by the fire code official prior to installation. No building projections are allowed within this fire lane area. Include all turning radius for fire department access on site plan. Note: Tiller truck is approximately 60' in length.
17. One or more fire hydrants are required for your Project. These hydrants shall have two, 2 1/2" ports and one, 4" port, with a minimum fire flow of 3000 gallons per minute for 3 hours. Hydrants shall be of all bronze construction, painted "fire hydrant yellow" and be installed per Padre Dam Water District requirements. Exact location of required hydrants is to be determined by the fire code official prior to installation. All underground utilities including fire mains, fire hydrants and fire service underground devices shall be installed and approved prior to the delivery of construction materials. Show location of existing and proposed hydrants.

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18. A Fire sprinkler monitoring system is required for the buildings to send a signal to a central station monitoring company upon waterflow activation of the fire sprinkler systems. Separate plans shall be submitted to the Fire Department for any fire alarm system(s) or devices for approval prior to installation. The fire sprinkler monitoring system shall be located in the "Fire Riser Room". Plans & documentations for this system shall include, manufacturer cut sheets for all fire alarm devices, California State Fire Marshal Listing sheets for all appropriate devices, plans showing locations of all devices, line diagram & point to point diagram of the alarm system and complete battery & voltage drop calculations for the system.
19. Address numbers shall be placed near the roofline of all structures visible from the street or access roadway. Numbers shall be block style, 12" in height, black in color (or other approved color), in contrast with their background. Address numbers shall also be illuminated for nighttime visibility. A Potter, "SASH-120" Horn/Strobe (or equivalent) shall be located below each address placement for indication of fire sprinkler activation. Exact location and color of address numbers shall be approved by the fire code official prior to installation.
20. The building is required to be constructed with an approved automatic fire sprinkler system installed by a licensed fire sprinkler contractor. Separate plans are required to be submitted to the Fire Department for approval prior to installation. If the fire sprinkler system has 20 or more sprinkler heads, the sprinkler system is required to be monitored by an approved central station monitoring company. Contact the Fire Department for specific requirements for the automatic fire sprinkler system. A Potter, "SASH-120" Horn/Strobe (or equivalent) shall be located below each address placement for indication of fire sprinkler activation.
21. Provide an all-weather, exterior horn/strobe (or equivalent) in lieu of exterior bell for audio/visual notification of sprinkler activation. The horn/strobe is to be installed near the address placement for the building. Exact installation location of the horn/strobe is to be determined by the fire code official prior to installation.
22. A separate plan for the underground fire service (providing water supply to the fire sprinkler system) is required to be submitted to the Fire Department for approval prior to construction. Thrust block inspections are required for the underground fire service piping prior to filling with water. An underground hydrostatic test shall be conducted (at 250 psi for two hours) and a flush of the underground fire service is required. The test and flush are required to be inspected and approved prior to acceptance of the underground fire service. Contact the Santee Fire Department at least 48 hours in advance to schedule the inspections.

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23. Each building or address (equipped with an automatic fire sprinkler system) shall have a walk-in, enclosed, fire sprinkler riser room accessible from the outside of the building or address. The exact size and location of the riser room shall be approved by the Fire Department prior to construction. This room shall contain the fire sprinkler riser(s) for the building/address, pressure gauges for the system, applicable valves, sprinkler head box, "test and drain" inspectors test valve and any diagrams or documentation for the fire protection systems. These rooms shall have exterior locking hardware and a Knox box shall be located at an approved location near the room for easy Fire Department access. The room shall be provided with lighting on the emergency circuit or have battery backup power. The exterior side of the riser room door shall have labeling or signage approved by the fire code official indicating "FIRE RISER ROOM".
24. This device that supplies water to the automatic fire sprinkler system shall be placed in an approved location within 50' of a fire hydrant. The device shall be installed per San Diego County Regional Standard WF-05. The finished height of the Fire Department Connection (FDC) shall be no higher than 48" from grade. The entire device may be painted dark green or brown to blend in with adjacent landscaping. The Fire Department Connection (FDC) shall be painted red. The Fire Department Connection (FDC) once installed, shall be "pinned" in place for theft protection. The (RPDA) device shall be stenciled with 2" white numbers indicating the address served. The assembly shall be equipped with a chain and breakaway locks for security. Location of these devices shall be approved prior to installation. If the building is equipped with central station monitoring of the sprinkler system, the control valves on the device shall be monitored for tamper of the valves.
25. A Knox Box key safe for emergency access of Fire Department personnel is required for the building. Knox Boxes shall be installed at the front entrance, riser room and other required location(s). Knox Box applications may be obtained from the Fire Department. Approval of the number and exact mounting location shall be determined by the fire code official prior to installation.
26. Santee has adopted the use of Knox Fire Department Connection (FDC) Plugs for FDC hose connections to the automatic fire sprinkler systems. These plugs ensure that the FDC's will be clear of obstructions and allow for the proper Fire Department use of automatic fire sprinkler systems. Knox Plugs can be ordered online directly from the Knox Company at Knoxbox.com. Order FDC Plugs for use in the City of Santee. Order model #3043 (two per building if using Siamese connection). Contact Santee Fire Department if assistance is needed in ordering.

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27. A manual and/or automatic fire alarm system is required for the building. Separate plans shall be submitted to the Fire Department for any fire alarm system(s) or devices for approval prior to installation. The fire alarm control panel or a remote keypad for the system shall be located in the "Fire Riser Room". Plans & documentations for the fire alarm system shall include, manufacturer cut sheets for all fire alarm devices, California State Fire Marshal Listing sheets for all appropriate devices, plans showing locations of all devices, line diagram & point to point diagram of the alarm system and complete battery & voltage drop calculations for the system.
28. A minimum of one, 2A10BC fire extinguisher shall be located every 75' of travel distance throughout the building. Exact extinguisher location to be determined by the fire code official prior to installation.
29. Provide NFPA 704 signage for labeling of hazardous materials use, handling or storage. Exact location of signage to be approved by the fire code official prior to installation.
30. At the time of mid-construction, or Rough Fire Inspections, a GIS shape file or geo-referenced TIFF file of the site-plan shall be provided electronically or on digital media to the Fire Department for emergency response mapping. If neither of the two are available, a PDF shall be provided. The site plan shall show all fire access roadways/driveways, buildings, address numbers, fire hydrants, fire sprinkler connections, and other details as required. Please contact the Fire Department for exact details to be submitted for your Project.

SECTION 3: The applicant shall defend, indemnify, and hold harmless the City of Santee and its officers, employees, and agents from any claim, action, or proceeding against the City and/or its officers, employees or agents to attack or set aside, void, or annul the approval of the City of Santee concerning this Conditional Use Permit, or any action relating to or arising out of its approval.

SECTION 4: The terms and conditions of Conditional Use Permit P2022-10 shall be binding upon the permittee and all persons, firms, and corporations having an interest in the property subject to this Conditional Use Permit and the heirs, executors, administrators, successors and assigns of each of them, including municipal corporations, public agencies and districts.

SECTION 5: The approval of Conditional Use Permit P2022-10 expires on October 25, 2026 at 5:00 p.m. except where substantial use has commenced prior to its expiration. If use of the development has not commenced within the three-year period, said expiration date may be extended pursuant to a request for time extension received 60 days prior to the original expiration date. The City Council expressly grants to the Planning & Building Director the authority to extend the expiration date of this approval pursuant to Section 13.04.090 of the Santee Municipal Code, when a request for an extension is filed 60 days

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prior to the original expiration date.

SECTION 6: Pursuant to Government Code Section 66020, the 90-day approval period in which the applicant may protest the imposition of any fees, dedications, reservations, or exactions imposed pursuant to this approval, shall begin on October 25, 2023.

SECTION 7: The applicant shall defend, indemnify, and hold harmless the City of Santee and its officers, employees and agents from any claim, action, or proceeding against the City and/or its officers, employees or agents to attack or set aside, void, or annul the approval of the City of Santee concerning this Resolution or any action relating to or arising out of its approval.

SECTION 8: Pursuant to Fish and Game Code Section 711.4(c), all Project applicants and public agencies subject to the California Environmental Quality Act shall pay a filing fee for each proposed Project, as specified in subdivision 711.4(d) for any adverse effect on wildlife resources or the habitat upon which wildlife depends unless a "no effect" finding is made by the California Department of Fish and Wildlife. This fee is due and payable as a condition precedent to the County Clerk's filing of a Notice of Determination. The City of Santee hereby notifies the Applicant that in order to comply with State Law, the Applicant shall remit to the City of Santee Department of Development Services, within two (2) working days of the effective date (as defined in Section 8 below) of this approval, a certified check payable to the "County Clerk, County of San Diego" in the amount of \$2,814.00. This fee includes an authorized County administrative fee of \$50. Failure to remit the required fee in full within the time specified above will result in notification to the State that a fee was required but not paid, and could result in State imposed penalties and recovery under the provisions of the Revenue and Taxation Code. In addition, Section 21089 (b) of the Public Resources Code, and Section 711.4 (c) of the Fish and Game Code, provide that no Project shall be operative, vested, or final until the required filing fee is paid.

SECTION 9: The documents and materials that constitute the record of proceedings on which these findings have been based are located with the City Clerk at the City of Santee City Clerk's office at 10601 Magnolia Avenue, Building #3, Santee, CA 92071.

ADOPTED by the City Council of the City of Santee, California, at a Regular Meeting thereof held this 25th day of October 2023, by the following roll call vote to wit:

AYES:

NOES:

ABSENT:

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APPROVED:

**_____
JOHN W. MINTO, MAYOR**

ATTEST:

**_____
ANNETTE ORTIZ, CMC, CITY CLERK**